

City of Surrey
PLANNING & DEVELOPMENT REPORT

File: 7916-0431-00

Planning Report Date: June 12, 2017

PROPOSAL:

- **Heritage Revitalization Agreement**
- **Development Permit**
- **Development Variance Permit**

to permit the expansion of the Surrey Museum and relocation and preservation of Anderson Cabin.

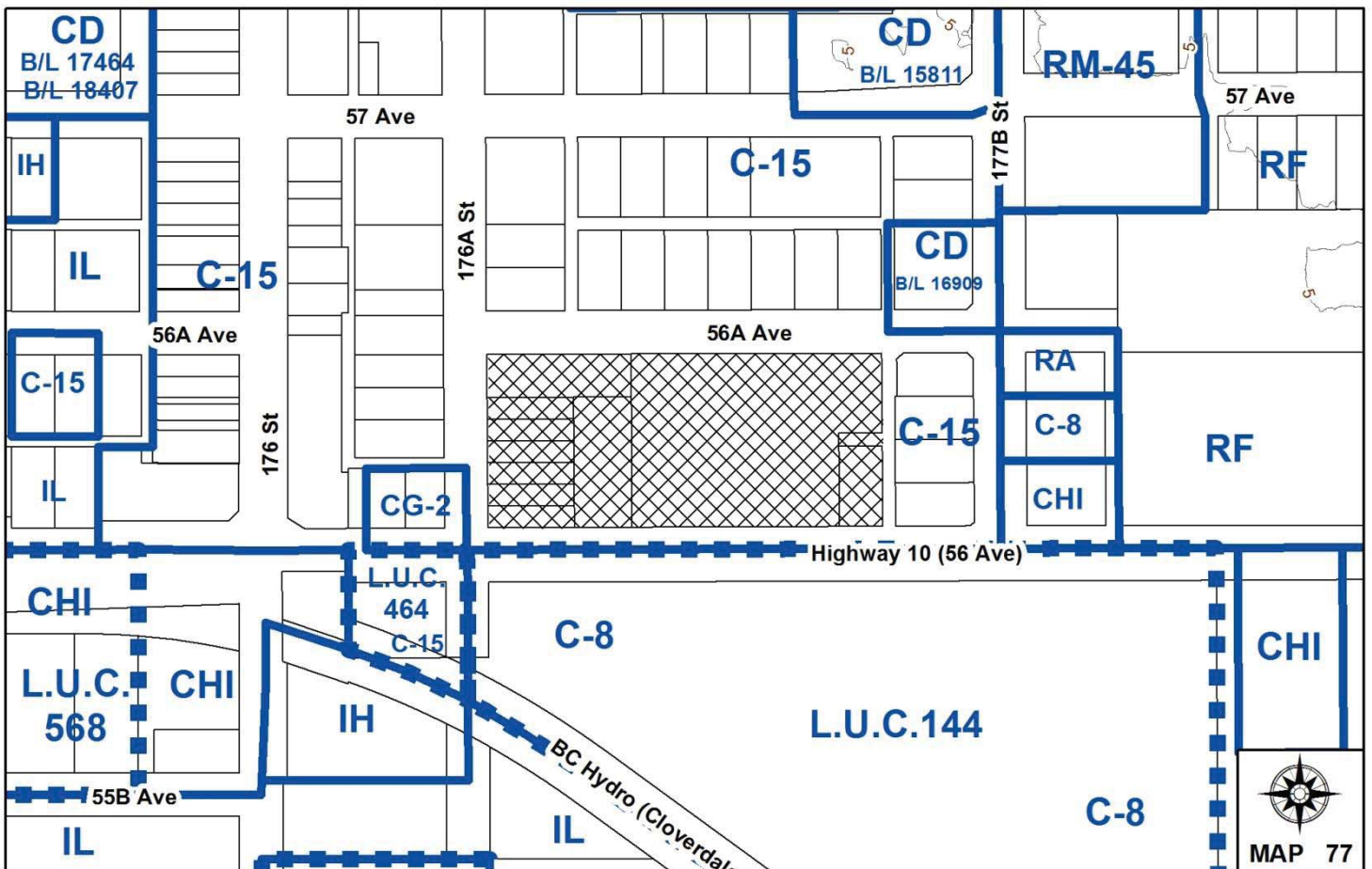
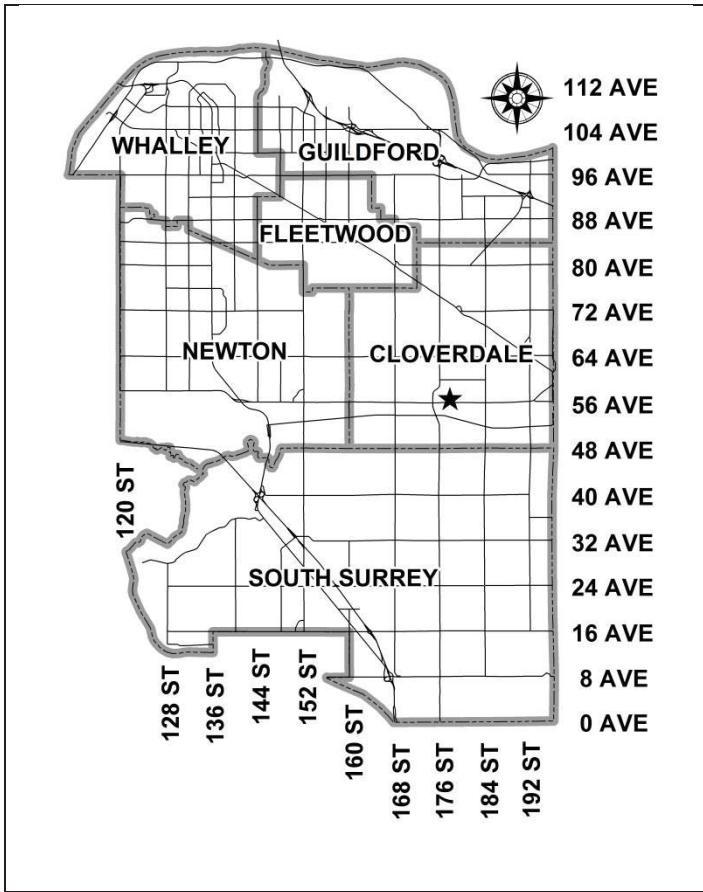
LOCATION: 17671, 17675 & and 17735 – No. 10 (56 Ave.) Highway
 5642 – 176A Street
 17710 – 56A Avenue
 Portion of lane

OWNER: City of Surrey

ZONING: RF

OCB DESIGNATION: Town Centre

TCP DESIGNATION: Institutional and Parks and Open Space



RECOMMENDATION SUMMARY

- Introduce a Heritage Revitalization Agreement (HRA) By-law to relocate and preserve the Anderson Cabin.
- Approval to draft Development Permit.
- Approval for Development Variance Permit to proceed to Public Notification.

DEVIATION FROM PLANS, POLICIES OR REGULATIONS

- Seeking a variance to the off-street parking requirement of the Zoning By-law.

RATIONALE OF RECOMMENDATION

- Complies with the "Institutional" designation in the OCP and the "Institutional" designation in the Cloverdale Town Centre Plan (TCP).
- The proposed expansion to the Surrey Museum complies with the objectives of the 2008-2017 Parks, Recreation and Culture Strategic Plan.
- The intent of the proposed Heritage Revitalization Agreement (HRA) By-law is to relocate, retain and preserve the Anderson Cabin, which is on the Surrey Heritage Register.
- The HRA By-law will contribute to the City's goal of promoting the long-term preservation of the City's heritage stock.
- This proposal will unify the Surrey Museum, Cloverdale Library and Surrey Archives site by consolidating the properties into an educational and cultural campus within the Cloverdale Town Centre area.
- Based on pre-existing uses on the site and the 1,227-square metre (13,207-sq. ft.) proposed addition to the museum, a variance is required for on-site parking requirements. A parking demand study completed by Bunt & Associates has been submitted and supports the proposed relaxation.

