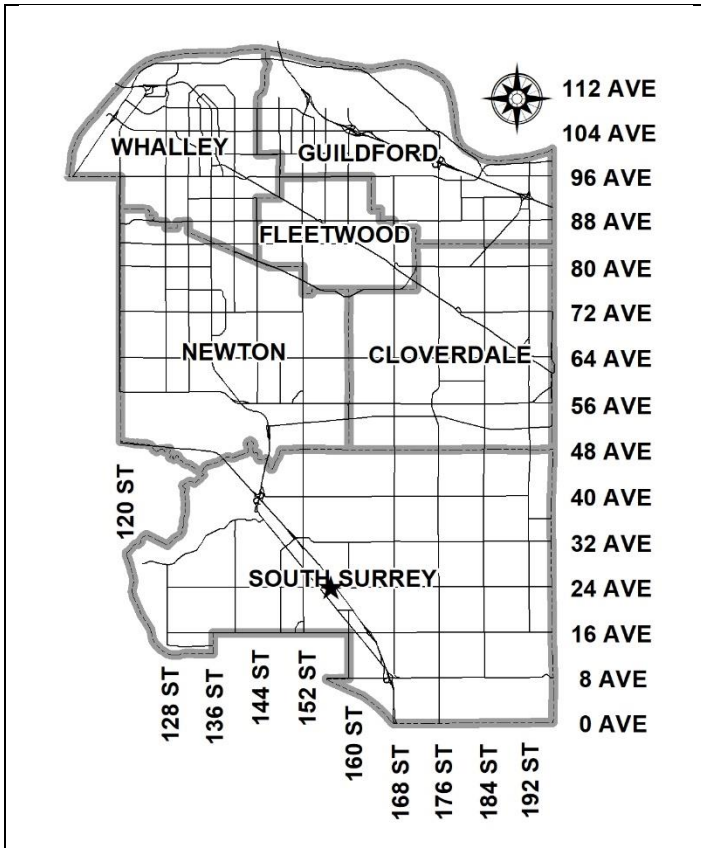


City of Surrey  
**PLANNING & DEVELOPMENT REPORT**

Application No.: 7919-0305-00

Planning Report Date: February 24, 2020



**PROPOSAL:**

- **LAP Amendment** from Mobile Home Park to Single Family Small Lots
  - **Rezoning** of 2388 – 157 Street from RF to RF-12 and a portion of 2371 -157A Street from CD (By-law No. 15162) to RF-12
- Development Variance Permit**

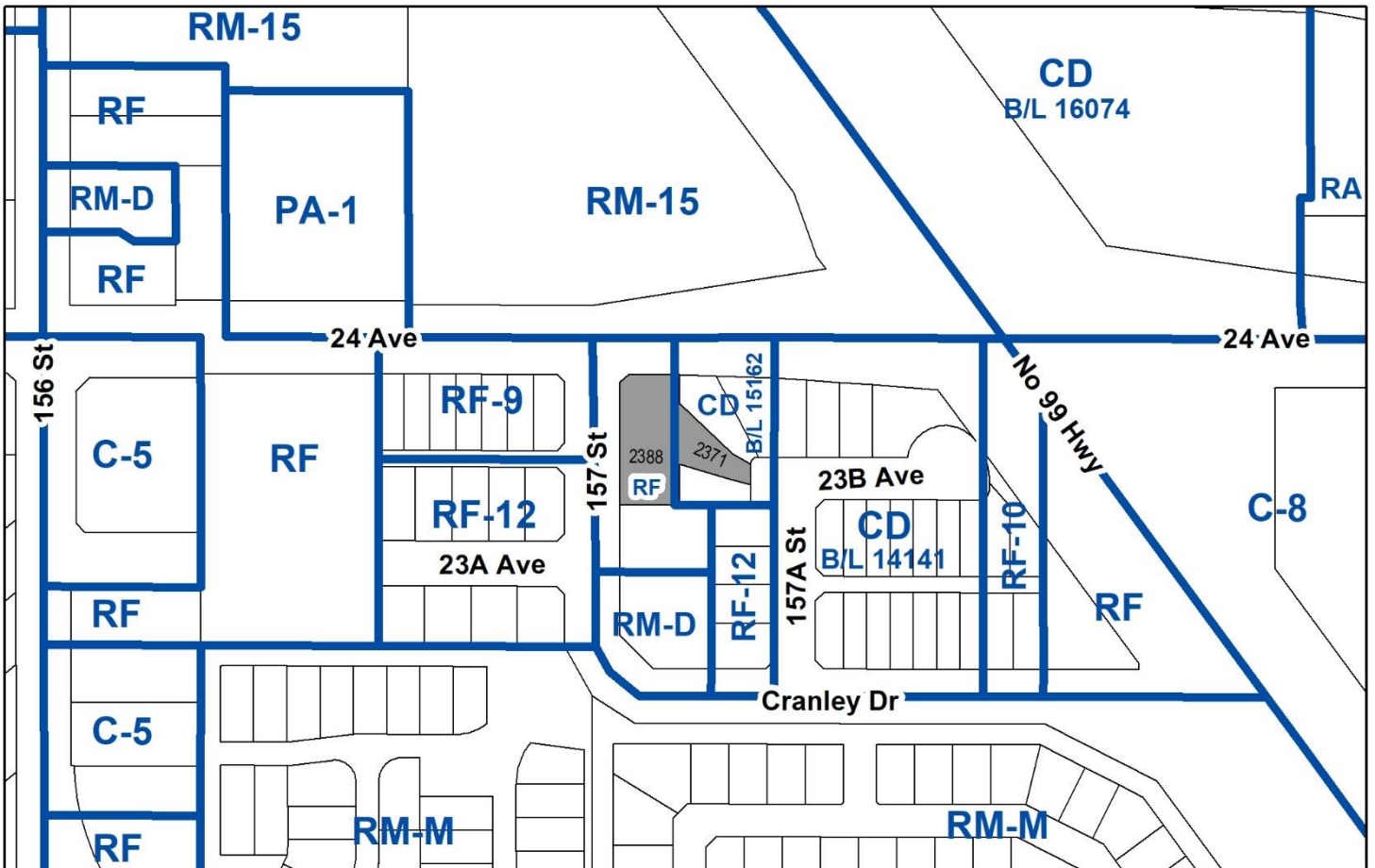
to allow subdivision into 3 new single family small lots

**LOCATION:** 2388 – 157 Street  
 2371 – 157A Street

**ZONING:** RF and CD (By-law No. 15162)

**OCP DESIGNATION:** Urban

**LAP DESIGNATION:** Mobile Home Park



## RECOMMENDATION SUMMARY

- Bylaw Introduction and set date for Public Hearing for Rezoning.
- Approval for Development Variance Permit to proceed to Public Notification.

## DEVIATION FROM PLANS, POLICIES OR REGULATIONS

- Proposing to amend the land use designation of the King George Highway Corridor Local Area Plan (LAP) for the site from "Mobile Home Park" to "Single Family Small Lots".
- Proposing to reduce the rear yard setback requirements of the "Single Family Residential (12) Zone (RF-12)".

## RATIONALE OF RECOMMENDATION

- The proposal complies with the Urban designation in the Official Community Plan (OCP).
- The proposed density and building form are appropriate for this part of the King George Highway Corridor LAP.
- The proposal is an infill development, which is consistent with the established land use and subdivision patterns in this neighbourhood.
- Subdivision of 2388 – 157 Street was planned for and protected through Development Application No. 7903-0229-00, which created 2371 – 157A Street. A 1.9 metre wide strip of land at the rear of 2371 – 157A Street, and spanning the length of 2388 – 157 Street, was protected through a Section 219 Restrictive Covenant that prevented any buildings from being constructed in that area until subdivision occurred. The 1.9 metre strip of land provides 2388 - 157 Street with the required area to facilitate the proposed three lot subdivision.
- The proposed rear yard variances for the three proposed lots ensure functional floor plans while maintaining sufficient privacy from neighbouring dwellings to the east.