RECOMMENDATION

The Planning & Development Department recommends that:

1. a By-law be introduced for a Heritage Revitalization Agreement to allow for the relocation and conservation of the Anderson Cabin.
2. Council authorize staff to draft Development Permit No. 7916-0431-00 generally in accordance with the attached drawings (Appendix II).
3. Council approve Development Variance Permit No. 7916-0431-00 (Appendix V) to reduce the minimum number of required off-street parking spaces in the Zoning By-law from 182 to 104, to proceed to Public Notification.
4. Council instruct staff to resolve the following issues prior to approval:
 - (a) ensure that all engineering requirements and issues including restrictive covenants, dedications, and rights-of-way where necessary, are addressed to the satisfaction of the General Manager, Engineering;
 - (b) submission of a subdivision layout to the satisfaction of the Approving Officer;
 - (c) approval from the Ministry of Transportation & Infrastructure;
 - (d) submission of a finalized tree survey and a statement regarding tree preservation to the satisfaction of the City Landscape Architect;
 - (e) submission of a finalized landscaping plan and landscaping cost estimate to the specifications and satisfaction of the Planning and Development Department;
 - (f) resolution of all urban design issues to the satisfaction of the Planning and Development Department; and
 - (g) completion of the road closure and acquisition of a portion of the existing lane on the west portion of the subject site.

REFERRALS

Engineering:	The Engineering Department has no objection to the project subject to the completion of Engineering servicing requirements as outlined in Appendix III.
Parks, Recreation & Culture:	Parks has no concerns with the proposed project.
Ministry of Transportation & Infrastructure (MOTI):	The Ministry has no concerns with the proposed variance.

Surrey Heritage Advisory Commission (SHAC): The proposal was presented at the May 24, 2017 SHAC meeting. No concerns were raised and support was given for the proposed Heritage Revitalization Agreement (Appendix VIII).

SITE CHARACTERISTICS

Existing Land Use: Cloverdale Library, Surrey Archives, Veteran's Square, Anderson Cabin and Surrey Museum.

Adjacent Area:

Direction	Existing Use	TCP Designation	Existing Zone
North (Across 56A Avenue):	Two-storey commercial buildings and single family dwellings	Town Centre Commercial, Parking and Medium Density Residential	C-15
East (Across lane):	Vacant lot, one-storey office building, one-storey medical office and one-storey florist shop	Retail/Service Commercial	C-15
South (Across Highway No. 10):	Clover Square Shopping Centre	Retail/Service Commercial	Land Use Contract No. 144 (underlying C-8)
West (Across 176A Street):	One and two-storey commercial retail buildings and a vacant gas station site.	Town Centre Commercial and Parks/Open Space	C-15 and CG-2

DEVELOPMENT CONSIDERATIONS

Background

- The 3.7-acre (17,780-square metre) subject site includes five properties and an unopened lane allowance located on the northeast corner of 176A Street and Highway No. 10 (56 Avenue), in the Cloverdale Town Centre.
- The subject site is designated "Town Centre" in the Official Community Plan (OCP), "Institutional and Parks/Open Space" in the Cloverdale Town Centre Plan (TCP) and is zoned "Town Centre Commercial Zone (C-15)".
- On September 7, 2004, Council approved Development Permit No. 7903-0474-00 and Development Variance Permit No. 7903-0474-00 to allow the development of the Surrey Museum. The museum officially opened on October 18, 2005, and accommodates both permanent and temporary exhibit displays, as well as hosts a number of programs and events. There is also a textile studio and library located on the upper floor of the museum.

- Situated north of the museum on the subject property is the Anderson Cabin, which is the oldest remaining pioneer-era log shelter in Surrey. The Cloverdale Library and City of Surrey Archives Building are located on the western portion of the subject site to the west of the museum.

Current Proposal

- The applicant is requesting a Development Permit to allow a 1,277-square metre (13,746 sq. ft.) addition on the north and east sides of the existing Surrey Museum on a consolidated site.
- The total floor area of all existing buildings and the proposed addition is 5,530 square metres (59,524 sq. ft.) representing a floor area ratio (FAR) of 0.37 on the site once the associated land consolidation is complete, which complies with the maximum 1.5 FAR permitted under the C-15 Zone
- The proposed addition to the existing Surrey Museum complies with all the requirements of the C-15 Zone, except for the total number of on-site parking spaces (see By-law Variance section).

DESIGN PROPOSAL AND REVIEW

Building Design

- The proposed addition to the existing Surrey Museum building will be one storey in height and will provide approximately 1,277 square metres (13,746 sq. ft.) of additional museum space.
- The proposed addition consists of two parts, a single-storey free-standing structure is proposed to be located to the north of the existing building linked by an enclosed glass walkway to the existing museum, and a secondary addition is proposed as an expansion on the east side of the existing museum building.
- The proposed north addition relocates the main civic entrance closer to the street on 56A Avenue, defining the 56A Avenue street edge and providing new civic presence at the street. The addition is proposed to house all of the new public programs including a grand foyer space for public events, a class A feature gallery, a children's gallery and new reception and administrative spaces.
- The secondary addition on the east side of the existing building is proposed to house a workshop and an expanded collections storage space.
- The proposed north addition is a contemporary building form with an exposed Nail Laminated Timber (NLT) roof structure and a massing that is in keeping with the scale of the existing buildings along 56A Avenue and the neighbourhood in general.

- Brick cladding in a midnight black colour, is proposed on the north, south and west elevations of the north addition. Brick, one of the oldest cladding materials, is taken out of its most common context as a base building element and is proposed to be treated as a textile, wrapping around the building and revealing the openings and glazing. An historical material proposed in a contemporary building form and technique sets up a timeless building character.
- Glazing is proposed for the entire east elevation of the north addition to emulate a picture window and to connect the interior children's gallery to the exterior education zone.
- The east addition is a simple single storey expansion to the existing museum building and will be clad in the same coloured brick as the existing museum building.

Signage

- One "Museum of Surrey" fascia sign is proposed to be located on both the north and east elevations of the proposed north addition.
- The proposed signage is non-illuminated and comprised of stainless steel pin-mounted lettering.

Site Circulation and Parking

- Direct pedestrian access is proposed from 56A Avenue to the main entrance of the new north addition.
- A new vehicular access is proposed from 56A Avenue, to the proposed surface parking lot located to the east of the museum building on the eastern portion of the site. Existing vehicular access will remain from 176A Street and 56A Avenue to the existing surface parking lots adjacent to the Surrey Archives and Cloverdale Library.
- Based on the requirement of 3 parking spaces per 100 square metres (1,075 sq. ft.) of floor area for museum use and 5 parking spaces per 100 square metres (1,075 sq. ft.) of floor area for the library use in Part 5 Parking and Loading/Unloading of Zoning By-law No. 12000, the entire 5,530-square metre (59,524-sq. ft.) campus requires a total of 182 parking spaces. The proposed development is providing 104 parking spaces (see By-law Variance section).
- An existing EV charging station will be relocated on site to accommodate electric vehicles.

Existing Heritage Building – Anderson Cabin and Proposed Heritage Revitalization Agreement

- The current development proposal also includes relocating the Anderson Cabin to a new location on the museum site. The Cabin has been located on the museum grounds since 1971, when it was relocated from a farm site in Surrey. The current location is within the building footprint of the proposed museum expansion.
- Once the museum expansion is complete the Cabin is proposed to be permanently located southeast of its current location and just north of the proposed addition on the east side of the museum building.