## RECOMMENDATION

The Planning & Development Department recommends that:

1. A Bylaw be introduced to rezone the portion of the subject site shown as Block A on the Survey Plan attached as Appendix II, from "Single Family Residential Zone (RF)" and "Comprehensive Development Zone (By-law No. 15162)" to "Single Family Residential (12) Zone (RF-12)" and a date be set for Public Hearing.
2. Council approve Development Variance Permit No. 7919-0305-00 (Appendix VII) varying the following, to proceed to Public Notification:
  - (a) to reduce the minimum rear yard setback of the RF-12 Zone on proposed Lot 1 and Lot 2 from 7.5 metres to 5.0 metres for 50% of the width of the principal building face, while maintaining the rear yard setback for the remaining 50% of the rear building face at 7.5 metres which may be reduced to 4.5 metres for a rear deck; and
  - (b) to reduce the minimum rear yard setback of the RF-12 Zone on proposed Lot 3 from 7.5 metres to 5.0 metres for 50% of the principal building face and from 7.5 metres to 6.0 metres for the remaining 50% of the principal building face, which may be reduced to 4.5 metres for a rear deck.
3. Council instruct staff to resolve the following issues prior to final adoption:
  - (a) ensure that all engineering requirements and issues including restrictive covenants, dedications, and rights-of-way where necessary, are addressed to the satisfaction of the General Manager, Engineering;
  - (b) submission of a subdivision layout to the satisfaction of the Approving Officer;
  - (c) approval from the Ministry of Transportation & Infrastructure;
  - (d) submission of a finalized tree survey and a statement regarding tree preservation to the satisfaction of the City Landscape Architect;
  - (e) the applicant satisfy the deficiency in tree replacement on the site, to the satisfaction of the Planning and Development Department;
  - (f) the applicant adequately address the City's needs with respect to the City's Affordable Housing Strategy, to the satisfaction of the General Manager, Planning & Development Services; and
  - (g) demolition of existing buildings and structures to the satisfaction of the Planning and Development Department.
4. Council pass a resolution to amend the King George Highway Corridor Local Area Plan to redesignate the site from "Mobile Home Park" to "Single Family Small Lots" when the project is considered for final adoption.

**SITE CONTEXT & BACKGROUND**

Direction	Existing Use	NCP Designation	Existing Zone
Subject Site	Single Family Residential	Mobile Home Park	RF and CD (By-law No. 15162)
North (Across 24 Avenue):	Townhouses	Townhouse 15 (u.p.a.)	RM-15
East:	Single Family Residential	Mobile Home Park	CD (By-law No. 14141)
South:	Single Family Residential	Mobile Home Park & Single Family Small Lots	RF, CD (By-law No. 15162), and RF-12
West (Across 157 Street):	Single Family Residential	Single Family Small Lots	RF-9 and RF-12

**Context & Background**

- The subject site consists of the property at 2388 – 157 Street and the rear portion of 2371 - 157A Street. 2388 - 157 Street is situated at the southeast corner of 24 Avenue and 157 Street and is 957 square metres in area. 2371 – 157A Street is immediately to the east of 2388 – 157 Street and is 459 square metres in area.
- Both properties are designated "Urban" in the Official Community Plan (OCP) and are designated "Mobile Home Park" within the King George Highway Corridor Local Area Plan (LAP).
- 2371 – 157A Street was created through Development Application No. 7903-0229-00, which rezoned the parent property from "Single Family Residential Zone (RF)" to Comprehensive Development Zone (CD By-law No. 15162)", to allow subdivision into four single family small lots.
- Under Development Application No. 7903-0229-00, a 1.9 metre wide by 47.7 metre long strip of land at the rear of 2371 - 157A Street was protected through a Section 219 Covenant, for no-build to ensure that this strip of land was available to facilitate the future subdivision of 2388 – 157 Street. For this reason, 2371 - 157A Street is part of the current development application.
- The existing single family dwelling on 2371 – 157A Street is proposed to be retained. Three new lots fronting 157 Street are proposed as part of the subdivision application (Appendix I).

## DEVELOPMENT PROPOSAL

### Planning Considerations

- The applicant is proposing to rezone the property at 2388 – 157 Street from "Single Family Residential Zone (RF)" to "Single Family Residential (12) Zone (RF-12)" and the rear portion of the property at 2371 – 157A Street from "Comprehensive Development Zone (CD By-law No. 15162)" to "Single Family Residential (12) Zone (RF-12)", and to amend the King George Highway Corridor Local Area Plan (LAP) designation for the site from "Mobile Home Park" to "Single Family Small Lots" to allow subdivision from two lots into four lots.
- Three lots will be created along 157 Street and the lot along 157A Street will be retained in its current configuration with a slightly reduced lot size.

	Proposed
<b>Lot Area</b>	
Gross Site Area:	1,416 square metres
Road Dedication:	34 square metres
Undevelopable Area:	n/a
Net Site Area:	1,382 square metres
<b>Number of Lots:</b>	4
<b>Unit Density:</b>	28 units per hectare
<b>Range of Lot Sizes</b>	320 square metres – 375 square metres
<b>Range of Lot Widths</b>	9 metres – 17.2 metres
<b>Range of Lot Depths</b>	22 metres – 26 metres

### Referrals

Engineering: The Engineering Department has no objection to the project subject to the completion of Engineering servicing requirements as outlined in Appendix III.

School District: The School District has provided the following projections for the number of students from this development:

2 Elementary students at Jessie Lee Elementary School  
1 Secondary students at Earl Marriott Secondary School

(Appendix IV)

The applicant has advised that the dwelling units in this project are expected to be constructed and ready for occupancy by Fall 2021.

Ministry of Transportation & Infrastructure (MOTI): MOTI has no objection to the proposal.

## Transportation Considerations

- The three lots along 157 Street will be accessed from 157 Street.
- The lot along 157A Street will retain its current access.
- The east side of 157 Street will be upgraded to the local road standard.
- A 1.5 metre wide strip of road dedication will be required to achieve the ultimate 30 metre road allowance for 24 Avenue.

## POLICY & BYLAW CONSIDERATIONS

### Regional Growth Strategy

- The site is designated "General Urban" in the Regional Growth Strategy (RGS).
- General Urban areas are intended for residential neighbourhoods.
- The proposed single family residential development complies with the RGS designation for the site.

### Official Community Plan

#### Land Use Designation

- The site is designated "Urban" in the Official Community Plan (OCP).
- The "Urban" land use designation is intended to support low and medium density residential neighbourhoods with forms including detached, semi-detached, and ground-oriented attached housing. Densities may range up to 36 units per hectare outside of secondary plan areas and 72 units per hectare within secondary plan areas.
- The proposal complies with the OCP designation for the site.

#### Themes/Policies

- The proposed single family residential subdivision is supported by the following OCP policy:
  - A3.5 – Support infill development that is appropriate in scale and density to its neighbourhood context that uses compatible design to reinforce neighbourhood character.

*(The proposed development density is consistent with the single family small lots that have been developed in the immediate area within the past 20 years. The proposed single family small lot form offers an appropriate transition between the existing manufactured home park to the south and the existing low-density townhomes located to the north across 24 Avenue.*

*The applicant completed a design guideline for the proposed subdivision (summarized later in this report), which was based upon a character study of the existing neighbourhood, ensuring new single family dwellings are of consistent or complementary design to the existing neighbourhood).*

## **Secondary Plans**

### Land Use Designation

- The site is designated "Mobile Home Park" in the King George Highway Corridor LAP.

### Amendment Rationale

- The applicant is proposing to redesignate the site to "Single Family Small Lots" within the King George Highway Corridor Plan.
- The subject site and surrounding properties were originally designated for a new manufactured home park when the King George Highway Corridor LAP was adopted in 1995. This area was bound by 24 Avenue, Highway 99 and an existing manufactured home park, and was considered appropriate for a new manufactured home park.
- Development Application No. 7900-0154-00, to allow the development of 18 single family small lots, was approved at the Regular Council – Land Use meeting of April 23, 2001. The area approved for development into single family small lots represented a significant portion of the area designated for a new manufactured home park. As such a new manufactured home park was never developed in accordance with the King George Highway Corridor Plan and development of the area into a manufactured home park is no longer feasible.
- Since the introduction of single family small lots into the area through Development Application No. 7900-0154-00, four other development proposals for single family small lots has been approved by council (Development application Nos. 7902-0354-00, 7903-0229-00, 7910-0267-00, 7916-0308-00).
- Single family small lots has become the primary development pattern in this area as a result of these development applications. The proposed LAP amendment is consistent with previously approved amendments.
- The proposed development will not be subject to the Tier 2 Capital Plan Project CACs, as described in the Community Amenity Contribution section of this report. While an amendment to the King George Highway Corridor LAP is proposed, the redesignation of the site will not result in a residential density that is greater than what is already identified in the approved plan, as confirmed by the City's Realty Division.

## **Zoning Bylaw**

- The applicant proposes to rezone the subject site from "Single Family Residential Zone (RF)" and "Comprehensive Development Zone (CD By-law No. 15162)" to "Single Family Residential (12) Zone (RF-12)".

- The table below provides an analysis of the development proposal in relation to the requirements of the Zoning Bylaw, including the "Single Family Residential (12) Zone (RF-12)" and parking requirements.

<b>RF-12 Zone (Part 17A)</b>	<b>Permitted and/or Required</b>	<b>Proposed</b>
<b>Unit Density:</b>		
<b>Yards and Setbacks</b>		
Front Yard:	6.0 metres	6.0 metres
Side Yard:	1.2 metres	1.2 metres
Side Yard Flanking:	2.4 metres	2.4 metres
Rear:	7.5 metres*	5.0 metres
<b>Lot Size – RF-12 Type II</b>		
Lot Size:	Interior Lot: 320 square metres Corner Lot: 375 square metres	Interior Lot: 320 square metres Corner Lot: 375 square metres
Lot Width:	Interior Lot: 13.4 metres Corner Lot: 15.4 metres	Interior Lot: 14.5 metres Corner Lot: 17.2 metres
Lot Depth:	Interior Lot: 22 metres Corner Lot: 22 metres	Interior Lot: 22 metres Corner Lot: 22 metres
<b>Parking (Part 5)</b>	<b>Required</b>	<b>Proposed</b>
<b>Number of Spaces</b>	Single Family Dwelling: 3 parking spaces	4 parking spaces

\*The rear yard setback of a principal building may be reduced to 6.0 metres for a maximum of 50% of the width of the rear of the principal building. The rear yard setback may also be reduced from 7.5 metres to 4.5 metres for a deck.