- During construction, the Cabin will be moved and temporarily stored on the museum parking lot. The relocation and conservation plan outlines two different methods for moving the Cabin (translational or crane) to the temporary location and again when it is moved to the final location on the site.
- In the museum masterplan, the 1881 Municipal Hall and the Anniedale School are also to be relocated to the museum site. The three heritage buildings (the Anderson Cabin, the 1881 Municipal Hall and the Anniedale School) will be grouped in close proximity to each other and act as a heritage component of the future Education Zone on the museum site.
- The Anderson Cabin is listed on the Surrey Heritage Register (#153) and has been given a rating of 81% on the Surrey Heritage Evaluation Worksheet (Appendix VI).
- The Cabin is Surrey's oldest existing pioneer building and is an example of an early pioneer shelter. The cabin is an important cultural feature for the interpretation of Surrey's heritage to the public.
- The character-defining elements of the Cabin include:
 - Location on public land;
 - Simple form, modest scale and rectangular plan massing;
 - Hand-hewn square logs with notched corners and chinking;
 - Large original growth logs;
 - Gabled roof, clad with split cedar barn shakes; and
 - Lapped wooden siding in gable ends.
- The heritage protection instrument applied for, a Heritage Revitalization Agreement (HRA), is a voluntary agreement between landowners and the City. This agreement is given effect through a By-law under Section 610 of the Local Government Act. The attached HRA (Appendix VII) specifies the owner's obligations to protect, conserve, and maintain the building and includes provisions for replication and compensation in the event that the building is destroyed. The HRA also includes a Relocation and Conservation Plan that outlines:
 - The heritage significance of the Cabin;
 - The character-defining elements of the Cabin;
 - The maintenance and restoration requirements consistent with the heritage character of the Cabin;
 - The relocation plan for the Cabin; and
 - Interventions that would require or would not require issuance of heritage alteration permits before works can be completed.

- The HRA and Conservation Plan allow for normal maintenance, however any significant alterations in the future will be subject to Council-authorized Heritage Alteration Permit. At this time, there are no significant renovations or alterations proposed for the building.

Trees

- In order to accommodate the proposed expansion and associated parking, eighteen (18) trees were identified by Parks staff as candidates to be transplanted elsewhere in the City of Surrey. Forty (40) trees were identified for removal and sixty-nine (69) to be retained on the subject site. The trees have since been removed and transplanted.
- The applicant is proposing thirty-one (31) replacement trees. The new trees to be planted on the site will consist of maples and dogwoods.
- In summary, a total of 100 trees are proposed to be retained or replaced on the site.

SUSTAINABLE DEVELOPMENT CHECKLIST

The applicant prepared and submitted a sustainable development checklist for the subject site on October 20, 2016. The table below summarizes the applicable development features of the proposal based on the seven (7) criteria listed in the Surrey Sustainable Development Checklist.

Sustainability Criteria	Sustainable Development Features Summary
1. Site Context & Location (A1-A2)	<ul style="list-style-type: none"> • Within the Cloverdale Town Centre.
2. Density & Diversity (B1-B7)	<ul style="list-style-type: none"> • The proposed density is 0.37 FAR and includes the relocation, protection and conservation of the Anderson Cabin on the subject site through an HRA.
3. Ecology & Stewardship (C1-C4)	<ul style="list-style-type: none"> • Sub-surface rainwater detention tank for stormwater detention from proposed roofs and paved areas. • Proposed waste, compost and recycling collection area in the parking lot.
4. Sustainable Transport & Mobility (D1-D2)	<ul style="list-style-type: none"> • Existing EV charging station will be relocated on site. • New bicycle racks proposed.
5. Accessibility & Safety (E1-E3)	<ul style="list-style-type: none"> • Hierarchy of space, transparency at entrances, access control at all doors, open site lines between spaces, security cameras inside and outside, direct site lines from reception/security desk to all entrances, galleries and outdoor spaces. • Community spaces in foyer, program rooms and public galleries.
6. Green Certification (F1)	<ul style="list-style-type: none"> • n/a
7. Education & Awareness (G1-G4)	<ul style="list-style-type: none"> • The typical notifications to area residents has occurred (i.e. development proposal sign and Pre-notification letter).

ADVISORY DESIGN PANEL

ADP Date: January 26, 2017

The applicant has resolved all of the outstanding items from the ADP review to the satisfaction of the Planning and Development Department (see Appendix IV).

PRE-NOTIFICATION

Pre-notification letters were sent on April 5, 2017 and three development proposal signs were installed on January 28, 2017. Staff did not receive any comments.

BY-LAW VARIANCE AND JUSTIFICATION

(a) Requested Variance:

- To relax the off-street required parking in the Zoning By-law from 182 spaces to 104 spaces for the consolidated civic site.

Applicant's Reasons:

- There are currently 24 existing parking spaces serving the Cloverdale Library and Surrey Archives. Based on the proposed expanded museum area, a total of 98 parking spaces would be required. A total of 80 off-street parking spaces are proposed to be provided in a surface parking lot to the east of the museum, resulting in a shortfall of 18 parking spaces for the proposed expanded museum.
- Based on a parking study provided by Bunt and Associates, it is not anticipated that there will be an observed parking shortage on this site.
- It is anticipated that approximately half of the visitors to the site will be school groups arriving by school bus, while the remaining visitors to the site will typically come in small groups, arriving in one vehicle.

Staff Comments:

- On September 7, 2004, Council approved Development Variance Permit No. 7903-0474-00 to reduce the number of required on-site parking spaces from 120 to 98 for the Cloverdale Library, Surrey Archives and Surrey Museum based on the floor areas at that time.
- The subject site currently contains 4,253 square metres (45,780 sq. ft.) of total floor area comprised of the Cloverdale Library, Surrey Archives and existing Surrey Museum buildings. The proposed museum expansion will add an additional 1,277 square metres of floor area, bringing the total floor area to 5,530 square metres (59,524 sq. ft.) for the entire site.

- Based on the requirement of 3 parking spaces per 100 square metres (1,075 sq. ft.) of floor area for museum use and 5 parking spaces per 100 square metres (1,075 sq. ft.) for library use, in Part 5 Parking and Loading/Unloading of Zoning By-law No. 12000, the proposed 5,530-square metre (59,524-sq. ft.) floor area requires a total of 182 parking spaces.
- A total of 104 parking spaces are provided on the overall site, with 80 provided to the east of the museum building, and an additional 24 parking spaces provided to the east and south of the library.
- As the site borders Highway No. 10, the Ministry of Transportation and Infrastructure was consulted and the Ministry has no concerns with the proposed parking.
- Staff concur with the rationale provided by the applicant and the findings of the Bunt and Associates Parking study, and support the requested variance.

INFORMATION ATTACHED TO THIS REPORT

The following information is attached to this Report:

Appendix I.	Lot Owners, Action Summary and Project Data Sheets
Appendix II.	Site Plan, Building Elevations and Perspectives
Appendix III.	Engineering Summary
Appendix IV.	ADP Comments and Applicant's Responses
Appendix V.	Development Variance Permit No. 7916-0431-00
Appendix VI.	Heritage Worksheet
Appendix VII.	Heritage Revitalization Agreement By-law
Appendix VIII.	Surrey Heritage Advisory Commission Draft Minutes

original signed by Judith Robertson

Jean Lamontagne
General Manager
Planning and Development

JLM/da

3. Summary of Actions for City Clerk's Office

- (a) Introduce a By-law to allow for a Heritage Revitalization Agreement for the Anderson Cabin and grant three readings.
- (b) Proceed with Public Notification for Development Variance Permit No. 7916-0431-00 and bring the Development Variance Permit forward for an indication of support by Council. If supported, the Development Variance Permit will be brought forward for issuance and execution by the Mayor and City Clerk in conjunction with the final approval of the associated Development Permit.

DEVELOPMENT DATA SHEET

Existing Zoning: C-15

Required Development Data	Minimum Required / Maximum Allowed	Proposed
LOT AREA* (in square metres)		
Gross Total		
Road Widening area		
Undevelopable area		
Net Total		14,780 m ²
LOT COVERAGE (in % of net lot area)		
Buildings & Structures	80%	24%
Paved & Hard Surfaced Areas		46%
Total Site Coverage		70%
SETBACKS (in metres)		
Front (176A Street)	2.0 m	9.4 m
Rear (lane)	7.5 m	60 m
Side #1 (N)	2.0 m	8.56 m
Side #2 (S)	2.0 m	5.54 m
BUILDING HEIGHT (in metres/storeys)		
Principal	14 m	14 m
Accessory	n/a	
NUMBER OF RESIDENTIAL UNITS		
Bachelor		
One Bed		
Two Bedroom		
Three Bedroom +		
Total		
FLOOR AREA: Residential		
FLOOR AREA: Commercial		
Retail		
Office		
Total		
FLOOR AREA: Industrial		
FLOOR AREA: Institutional		5,530 m ²
TOTAL BUILDING FLOOR AREA	22,170 m ²	5,530 m ²

** If the development site consists of more than one lot, lot dimensions pertain to the entire site.*

Development Data Sheet cont'd

Required Development Data	Minimum Required / Maximum Allowed	Proposed
DENSITY		
# of units/ha /# units/acre (gross)		
# of units/ha /# units/acre (net)		
FAR (gross)		
FAR (net)	1.5	0.37
AMENITY SPACE (area in square metres)		
Indoor		
Outdoor		
PARKING (number of stalls)		
Commercial		
Industrial		
Residential Bachelor + 1 Bedroom		
2-Bed		
3-Bed		
Residential Visitors		
Institutional	182	104*
Total Number of Parking Spaces	182	104*
Number of accessible stalls	2	9
Number of small cars	64	0
Tandem Parking Spaces: Number / % of Total Number of Units		
Size of Tandem Parking Spaces width/length		

* Seeking variance

Heritage Site	YES	Tree Survey/Assessment Provided	YES
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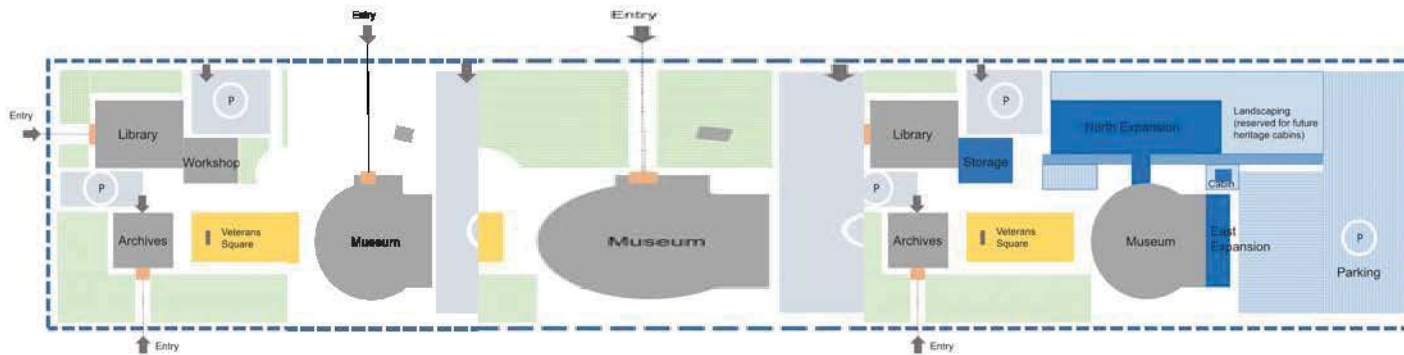
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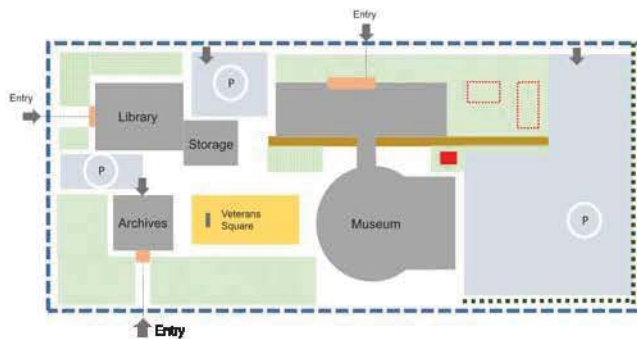
SURREY MUSEUM EXPANSION

Advisory Design Panel - January 26, 2017

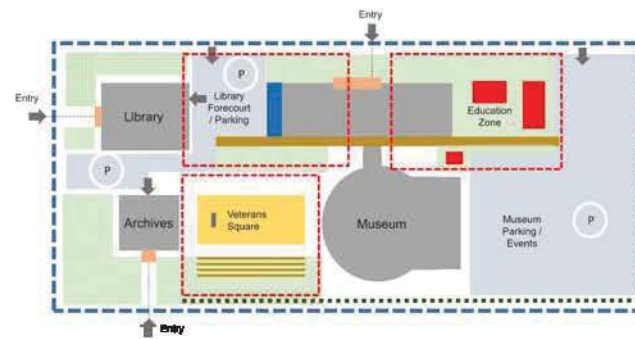


a. Existing Conditions

b. Project Scope



c. Proposed Expansion - Museum + Site



d. Masterplan

a. Existing Conditions

- 3 separate buildings with main entry facing away from Veterans' Square and each other;
- Veterans' Square is ill-defined at edges, with very little activity from adjacent buildings on daily basis apart from large event days;
- Precinct lacks unified identity and sense of place;
- Museum entrance is difficult to find given drum shape and repetitive decorative elements;
- Museum is in odd relationship with streets, too close to 56 Avenue to be recognized as prominent Institution from moving vehicle, and too far from 56A Avenue to have civic presence.
- Opaque walls and reflective glass are uninviting, obscure views and inconsistent with Museum's vision for community access and participation