- Corporate Report 2016-R158 introduced the Single Family Residential (13) Zone (RF-13) to Surrey Zoning By-law No. 12000. The Corporate Report states that, "new applications would be supported by staff under the RF-13 provisions, except in limited circumstances."
- As the proposal is completing a subdivision concept that was established by Development Application No. 7903-0229-00, and the site area does not conform to the minimum area requirements of the RF-13 Zone, rezoning to the RF-12 zone is appropriate in this circumstance.

#### Setback Variances

- The applicant is requesting the following variances:
  - to reduce the minimum rear yard setback of the RF-12 Zone on proposed Lots 1 and 2 from 7.5 metres to 5.0 metres for 50% of the width of the principal building face, while maintaining the rear yard setback for the remaining 50% of the rear building face at 7.5 metres which may be reduced to 4.5 metres for a rear deck; and
  - to reduce the minimum rear yard setback of the RF-12 Zone on proposed Lot 3 from 7.5 metres to 5.0 metres for 50% of the principal building face and from 7.5 metres to 6.0 metres for the remaining 50% of the principal building face, which may be reduced to 4.5 metres for a rear deck.



- The rear yard setback for the principal building in the RF-12 Zone is 7.5 metres. However, the rear yard setback for 50% of the building face may be reduced to 6.0 metres. The setback may be further reduced to 4.5 metres to accommodate a rear deck. The applicant has proposed to reduce the rear yard setback for 50% of the building face, on all proposed lots, to 5.0 metres in order to improve the functionality of the floor plan.
- Mike Tynan of Tynan Consulting, who has been retained as the design consultant for the project, has indicated that 22 metre deep lots result in shallow room depths of 3.5 metres, for the area located behind the garage. Mike Tynan submits that 3.5 metre deep rooms are not deep enough for either a great room or a kitchen.
- Increasing the room depth by 1 metre will allow increased flexibility in the home design and allow for a more functional layout.
- A 1.5 metre wide drainage easement adjacent to the south property line of proposed Lot 3 is proposed as part of the servicing concept for the application.
- The 1.5 metre easement, exceeds the 1.2 metre setback of the RF-12 Zone. Furthermore, the easement will be open-to-sky, meaning no overhangs or other protrusions are allowed within the easement, including roof overhangs. Mike Tynan has indicated that the dwelling's foundation must have an increased setback from the south lot line to achieve a reasonable roof overhang. As such, the applicant has proposed to vary the other 50% of the rear building face of the principal dwelling on Lot 3 from 7.5 metres to 6.0 metres.
- The proposed variances to the rear yard setbacks on Lots 1 and 2 will have minimal impact on the privacy for the neighbours to the east. The dwellings on proposed Lots 1 and 2 will have an indirect interface with the neighbouring dwellings. The distance between the dwelling on proposed Lot 1 and the dwelling at 2377 – 157A street will exceed the combined rear yard setbacks of the RF-13 Zone. The distance between the dwellings on proposed Lot 2 and 2371 - 157A street will average 14 metres, equivalent to two seven metre setbacks.
- Proposed Lot 3 will be directly interfacing with the dwelling at 2363 – 157A Street. To mitigate this direct interface, the Design Consultant recommends requiring a 1.8 metre fence at the property line and the prohibition of upper floor balconies.
- Staff support the requested variances to proceed for consideration.

#### Lot Grading and Building Scheme

- The applicant retained Mike Tynan of Tynan Consulting as the Design Consultant. The Design Consultant conducted a character study of the surrounding homes and based on the findings of the study, proposed a set of building design guidelines (Appendix VI.).

- The Character Study states that the context homes in the area have a "Neo-Heritage" theme. New single family dwellings constructed as part of the proposed development will be designed in the following styles: "Traditional", "Heritage", "Neo-Traditional", "Neo-Heritage", or other compatible styles with appropriate transitions in massing and character, as determined by the design consultant.
- A preliminary lot grading plan, submitted by Hub Engineering Inc, and dated December 16, 2019, has been reviewed by staff and found to be generally acceptable. The applicant has proposed in-ground basements on all lots. The feasibility of in-ground basements will be confirmed once the City's Engineering Department has reviewed and accepted the applicant's final engineering drawings.

### **Capital Projects Community Amenity Contributions (CACs)**

- On December 16, 2019, Council approved the City's Community Amenity Contribution and Density Bonus Program Update (Corporate Report No. R224; 2019). The intent of that report was to introduce a new City-wide Community Amenity Contribution (CAC) and updated Density Bonus Policy to offset the impacts of growth from development and to provide additional funding for community capital projects identified in the City's Annual Five-Year Capital Financial Plan.
- The proposed development will be subject to the Tier 1 Capital Plan Project CACs and will provide \$2,000/unit if final adoption of the Rezoning By-law is approved by December 31, 2020. The contribution rates will be introduced based on a three-phase schedule, with rates increasing as of January 1, 2021. The proposed development will be required to pay the rates that are applicable at the time of Final Adoption.
- While an amendment to the King George Highway Corridor LAP is proposed, the proposed redesignation of the site from "Mobile Home Park" to "Single Family Small Lots" will not result in lift in residential density on the site, as confirmed by the City's Realty Division.
- The proposed development will not be subject to the Tier 2 Capital Plan Project CACs and only Tier 1 Capital Plan Project CACs will apply.

### **Affordable Housing Strategy**

- On April 9, 2018, Council approved the City's Affordable Housing Strategy (Corporate Report No. R066; 2018) requiring that all new rezoning applications for residential development contribute \$1,000 per unit to support the development of new affordable housing. The funds collected through the Affordable Housing Contribution will be used to purchase land for new affordable rental housing projects.
- The applicant will be required to contribute \$1,000 per lot to support the development of new affordable housing.

## PUBLIC ENGAGEMENT

- Pre-notification letters were sent on December 12, 2019, and the Development Proposal Signs were installed on December 18, 2019. Staff received four responses from neighbouring residents, who provided the following comments (*staff comments in italics*):

Residents expressed concern regarding traffic circulation and pedestrian safety:

*(As a condition of the proposed rezoning, the applicant will be required to complete road frontage upgrades along 157 Street. This will include pavement widening, installation of street lights, and sidewalk construction.*

*The intersection of 157 Street and 24 Avenue is currently unsignalized and permits both a left in and left out movement for 157 Street. Residents along 157 Street and Cranley Drive, south of 24 Ave, are quite limited in road network connectivity. It is acknowledged that left turns from 157 Street to 156 Street are not ideal, however, any restrictions to movements at this intersection would further reduce connectivity, and time restrictions enforced through signage may not be successful. Ultimately the connection of 23A Avenue from 157 Street to 156 Street (to be delivered through future development, an undetermined timeline) will provide connectivity to the signalized intersection at 156 Street and 24 Avenue).*

Residents also raised concerns regarding pedestrian safety at the intersection of 156 Street and 24 Avenue.

*(To promote safer, more efficient and comfortable pedestrian experience, the City initiated a project in 2016 where signal timing practices were modified at select intersections to be less auto oriented and cater to pedestrian movements. The project was focused on intersections within City Centre and around the Semiahmoo Mall in South Surrey where the highest sustained pedestrian demand exists. At this point, Staff have expanded the implementation of these features to many other intersections across the City.*

*One of these features that exist at this intersection is resting in the "Walk" indication along 24 Avenue for the entire duration of the green signal, regardless of the pushbutton being pressed. The intent is to improve pedestrian service time, comfort and compliance, while also increasing the awareness of drivers that pedestrians have equal, if not higher, priority at signalized intersections. In the upcoming weeks staff will also modify the pedestrian operation for the signal indications along 156 Street (crossing to the north or south). The change will ensure that any time the vehicle signals turn green for 156 Street, that the pedestrian "Walk" will be activated automatically as well.*

*In 2022 the City will be constructing a northbound right turn lane at this intersection to reduce the current congestion and frustration drivers are experiencing making the turn. With this geometric improvement, staff will also look to reduce the length of the crosswalk, to minimize pedestrian exposure in the roadway and improve sight lines between drivers and pedestrians. Following these changes, staff will continue to monitor the operation and make further enhancements as justified to improve road safety for all users).*

- Residents expressed concerns regarding on-street parking capacity along 157 Street and Cranley Drive, requesting that proposed development be able to meet the parking demand on-site.

*(All proposed lots will be exceeding the parking requirements of the Zoning By-law, as only 3 spaces are required, and 4 parking spaces will be provided. Staff will be re-evaluating on street parking for the block in the detailed engineering design phase of the project should Council support the application. It should be noted that safety and circulation are prioritized over the provision of on-street parking spaces).*

- The subject development application was reviewed by the Little Campbell Watershed Society (LCWS), who provided the following comments (*staff comments in italics*):

The LCWS commented that 100% infiltration through some sort of rain garden or detention methodology will be workable and employed in the proposal.

*(The proposed single family development site is within the Fergus Creek Integrated Stormwater Management Plan (ISMP), tributary to the Little Campbell, where there are specific on-lot mitigation requirements from the ISMP that are to be followed. ISMP recommendations, in conjunction with the City's drainage by-law for single family residential, is that all roof leaders are to be disconnected and discharge onto splash pads/pervious areas to allow for direct infiltration. An augmented layer of 450mm topsoil is typically the required standard to enhance volume capture with an additional requirement for infiltration systems to supplement topsoil performance. Infiltration systems can include rain gardens, infiltration swales with storage, surface or subsurface storage, underground galleries).*

The LCWS requested consideration be given for the proposal to have setbacks that protect for a six-lane road on 24 Avenue, to provide enhanced rapid public transit in the Grandview Heights/South Surrey area.

*(24 Avenue was widened to 5 lanes between 152 Street and Highway 99 (including the overpass of Highway 99) in the mid 2000's. At that time there was no transit service on 24 Avenue and no consideration of future rapid transit. The City established setbacks to build a 5 lane road; buildings along the corridor developed accordingly. Transit service was introduced on 24 Avenue in 2012 and subsequent plans to widen 24 Avenue east of Highway 99 acknowledged the need to plan for future rapid transit. Development has already occurred on 24 Avenue west of Highway 99 at the 5-lane standard, so it would be inconsistent for the City to request a special 6-lane set back for this particular development.*

*Moving forward, Surrey is working closely with TransLink on developing a long-range rapid transit vision for the city. In Fall 2019 Surrey submitted a long-range rapid transit vision to TransLink for inclusion in their update of the Regional Transportation Strategy – Transport 2050. Rapid transit on 24 Avenue between 152 Street and 200 Street was included in the vision. The timing and technology of rapid transit on 24 Avenue is yet to be determined. For the 5-lane section of 24 Avenue between 152 Street and Highway 99, planners in the future will decide whether it makes more sense to purchase properties on 24 Avenue to achieve the width required to maintain road capacity for cars and add rapid transit, or re-allocate existing car lanes to rapid transit).*

## TREES

- Cory Plester and Tim Vandenberg, ISA Certified Arborist of Mike Fadum and Associates prepared an Arborist Assessment for the subject property. The table below provides a summary of the tree retention and removal by tree species:

**Table 1: Summary of Tree Preservation by Tree Species:**

Tree Species	Existing	Remove	Retain
<b>Alder and Cottonwood Trees</b>			
Alder	6	6	0
Cottonwood	2	2	0
<b>Deciduous Trees (excluding Alder and Cottonwood Trees)</b>			
Cherry	1	1	0

Tree Species	Existing	Remove	Retain
Purple Leaf Plum	1	1	0
Plum	1	1	0
Paper Birch	1	1	0
Weeping Willow	2	2	0
Japanese Snowbell	1	0	1
<b>Coniferous Trees</b>			
Douglas Fir	1	0	1
Austrian Pine	1	1	0
Western Red Cedar	2	2	0
<b>Total (excluding Alder and Cottonwood Trees)</b>	<b>11</b>	<b>9</b>	<b>2</b>
<b>Total Replacement Trees Proposed (excluding Boulevard Street Trees)</b>			
		<b>3</b>	
<b>Total Retained and Replacement Trees</b>			
		<b>5</b>	
<b>Contribution to the Green City Program</b>			
		<b>\$9,200</b>	

- The Arborist Assessment states that there are a total of 11 mature trees on the site, excluding Alder and Cottonwood trees. 8 existing trees, approximately 42% of the total trees on the site, are Alder and Cottonwood trees. It was determined that two trees can be retained as part of this development proposal. The proposed tree retention was assessed taking into consideration the location of services, building footprints, road dedication and proposed lot grading.
- For those trees that cannot be retained, the applicant will be required to plant trees on a 1 to 1 replacement ratio for Alder and Cottonwood trees, and a 2 to 1 replacement ratio for all other trees. This will require a total of 26 replacement trees on the site. Since only 3 replacement trees can be accommodated on the site (based on an average of 1 tree per lot), the deficit of 23 replacement trees will require a cash-in-lieu payment of \$9,200, representing \$400 per tree, to the Green City Program, in accordance with the City's Tree Protection Bylaw.
- In summary, a total of five trees are proposed to be retained or replaced on the site with a contribution of \$9,200 to the Green City Program.

**INFORMATION ATTACHED TO THIS REPORT**

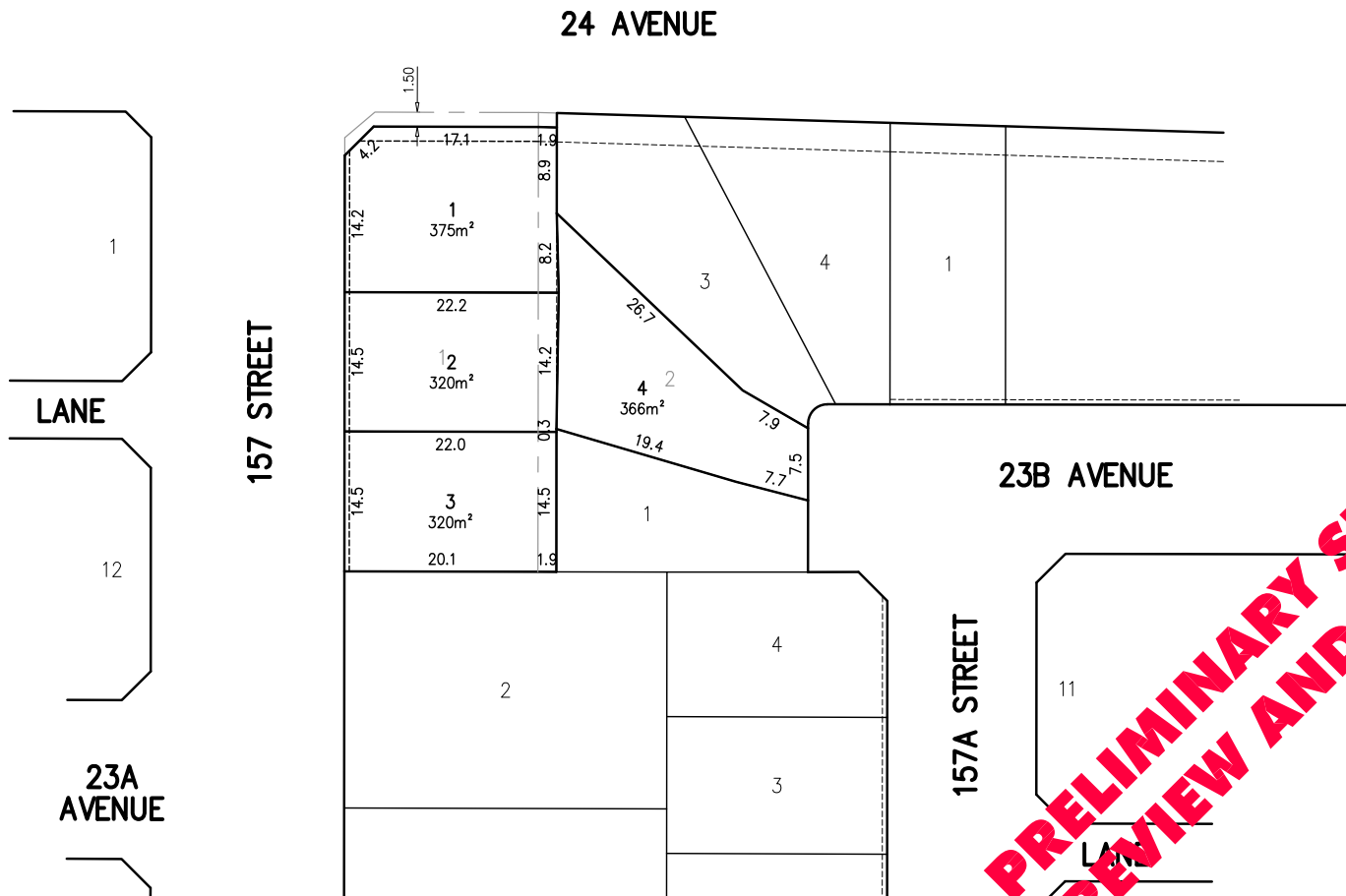
The following information is attached to this Report:

- Appendix I. Site Plan
- Appendix II. Block Plan
- Appendix III. Engineering Summary
- Appendix IV. School District Comments
- Appendix V. Summary of Tree Survey and Tree Preservation
- Appendix VI. Summary of Building Design Guidelines
- Appendix VII. Development Variance Permit No. 7919-0305-00