b. Project Scope

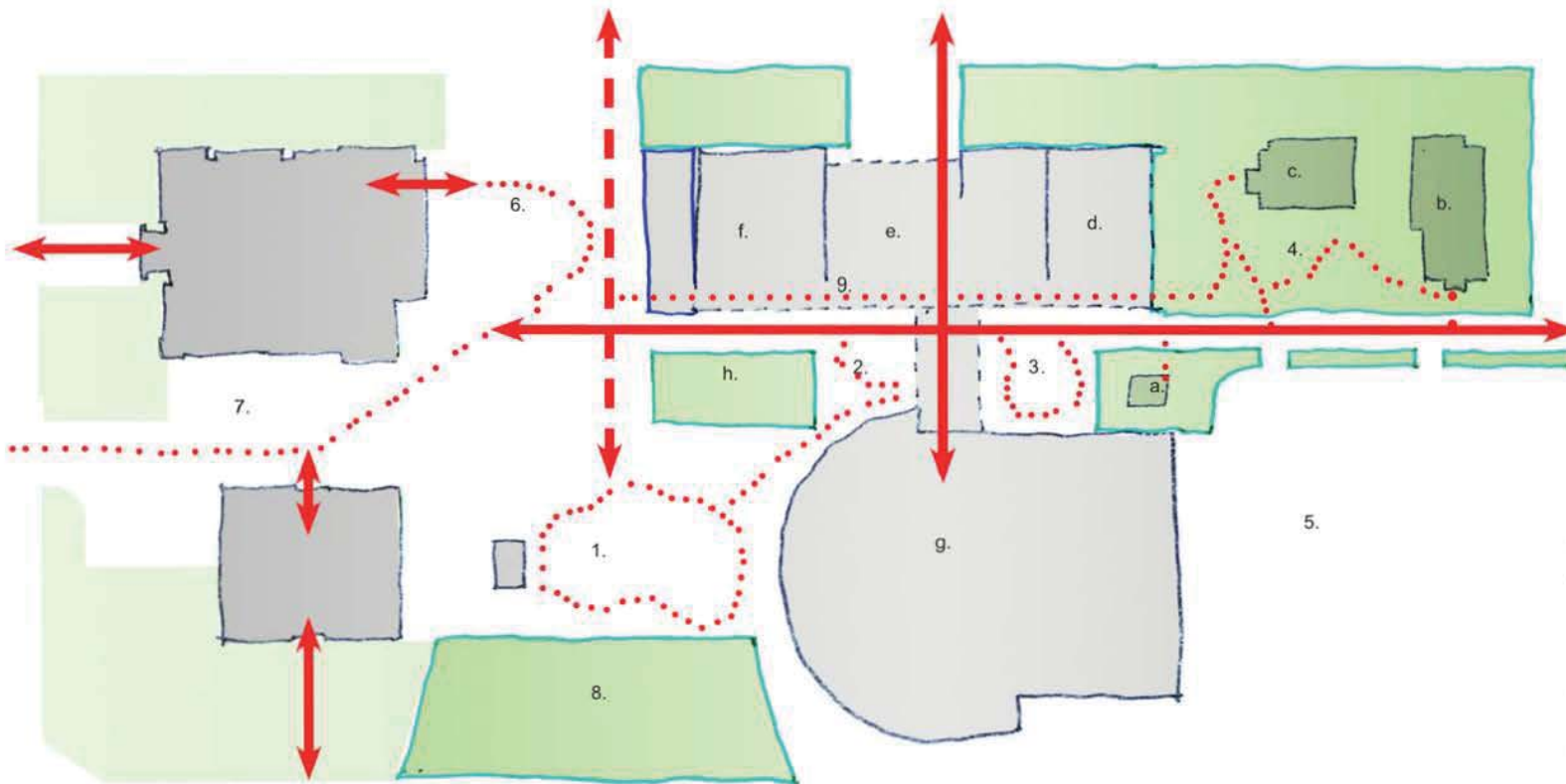
- North Expansion defines 56A Avenue street edge together with library; provides new civic presence to grand Foyer and walking entrance from Cloverdale Town Centre and vehicular drop-off
- East Expansion and Parking Lot;
- Distinctive paving establishes East-West axis for experience of the Museum, while new hardscape defines outdoor classrooms and softscape defines leisure areas and reserves space for future heritage cabins in education zone;

c. Proposed Expansion - Museum and Site

d. Masterplan

- Future removal of Workshop makes way for shared paved courtyard / parking between library and museum, replanning of library with new entrance on east side;
- 1881 Municipal Hall and Annedale School are added to Anderson Cabin to complete the heritage component in the Education Zone;
- Veterans' Square will be expanded with improved landscaping elements on edges including raised berm at south for seating, vertical screening and identifying element towards 56 Avenue

SITE DIAGRAMS

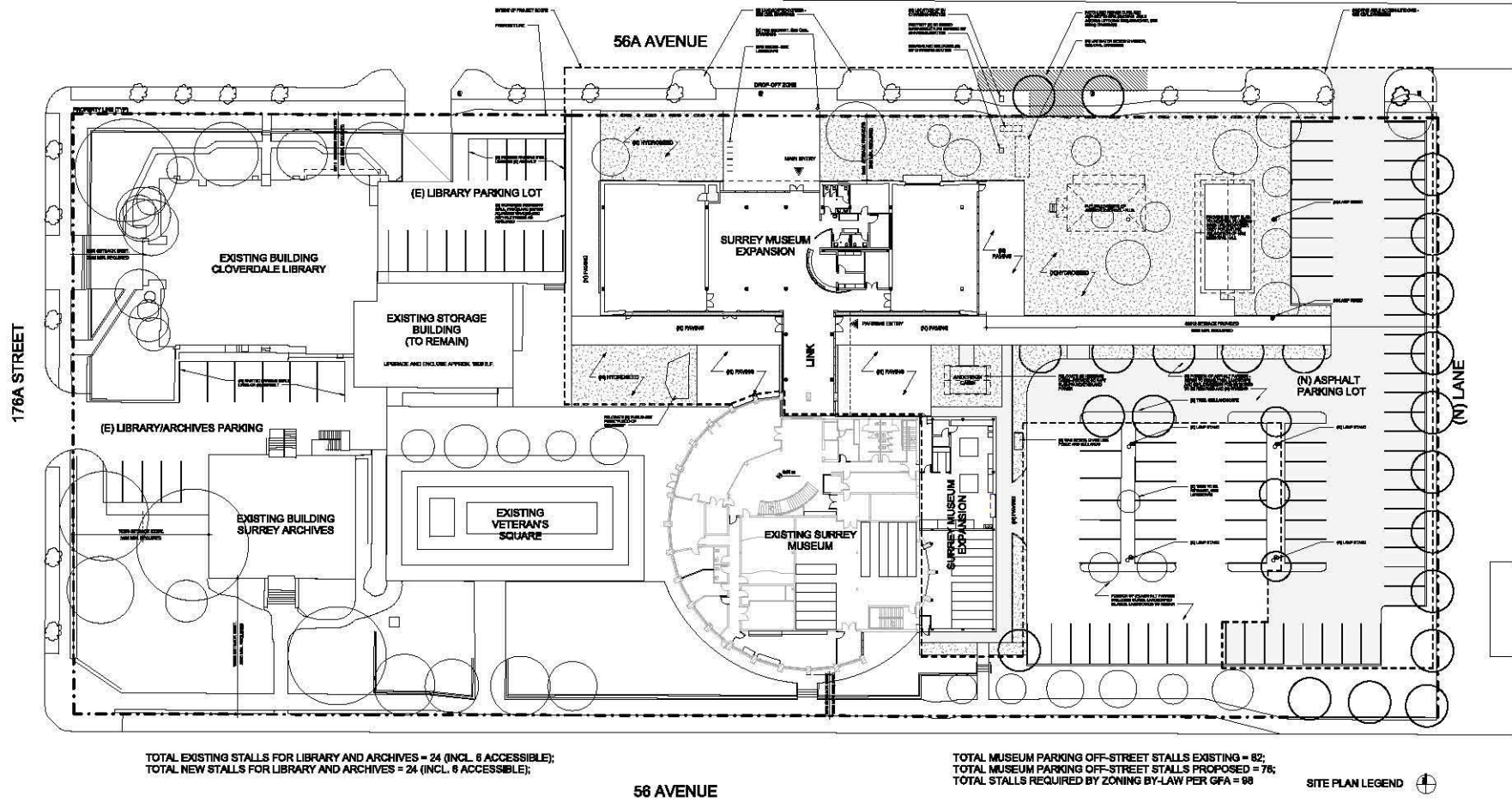


- 1. VETERANS' SQUARE
- 2. CAFE SEATING
- 3. OUTDOOR CLASSROOM
- 4. EDUCATION ZONE
 - a. ANDERSON CABIN
 - b. 1881 HALL
 - c. ANNIDALE SCHOOL
 - d. CHILDREN'S GALLERY
- 5. MUSEUM PARKING / EVENTS
- 6. LIBRARY FORECOURT / EVENTS / PARKING
- 7. PARKING
- 8. RAISED BERM WITH LANDSCAPED SEATING
- 9. EVENT ZONE
 - e. FOYER
 - f. FEATURE GALLERY
 - g. PERMANENT GALLERY AND PROGRAM ROOMS
 - h. ACTIVITY LAWN

SITE DIAGRAM

PROJECT INFORMATION / SUMMARY	
Development application number: 210-0001-01	
Address:	1770 56A Avenue, Surrey, B.C. V5B 5B9
City:	City of Surrey
Plan Type:	Site Plan (Development Agreement) / Part 1 (SPT) / Part 2 (SPT)
Designated Use:	Community Centre
City Zone:	C-2 Town Centre, Community and Town (C-2) of L-2
Site Area:	2,000 sqm (77,817 sq ft) - approx. 1.6a
Project Name:	56A Avenue (New Museum) and 56A Avenue (New Library)
Client:	City of Surrey
Scale:	1:1000
Site Area:	2,000 sqm (77,817 sq ft) - approx. 1.6a
Site Coverage:	70% proposed building + paved parking

Vehicle/Driver:	From Year 2014 (New) to 2014 (Old) - 100% of 2014 (New) - 100% of 2014 (Old) - 100% of 2014 (New) - 100% of 2014 (Old)
Height of Site:	Proposed: 15m max. (subject to 15m max. proposed) / 15m max. (subject to 15m max. proposed) / 15m max. (subject to 15m max. proposed)
Floor Area:	1,000 sqm (subject to 1,000 sqm) / 1,000 sqm (subject to 1,000 sqm) / 1,000 sqm (subject to 1,000 sqm)
Off-street Parking:	24 stalls (subject to 24 stalls) / 24 stalls (subject to 24 stalls) / 24 stalls (subject to 24 stalls)
Landmark:	Close proximity to 56A Avenue (1.6a)



TOTAL EXISTING STALLS FOR LIBRARY AND ARCHIVES = 24 (INCL. 8 ACCESSIBLE);
 TOTAL NEW STALLS FOR LIBRARY AND ARCHIVES = 24 (INCL. 8 ACCESSIBLE);

TOTAL MUSEUM PARKING OFF-STREET STALLS EXISTING = 82;
 TOTAL MUSEUM PARKING OFF-STREET STALLS PROPOSED = 76;
 TOTAL STALLS REQUIRED BY ZONING BY-LAW PER GFA = 98

SITE PLAN LEGEND



MUSEUM OF SURREY
 1770 56A AVENUE, SURREY, B.C. V5B 5B9

SITE PLAN

GENERAL NOTES

1. LANDSCAPING AND PLANTING SHALL BE PERFORMED BY THE CONTRACTOR. REFER TO LANDSCAPE CONSULTANT FOR THE DETAILING OF TREES AND PLANTING.
2. ALL EXISTING AND PROPOSED UTILITIES SHALL BE SHOWN AND REFER TO CIVIL DRAWINGS FOR ALL UTILITIES. REFER TO THE ENGINEER FOR ALL UTILITIES.
3. REFER TO THE ARCHITECT FOR ALL LANDSCAPE LIGHTING, SECURITY, AND OTHER UTILITIES.
4. ALL UTILITIES SHALL BE SHOWN AND REFER TO THE ARCHITECT FOR ALL UTILITIES.
5. ALL UTILITIES SHALL BE SHOWN AND REFER TO THE ARCHITECT FOR ALL UTILITIES.

A1.01

SCALE: 1:200
 HCMA Architects + Design
 400 - 875 West Hastings Street
 Vancouver, BC V6P 1E1 Canada
 T: 604.732.8000
 W: hcma.ca



SITE PLAN - FUTURE MASTERPLAN



The museum expansion's new public programming will be easily accessible at street level and define both the museum's civic entry and Veterans' Square. Our planning approach centres on a highly legible visitor experience beginning with arrival (from north or east) directly into the heart of the expanded museum - defined by a cylindrical drum. Each of the three main galleries and program spaces flow naturally from this panoramic point, which itself sits in an activated Foyer space intended for cultural events.

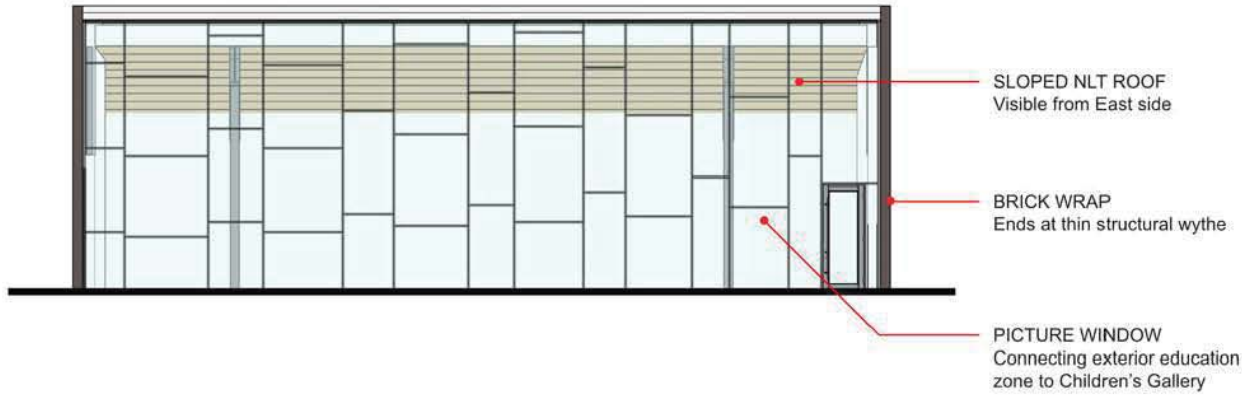
The character of the new public wing will itself define the vision and identity of a rebranded new Museum of Surrey, which itself must reconcile competing forces to remain relevant in a rapidly growing demographic:

It must continue its historical mandate yet become inspiring and optimistic about the future;

It must be secure and controlled yet open and inviting;

It must become more diverse and inclusive of a changing community yet maintain a simple and coherent message;