*approved by Ron Hintsche*

Jean Lamontagne  
General Manager  
Planning and Development

BD/cm



**PRELIMINARY SUBJECT TO REVIEW AND APPROVAL**

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CLIENT:	PROJECT: 2388 157 STREET, SURREY		
DRAWING TITLE: RESIDENTIAL SUBDIVISION			
PROJECT No. 19041	DATE: FEB 2020	LEGAL:	MUNICIPAL PROJECT No:
SCALE: 1:500			
PRELIMINARY PLAN - SUBJECT TO APPROVAL(S) FROM FEDERAL, PROVINCIAL AND LOCAL AUTHORITIES			

**Hub Engineering Inc.**  
 Engineering and Development Consultants

Member  
**PACIFIC LAND GROUP**

Suite 212, 12992 - 76 Avenue, Surrey, B.C. V3W 2V6  
 tel: 604-572-4328 | fax: 604-501-1625 | mail@hub-inc.com | www.hub-inc.com







## INTER-OFFICE MEMO

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TO: **Manager, Area Planning & Development  
- South Surrey Division  
Planning and Development Department**

FROM: **Development Services Manager, Engineering Department**

DATE: **Feb 18, 2020** PROJECT FILE: **7819-0305-00**

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RE: **Engineering Requirements  
Location: 2388 157 Street & 2371 157A Street**

**REZONE AND SUBDIVISION*****Property and Right-of-Way Requirements***

- Dedicate 1.5 m along 24 Avenue for the ultimate Arterial Road Standard;
- Dedicate a 3.0 m x 3.0 m corner cut at the intersection of 24 Avenue and 157 Street; and
- Register a 0.5 m Statutory Right-of-Way along 24 Avenue and 157 Street.

***Works and Services***

- Construct the east side of 157 Street to the Local Road Standard;
- Construct a curb bulge at the intersection of 24 Avenue and 157 Street;
- Construct a 6.0 m wide concrete letdown to each lot;
- Construct a storm main along 157 Street to service the development;
- Remove existing driveway letdown along 157 Street;
- Provide a video inspection of the existing service connections to 2371 – 157A Street;
- Provide a sanitary, storm and water connection to each lot; and
- Provide on-lot stormwater mitigation features to meet the Fergus Creek Integrated Storm Management Plan (ISMP).

A Servicing Agreement is required prior to Rezone and Subdivision.

**DEVELOPMENT VARIANCE PERMIT**

There are no engineering requirements relative to issuance of the Development Permit/  
Development Variance Permit.

Tommy Buchmann, P.Eng.  
Development Services Manager

SC



Planning December 10, 2019

**THE IMPACT ON SCHOOLS**

APPLICATION #: 19 0305 00

**SUMMARY**

The proposed 4 Single family with suites are estimated to have the following impact on the following schools:

Projected # of students for this development:

Elementary Students:	2
Secondary Students:	1

September 2019 Enrolment/School Capacity

<b>Jessie Lee Elementary</b>	
Enrolment (K/1-7):	58 K + 351
Operating Capacity (K/1-7)	38 K + 372
<b>Earl Marriott Secondary</b>	
Enrolment (8-12):	1902
Capacity (8-12):	1500

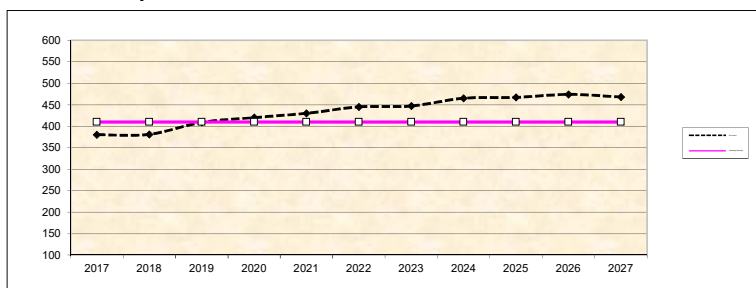
**School Enrolment Projections and Planning Update:**

The following tables illustrate the enrolment projections (with current/approved ministry capacity) for the elementary and secondary schools serving the proposed development.

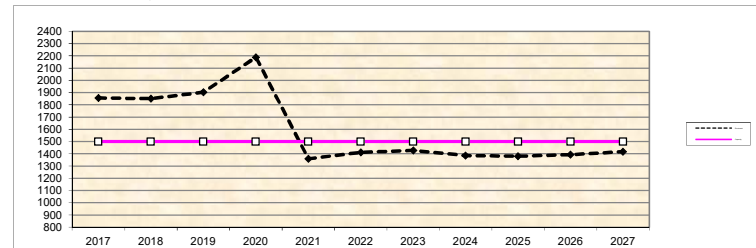
As of September 2019, actual enrollment to Jessie Lee Elementary has grown modestly this year; and, registration this year filled the school to 100% operating capacity. The 10 year projections to show a modest growth trend. It is being projected that next year the school may require its first portable to deal with growth. However, the growth trend to date is not extreme enough to warrant an addition as growth over the next 10 years can be managed with 4 portables or less. Currently there are no capital expansion project requests for Jessie Lee.

A new high school in the Grandview area is currently in construction and is targeted to open September 2021. This new facility will relieve the overcrowding at Earl Marriot Secondary. When the new secondary school opens, newly adopted boundary changes will come into effect. At this time, Jessie Lee will no longer feed two secondary schools but will only feed Earl Marriott.

**Jessie Lee Elementary**



**Earl Marriott Secondary**



\* Nominal Capacity is estimated by multiplying the number of enrolling spaces by 25 students. Maximum operating capacity is estimated by multiplying the number of enrolling spaces by 27 students.

**Table 1: Summary of Tree Preservation by Tree Species:**

Tree Species	Existing	Remove	Retain
<b>Alder and Cottonwood Trees</b>			
Alder/Cottonwood	8	8	0
<b>Deciduous Trees</b>			
Cherry sp.	1	1	0
Plum, Purple leaf	1	1	0
Plum sp.	1	1	0
Birch, Paper	1	1	0
Willow, Weeping	2	2	0
Snowbell, Japanese	1	0	1
<b>Coniferous Trees</b>			
Pine, Austrian	1	1	0
Cedar, Western Redcedar	2	2	0
Douglas-fir	1	0	1
<b>Total (Not including Alder and Cottonwood)</b>	11	9	2
<b>Additional Trees in the proposed Open Space / Riparian Area</b>	NA	NA	NA
<b>Total Replacement Trees Proposed (excluding Boulevard Street Trees)</b>		3	
<b>Total Retained and Replacement Trees</b>		5	

**\*TOTALS DO NOT INCLUDE OFFSITE TREES**



## Tree Preservation Summary

Surrey Project No: TBD

Address: 2388 157 Street, 2371 157A Street Surrey, BC

Registered Arborist: Corey Plester #PN-8523A

On-Site Trees	Number of Trees
<b>Protected Trees Identified</b> (on-site and shared trees, including trees within boulevards and proposed streets and lanes, but excluding trees in proposed open space or riparian areas)	19
<b>Protected Trees to be Removed</b>	17
<b>Protected Trees to be Retained</b> (excluding trees within proposed open space or riparian areas)	2
<b>Total Replacement Trees Required:</b>  <ul style="list-style-type: none"> <li>- Alder &amp; Cottonwood Trees Requiring 1 to 1 Replacement Ratio 8 X one (1) = <u>8</u></li> <li>- All other Trees Requiring 2 to 1 Replacement Ratio 9 X two (2) = <u>18</u></li> </ul>	26
<b>Replacement Trees Proposed</b>	3
<b>Replacement Trees in Deficit</b>	23
<b>Protected Trees to be Retained in Proposed [Open Space / Riparian Areas]</b>	NA