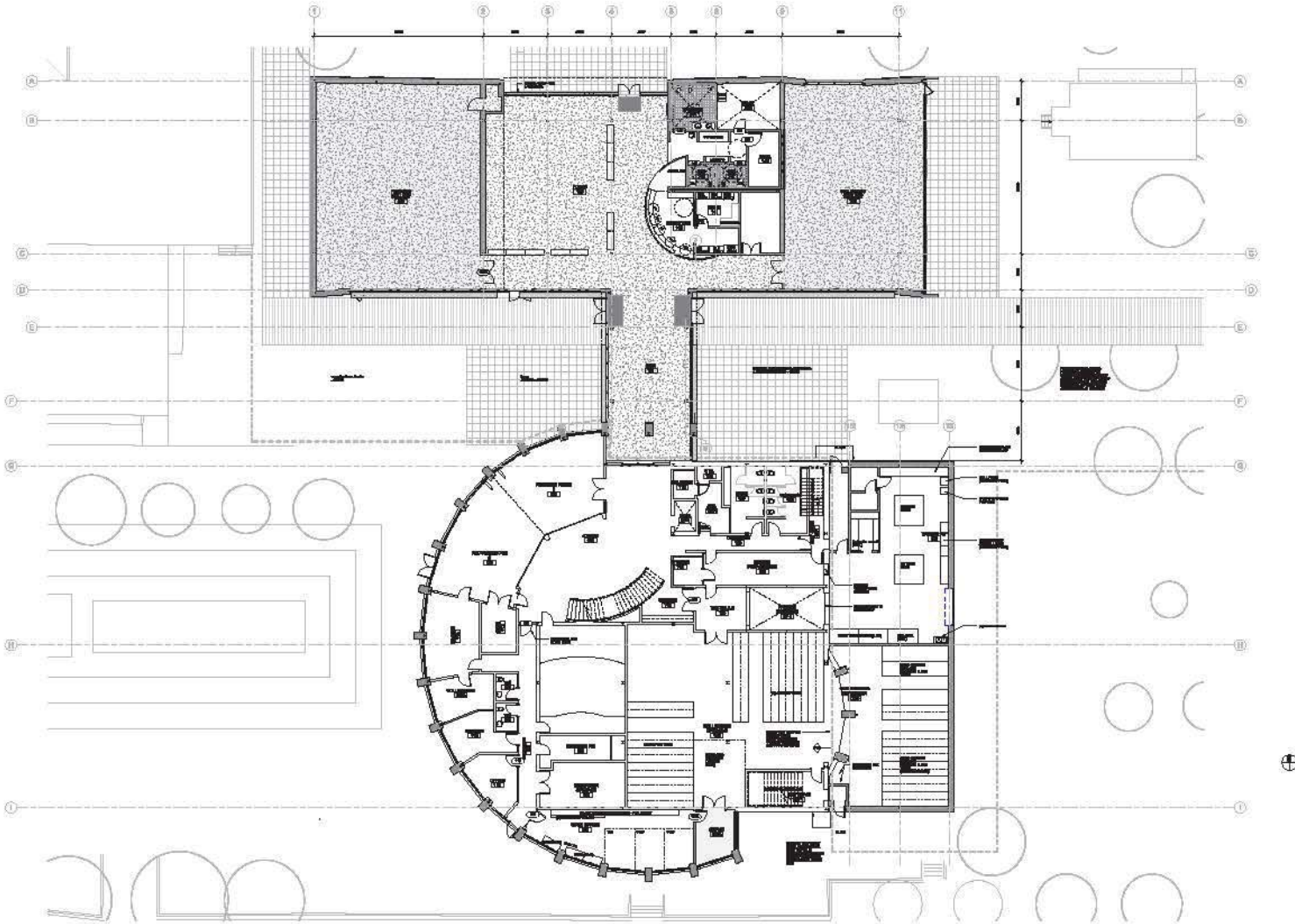
PLANNING APPROACH



BRICK WRAP AT NORTH EXPANSION
Material refers to context, detailed out of context to accentuate the life of a museum



BRICK WRAP AT EAST EXPANSION
Material/ Detailing to Match Existing



FLOOR PLAN



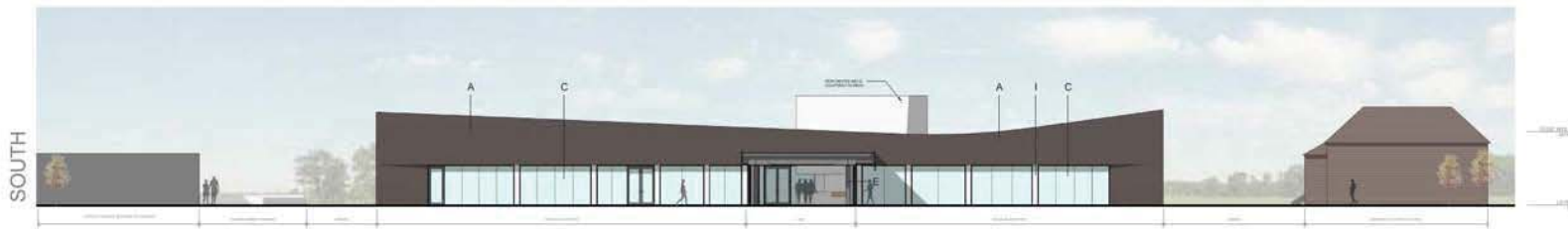
EAST-WEST SECTION



NORTH-SOUTH SECTION LOOKING EAST



NORTH-SOUTH SECTION LOOKING WEST



MATERIALS LEGEND

- A BRICK - TYPE A
- B BRICK - TYPE B (TO MATCH EXISTING MUSEUM)
- C GLAZING
- D SLAT ROOF PANELS
- E PAINTED STEEL STRUCTURE
- F METAL PANELS
- G BRICKWORK
- H COMPOSITE ALUMINUM PANELS
- I PAINTED STEEL
- J EXISTING BUILDING (BRICK)



BUILDING ELEVATIONS

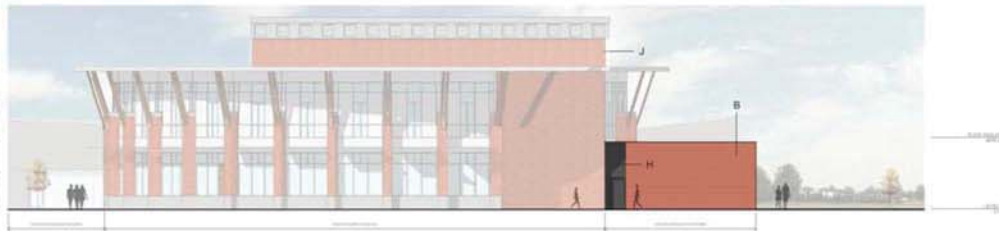
NORTH - EXP.



MATERIALS LEGEND

- A BRICK - TYPE A
- B BRICK - TYPE B (20' MAXIMUM EXPOSURE HEIGHT)
- C GLASS
- D ALU. SLUR PANELS
- E FINISHED BRICK - 10' MAX. HIGH
- F METAL PANELS
- G WOOD PANELS
- H 1/2" COMPACT ALUMINUM PANELS
- I FINISHED BRICK
- J EXTERIOR BUILDING SERVICE

SOUTH - EXP.



BUILDING ELEVATIONS

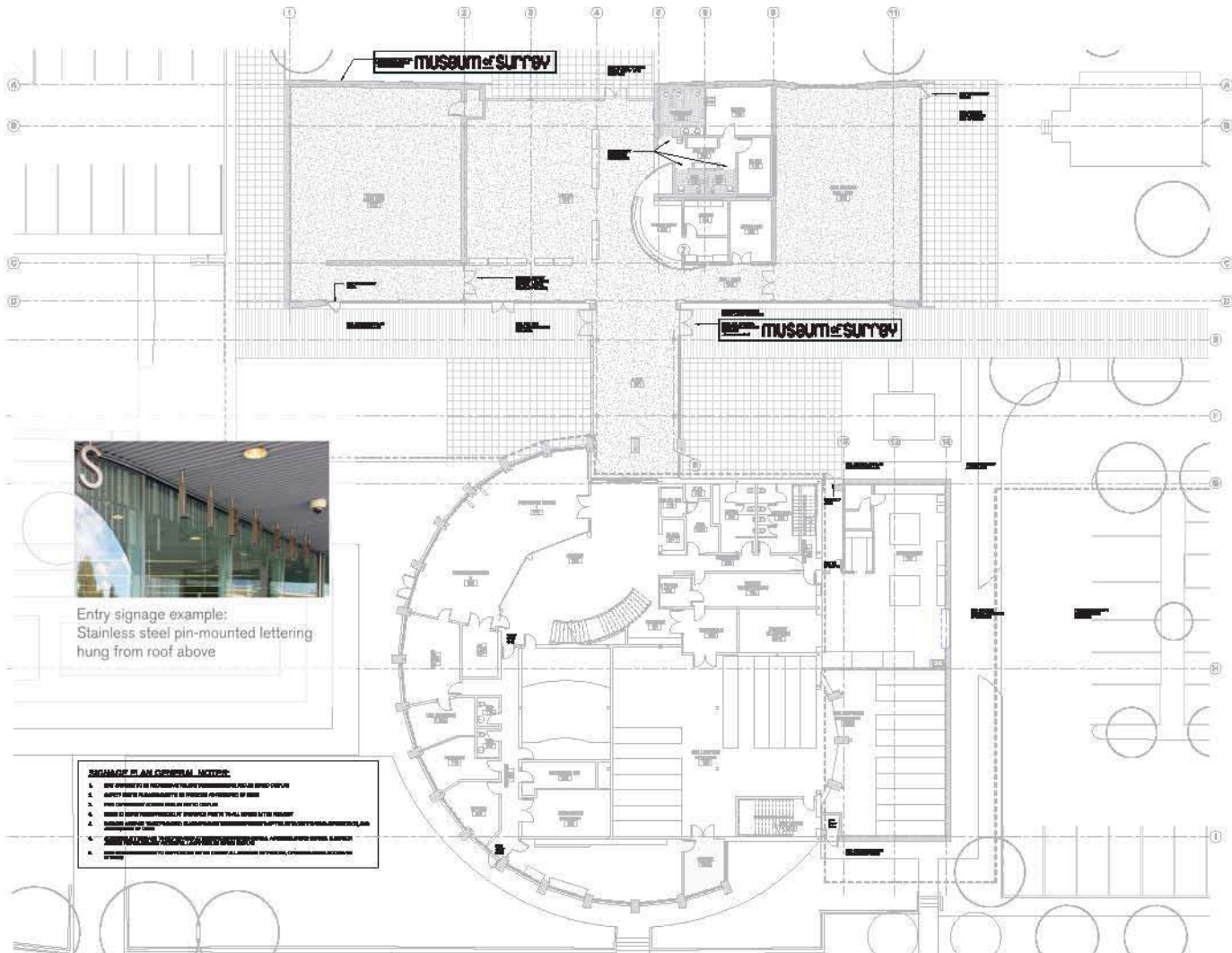


56A 56 A Avenue Streetscape Elevation



56 Avenue Streetscape Elevation

STREETSCAPE ELEVATIONS



Entry signage example:
 Stainless steel pin-mounted lettering
 hung from roof above