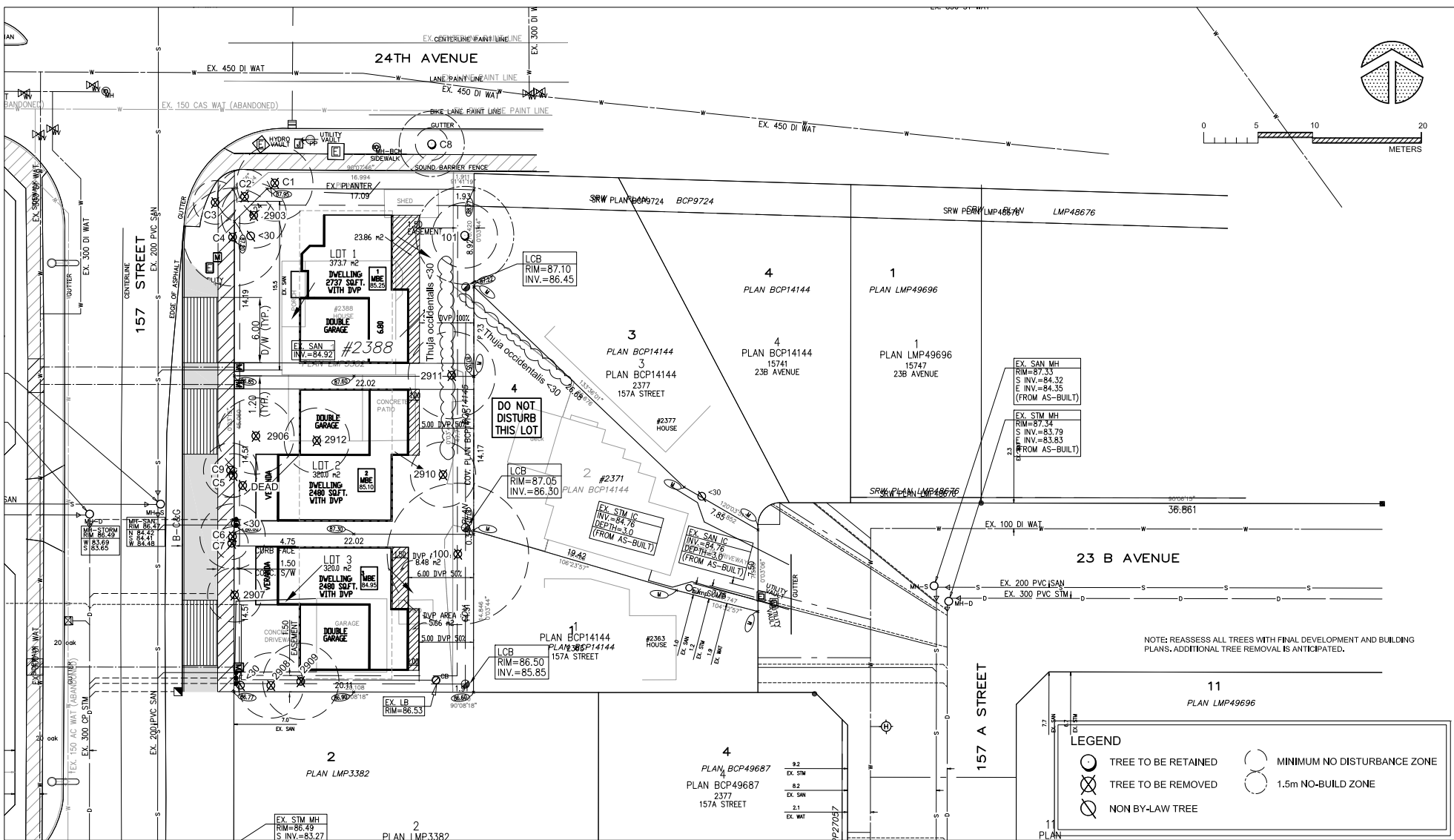
Off-Site Trees	Number of Trees
<b>Protected Off-Site Trees to be Removed</b>	0
<b>Total Replacement Trees Required:</b>  <ul style="list-style-type: none"> <li>- Alder &amp; Cottonwood Trees Requiring 1 to 1 Replacement Ratio 0 X one (1) = 0</li> <li>- All other Trees Requiring 2 to 1 Replacement Ratio 0 X two (2) = 0</li> </ul>	0
<b>Replacement Trees Proposed</b>	N/A
<b>Replacement Trees in Deficit</b>	N/A

Summary report and plan prepared and submitted by: Mike Fadum and Associates Ltd.

Signature of Arborist:

Date: October 22, 2019





NOTE: REASSESS ALL TREES WITH FINAL DEVELOPMENT AND BUILDING PLANS. ADDITIONAL TREE REMOVAL IS ANTICIPATED.

**LEGEND**

- TREE TO BE RETAINED
- TREE TO BE REMOVED
- NON-BY-LAW TREE
- MINIMUM NO DISTURBANCE ZONE
- 1.5m NO-BUILD ZONE

NO.	DATE	BY	REVISION
1	OCT21/19	MK	GRADING PLAN & SITE PLAN
2	OCT22/19	MK	GRADING PLAN

STAMP

**MIKE FADUM AND ASSOCIATES LTD.**  
**VEGETATION CONSULTANTS**

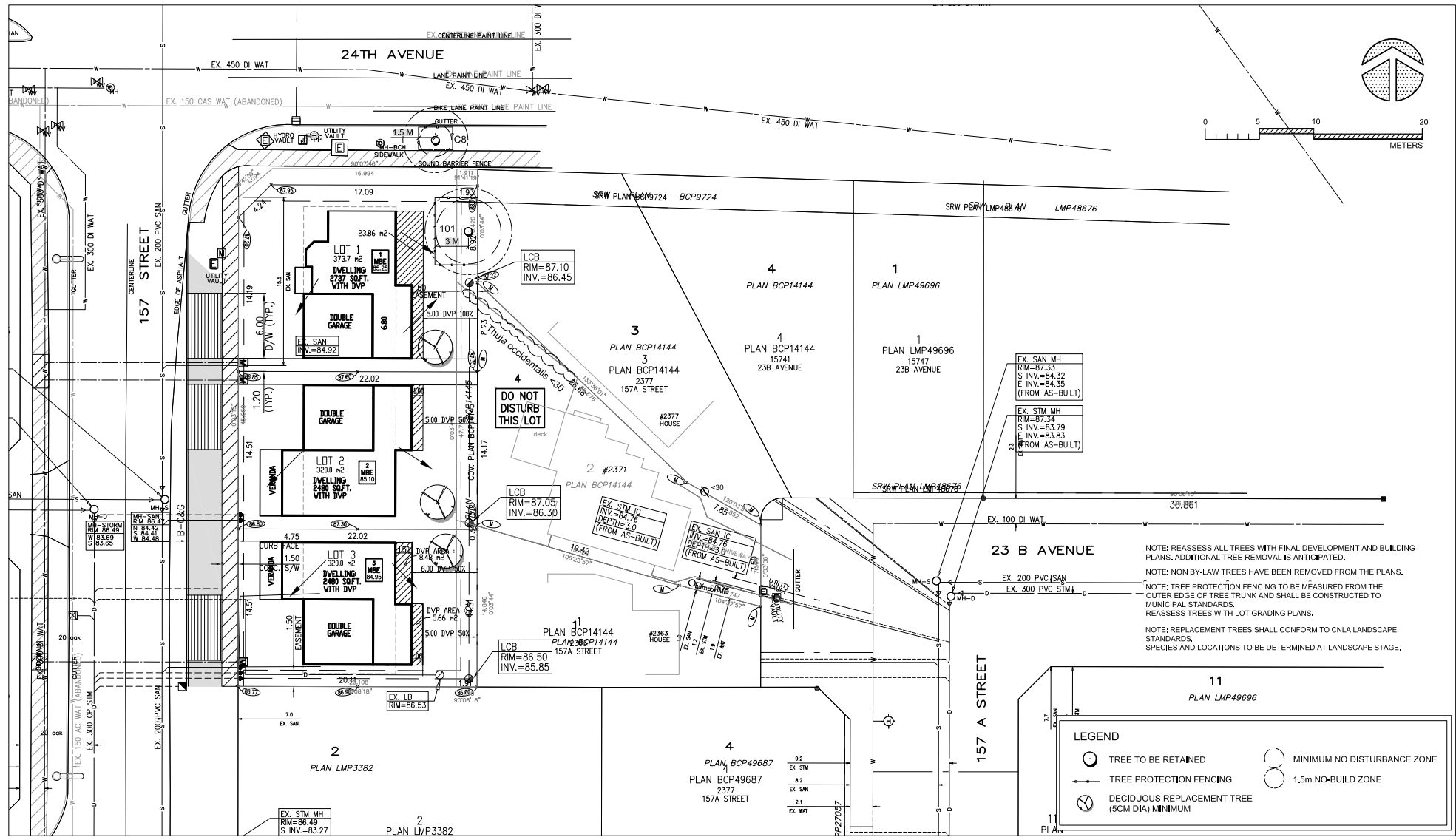
#105, 8277 129 St.  
 Surrey, British Columbia  
 V3W 0A6  
 Ph: (778) 593-0300  
 Fax: (778) 593-0302  
 Email: mfadum@fadum.ca

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PROJECT TITLE  
**2388 - 157 STREET**  
 SURREY, B.C.

SHEET TITLE  
**T1 - TREE REMOVAL AND PRESERVATION PLAN**  
 CLIENT  
 \_\_\_\_\_

DRAWN MK  
 SCALE AS SHOWN  
 DATE AUGUST 8, 2019  
**T-1**  
 SHEET 1 OF 2



NOTE: REASSESS ALL TREES WITH FINAL DEVELOPMENT AND BUILDING PLANS, ADDITIONAL TREE REMOVAL IS ANTICIPATED.  
 NOTE: NON-BY-LAW TREES HAVE BEEN REMOVED FROM THE PLANS.  
 NOTE: TREE PROTECTION FENCING TO BE MEASURED FROM THE OUTER EDGE OF TREE TRUNK AND SHALL BE CONSTRUCTED TO MUNICIPAL STANDARDS. REASSESS TREES WITH LOT GRADING PLANS.  
 NOTE: REPLACEMENT TREES SHALL CONFORM TO CNLA LANDSCAPE STANDARDS. SPECIES AND LOCATIONS TO BE DETERMINED AT LANDSCAPE STAGE.

**LEGEND**

- TREE TO BE RETAINED
- TREE PROTECTION FENCING
- DECIDUOUS REPLACEMENT TREE (5CM DIA) MINIMUM
- MINIMUM NO DISTURBANCE ZONE
- 1.5m NO-BUILD ZONE

NO.	DATE	BY	REVISION
1	OCT21/19	MK	GRADING PLAN & SITE PLAN
2	OCT22/19	MK	GRADING PLAN

**MIKE FADUM AND ASSOCIATES LTD.**  
**VEGETATION CONSULTANTS**

#105, 8277 129 St  
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 V3V 0A6  
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PROJECT TITLE  
**2388 - 157 STREET**  
 SURREY, B.C.

SHEET TITLE  
**T2 - TREE PROTECTION PLAN**

CLIENT

DRAWN MK  
 SCALE AS SHOWN  
 DATE AUGUST 8, 2019

**T-2**  
 SHEET 2 OF 2

# BUILDING GUIDELINES SUMMARY

Surrey Project no: 19-0305-00  
Project Location: 2388 - 157 Street, Surrey, B.C.  
Design Consultant: Tynan Consulting Ltd., (Michael E. Tynan)

**The draft Building Scheme proposed for this Project has been filed with the City Clerk. The following is a summary of the Residential Character Study and the Design Guidelines which highlight the important features and form the basis of the draft Building Scheme.**

## 1. Residential Character

### 1.1 General Description of the Existing and/or Emerging Residential Character of the Subject Site:

The subject site is located within a mixed use area that includes the following zones within a one block radius; RM-D, RF-9, RF-12, RF, PA-1, C-5, RM-15, and RM-M. Densities in the surrounding residential zones are high.

North of the subject site (on the opposite side of 24 Avenue) is a 44 unit Townhouse site developed under Surrey project 93-0186-00. The structures are two storeys high and are substantially concealed by a dense, well kept coniferous hedge positioned above a stone retaining wall with dense mature vegetation behind. The structures have all-common hip roofs at an 8:12 slope with an asphalt shingle surface. The units are clad in horizontal vinyl siding in natural tones with white trim. The townhouses are compatible with the surrounding single family dwellings.

Three lots south of the subject site (south of Cranley Drive) is a massive RM-M zoned development "La Mesa", comprising hundreds of units. The units are partially visible through the entrance at the intersection of 157 Street and Cranley Drive, but are otherwise obscured by a dense, well kept coniferous hedge.

West of the subject site is a 16 lot site containing seven RF-9 homes with rear lanes, fronting 24 Avenue, and nine RF-12 zone homes, all developed under Surrey project 02-0354-00. These homes are all readily identifiable as "Neo-Heritage" style. Homes are Two-Storey type, ranging in size from 1700 sq.ft. (the RF-9 homes) to 2800 sq.ft. (the RF-12 homes). The homes have an 8:12 pitch main common gable or common hip roof with several street facing common gable projections; many articulated with wood shakes, layered fascia, and decorative wood braces. Roofs are surfaced with shake profile asphalt shingles. Massing designs are low to mid-scale, exhibiting architecturally interesting, proportionally consistent, well balanced feature projections. All homes have a single storey high (human scale) front entrance porch/veranda. Homes are clad in vinyl and most have substantial wood feature walls and gable ends and bold window and door trim. Most homes have a natural hue wall colour with neutral (white) trim colour scheme. Landscapes are average or better with respect to both the quality and quantity of plantings. Driveways are exposed aggregate. Overall, the homes present a pleasing themed appearance, and provide good context for the subject site.

## 1.2 Features of Surrounding Dwellings Significant to the Proposed Building Scheme:

- 1) **Context Homes:** The character of this area has been clearly defined by the new and aesthetically desirable housing stock. This is an infill situation in which new homes at the subject site should be similar in theme, representation, and character with the existing homes.
- 2) **Style Character :** The surrounding homes are best described as "Neo-Heritage" style, and there is substantial consistency / compatibility between them, resulting in an easily recognizable neighbourhood identity. Note that style range is not restricted in the building scheme. However, the consultant refers to the character study when reviewing plans for meeting style-character intent.
- 3) **Home Types :** All surrounding homes are Two-Storey type, and it is expected that all new homes constructed at the subject site will be Two-Storey type. However, home type (Two-Storey, Bungalow, Basement Entry, Split Level, etc..) will not be regulated in the building scheme.
- 4) **Massing Designs :** Massing designs should meet new standards for RF-12 zoned subdivisions. New homes should exhibit "mid-scale" massing. Various elements and projections on the front of the home should be interesting architecturally, and should be in pleasing natural proportions to one another. These elements and projections should be located so as to create balance across the façade.
- 5) **Front Entrance Design :** Front entrance porticos should be of a human scale, limited to a maximum height of one storey (10 feet) to ensure there is not proportional overstatement of this one element.
- 6) **Exterior Wall Cladding :** Although neighbouring homes have been constructed with horizontal vinyl siding, there has been a consistent trend over at least the past 15 years to require the use of cladding materials of a higher quality than vinyl throughout South Surrey. Vinyl is a low cost utility cladding material that is well suited to areas where affordability is an objective. This is not the case here, as all lots and new homes will be of high value. Vinyl therefore, is not recommended.
- 7) **Roof surface :** This is area in which all homes have asphalt shingle roofs. It is expected that most new homes will also have asphalt shingle roofs, and for continuity, asphalt shingles are recommended. A single cedar shingle or concrete tile roof would stand out as inconsistent due the large difference in textures (thickness) between asphalt shingles and cedar shingles or concrete tiles, and so these products are not recommended. However, where opportunities arise to introduce new environmentally sustainable products, they should be embraced. Generally, these materials have thicknesses between asphalt shingles and cedar shingles and will not appear out of place texturally. Therefore, to ensure consistency of character, only shake profile asphalt shingles and shake profile sustainable products are recommended. Where required by the BC Building Code for lower slope applications membrane roofing products can be permitted subject to consultant approval. Small decorative metal roofs should also be permitted.
- 8) **Roof Slope :** Roof slopes of 7:12 or higher have been used on context homes. This is a suitable minimum roof slope given the objectives of ensuring continuity with context homes and to ensure that homes appear style-authentic within the proposed style range.



**Streetscape:** North of the subject site is a 44 unit Townhouse project substantially concealed behind a dense coniferous hedge. The two storey high units, with 8:12 slope common hip roofs with asphalt shingle surfaces are pleasantly set in dense mature landscaping. South of the site is a massive RM-M site, also substantially concealed behind a dense hedge. West of the site are 16 homes, all "Neo-Heritage" style Two-Storey homes ranging in size between 1700 sq.ft. and 2800 sq.ft. The homes all have low to mid-scale massing designs in pleasing proportions, with one storey high entrance verandas/porches. Landscaping is average or better for the era of construction.

## 2. Proposed Design Guidelines

### 2.1 Specific Residential Character and Design Elements these Guidelines Attempt to Preserve and/or Create:

- the new homes are readily identifiable as one of the following styles: "Traditional", "Heritage", "Neo-Traditional", or "Neo-Heritage", or other compatible styles with appropriate transitions in massing and character, as determined by the design *consultant*. Note that the proposed style range is not contained within the building scheme, but is contained within the residential character study which forms the basis for interpreting building scheme regulations.
- a new single family dwelling *constructed* on any *lot* meets year 2017's design standards, which include the proportionally correct allotment of mass between various street facing elements, the overall balanced distribution of mass within the front facade, readily recognizable style-authentic design, and a high trim and detailing standard used specifically to reinforce the style objectives stated above.
- trim elements will include several of the following: furred out wood posts, articulated wood post bases, wood braces and brackets, louvered wood vents, bold wood window and door trim, highly detailed gable ends, wood dentil details, stone or brick feature accents, covered entrance verandas and other style-specific elements, all used to reinforce the style (i.e. not just decorative).
- the development is internally consistent in theme, representation, and character.
- the entrance element will be limited in height (relative dominance) to 10 feet, or one storey.

### 2.2 Proposed Design Solutions:

**Interfacing Treatment with existing dwellings)**

This is a "Neo-Heritage" themed area, in which all homes are considered to provide acceptable architectural context. New homes will have similar or better massing designs (equal or lesser massing scale, consistent proportionality between various elements, and balance of volume across the façade). New homes will have similar roofing materials. Wall cladding, feature veneers and trim treatments will meet or exceed standards found on the aforesaid context homes.

**Exterior Materials/Colours:**

Stucco, Cedar, Fibre-Cement Board, Brick, and Stone. Although vinyl is used on surrounding homes, vinyl siding is not recommended.

"Natural" colours such as browns, greens, clays, and other earth-tones, and "Neutral" colours such as grey, white, and cream are permitted. "Primary" colours in subdued tones such as navy blue, colonial red, or forest green can be considered providing neutral trim colours are used, and a comprehensive

colour scheme is approved by the consultant. "Warm" colours such as pink, rose, peach, salmon are not permitted. Trim colours: Shade variation of main colour, complementary, neutral, or subdued contrast only.

**Roof Pitch:** Minimum 7:12, with exceptions to prevent roof ridges from becoming too high (overshadowing of neighbouring lots), to allow for veranda roofs that do not cover upper floor windows, to allow for artistic expression in feature roofs, and to provide a path for exceptional designs with lower slope roofs to be approved subject to consultant approval.

**Roof Materials/Colours:** Only shake profile asphalt shingles with a raised ridge cap and new environmentally sustainable roofing products providing that aesthetic properties of the new materials are equal to or better than the traditional roofing products. Greys, browns, or black only. Membrane roofs also permitted where required by B.C. Building Code, and small metal feature roofs also permitted.

**In-ground basements:** In-ground basements are subject to determination that service invert locations are sufficiently below grade to permit a minimum 50 percent in-ground basement to be achieved. If achievable, basements will appear underground from the front.

**Treatment of Corner Lots:** Significant, readily identifiable architectural features are provided on both the front and flanking street sides of the dwelling, resulting in a home that architecturally addresses both streets. One-storey elements on the new home shall comprise a minimum of 40 percent of the width of the front and flanking street elevations of the single family dwelling. The upper floor is set back a minimum of 0.9 metres [3'- 0"] from the one-storey elements.

**Landscaping:** *Moderate modern urban standard:* Tree planting as specified on Tree Replacement Plan plus minimum 17 shrubs of a minimum 3 gallon pot size. Corner lots shall have an additional 8 shrubs of a minimum 3 gallon pot size, planted in the flanking street sideyard. Sod from street to face of home. Driveways: exposed aggregate, interlocking masonry pavers, stamped concrete, or coloured concrete in dark earth tones or medium to dark grey only.

**Compliance Deposit:** \$5,000.00

**Summary prepared and submitted by:** Tynan Consulting Ltd.      **Date:** November 25, 2019

**Reviewed and Approved by:**       **Date:** November 25, 2019

CITY OF SURREY

(the "City")

DEVELOPMENT VARIANCE PERMIT

NO.: 7919-0305-00

Issued To:

Address of Owner:

Issued To:

Address of Owner:

(collectively referred to as the "Owner")

1. This development variance permit is issued subject to compliance by the Owner with all statutes, by-laws, orders, regulations or agreements, except as specifically varied by this development variance permit.
2. This development variance permit applies to that real property including land with or without improvements located within the City of Surrey, with the legal description and civic address as follows:

Parcel Identifier: 017-691-893

Lot 1 Section 14 Township 1 New Westminster District Plan LMP3382

2388 - 157 Street

Parcel Identifier: 026-086-107

Lot 2 Section 14 Township 1 New Westminster District Plan BCP14144

2371 - 157A Street

(the "Land")

3. (a) As the legal description of the Land is to change, the City Clerk is directed to insert the new legal description for the Land once titles have been issued, as follows:

Parcel Identifier:

---

- (b) If the civic addresses change, the City Clerk is directed to insert the new civic addresses for the Land, as follows:
- 

4. Surrey Zoning By-law, 1993, No. 12000, as amended is varied as follows:

- (a) In Section F. Yards and Setbacks of Part 17A “Single Family Residential (12) Zone (RF-12)” the minimum rear yard setback for a principal building is reduced from 7.5 metres to 5.0 metres for 50% of the width of the principal building face, while maintaining the rear yard setback for the remaining 50% of the rear building face at 7.5 metres which may be reduced to 4.5 metres for a rear deck for proposed Lots 1 and 2; and

- (b) In Section F. Yards and Setbacks of Part 17A “Single Family Residential (12) Zone (RF-12)” the minimum rear yard setback for a principal building is reduced from 7.5 metres to 5.0 metres for 50% of the principal building face and from 7.5 metres to 6.0 metres for the remaining 50% of the principal building face which may be reduced to 4.5 metres for a rear deck for proposed Lot 3.

5. This development variance permit applies to only the portion the Land shown on Schedule A which is attached hereto and forms part of this development variance permit.

6. The Land shall be developed strictly in accordance with the terms and conditions and provisions of this development variance permit.

7. This development variance permit shall lapse if the Owner does not substantially start any construction with respect to which this development variance permit is issued, within two (2) years after the date this development variance permit is issued.

8. The terms of this development variance permit or any amendment to it, are binding on all persons who acquire an interest in the Land.

9. This development variance permit is not a building permit.

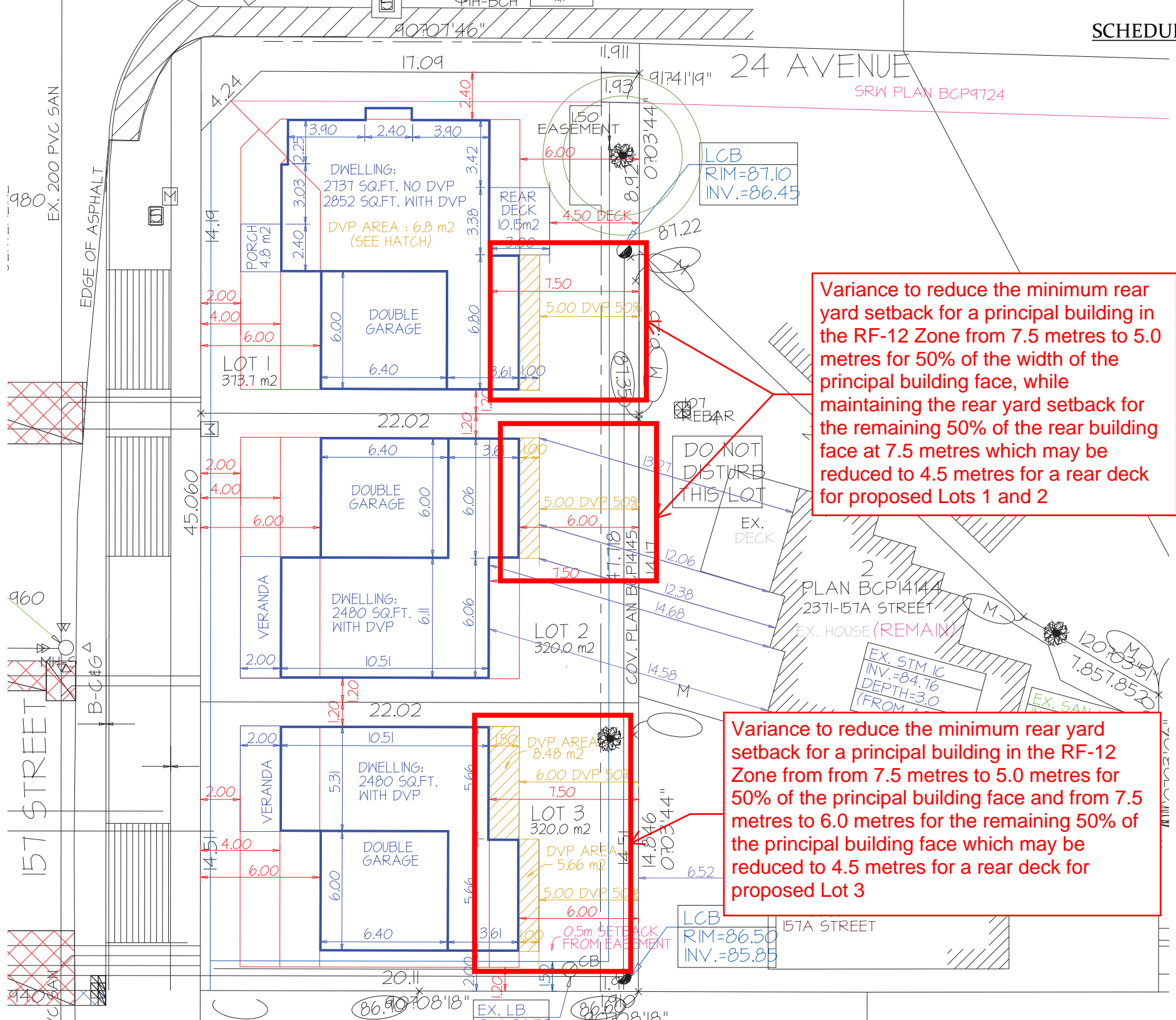
AUTHORIZING RESOLUTION PASSED BY THE COUNCIL, THE      DAY OF      , 20 .  
ISSUED THIS      DAY OF      , 20 .

---

Mayor – Doug McCallum

---

City Clerk – Jennifer Ficocelli



Variance to reduce the minimum rear yard setback for a principal building in the RF-12 Zone from 7.5 metres to 5.0 metres for 50% of the width of the principal building face, while maintaining the rear yard setback for the remaining 50% of the rear building face at 7.5 metres which may be reduced to 4.5 metres for a rear deck for proposed Lots 1 and 2

Variance to reduce the minimum rear yard setback for a principal building in the RF-12 Zone from from 7.5 metres to 5.0 metres for 50% of the principal building face and from 7.5 metres to 6.0 metres for the remaining 50% of the principal building face which may be reduced to 4.5 metres for a rear deck for proposed Lot 3