- SIGNAGE IN AN GENERAL NOTES:**
- 1. SEE SPECIFICATIONS FOR SIGNAGE MATERIALS AND FINISHES.
 - 2. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 3. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 4. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 5. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
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 - 9. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 10. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 11. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 12. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 13. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 14. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 15. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 16. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 17. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 18. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 19. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.
 - 20. VERIFY SIGNAGE PLACEMENTS AND FINISHES WITH CONTRACTOR.

SIGNAGE PLAN



NAIL LAMINATED TIMBER DOUGLAS FIR
EXPOSED ROOF



PERFORATED METAL
DRUM



PAINTED METAL
SOFFIT



PAINTED METAL
SIDING



BRICK MIDNIGHT BLACK
ADDITION



BRICK COPPERSTONE
EXISTING

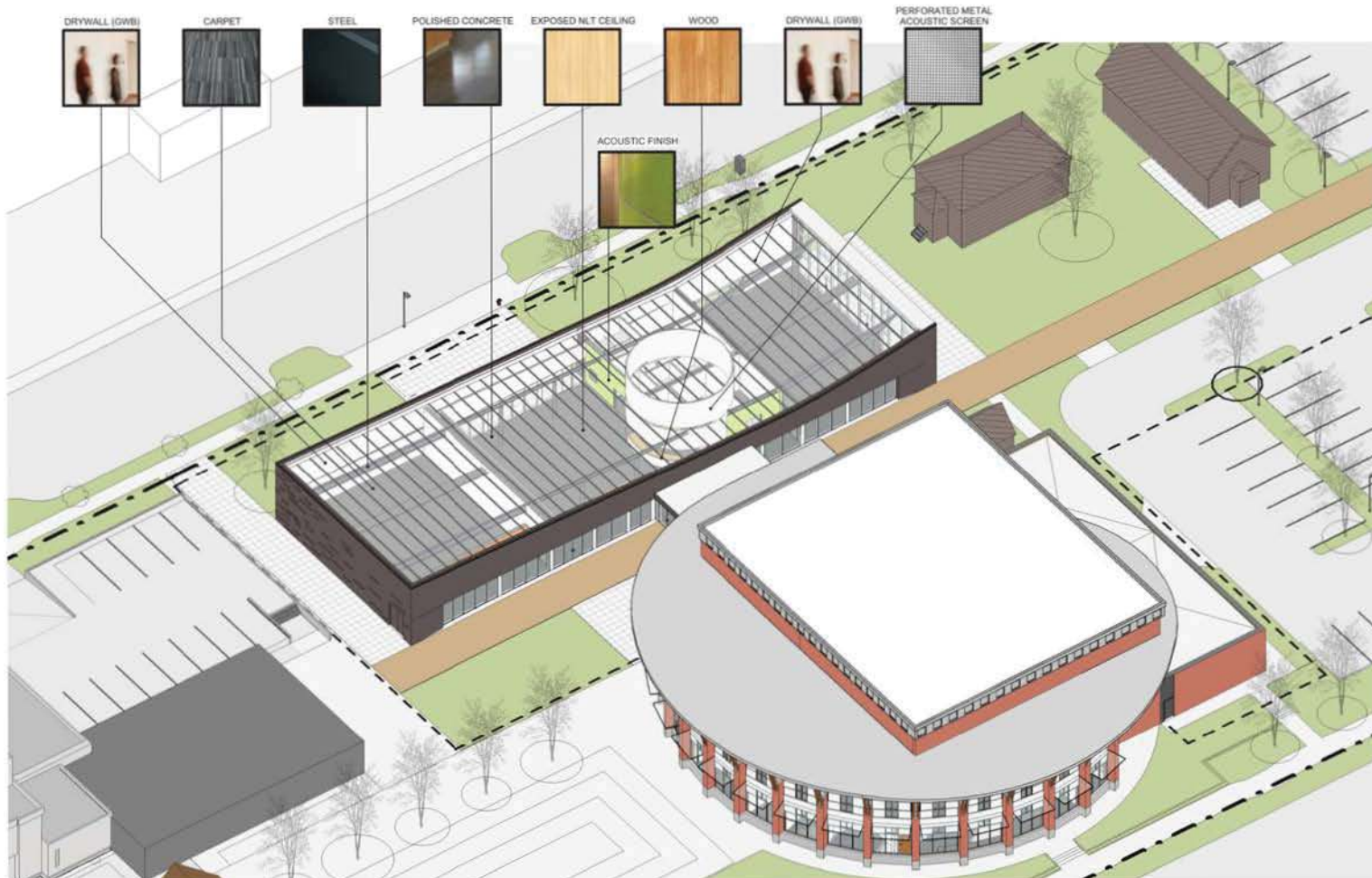


CLEAR GLAZING
CURTAIN WALL



CLEAR ANODIZED ALUMINUM
MULLIONS

EXTERIOR MATERIALS



INTERIORS CONCEPT

HCMA Architecture + Design
400 - 675 West Hastings Street
Vancouver BC V6B 1N2 Canada

T 604.732.6620
W hcma.ca

HCMA
Architecture
+ Design



PERSPECTIVE - BIRD'S EYE FROM 56A AVE

HCMA Architecture + Design
400 - 675 West Hastings Street
Vancouver BC V6B 1N2 Canada

T 604.732.6620
W hcma.ca

HCMA
Architecture
+ Design



PERSPECTIVE - NORTH ELEVATION

HCMA Architecture + Design
400 - 675 West Hastings Street
Vancouver BC V6B 1N2 Canada

T 604.732.6620
W hcma.ca



PERSPECTIVE - FROM 56A AVE



PERSPECTIVE - FOYER

HCMA Architecture + Design
400 - 675 West Hastings Street
Vancouver BC V6B 1N2 Canada

T 604.732.6620
W hcma.ca

HCMA
Architecture
+ Design



PERSPECTIVE - RECEPTION

HCMA Architecture + Design
400 - 675 West Hastings Street
Vancouver BC V6B 1N2 Canada

T 604.732.6620
W hcma.ca

HCMA
Architecture
+ Design



PERSPECTIVE - FROM PARKING

HCMA Architecture + Design
400 - 675 West Hastings Street
Vancouver BC V6B 1N2 Canada

T 604.732.6620
W hcma.ca

HCMA
Architecture
+ Design



PERSPECTIVE - VETERAN'S SQUARE

INTER-OFFICE MEMO

TO: **Manager, Area Planning & Development
- North Surrey Division
Planning and Development Department**

FROM: **Development Services Manager, Engineering Department**

DATE: **June 5, 2017** PROJECT FILE: **7816-0431-00**

RE: **Engineering Requirements (Commercial/Industrial)
Location: 17710 56A Avenue, 17675/17671/17635 No 10 (56 Ave) Hwy, 5624/5642
176A Street**

SUBDIVISION***Property and Right-of-Way Requirements***

- Dedicate varying width along Highway 10 (56 Avenue) for the existing sidewalk.
- Dedicate 0.952 metres along the Lane for the ultimate 8.0 metre Commercial/Industrial Lane Standard
- Dedicate 5.0 x 5.0 metre corner cut at the intersection of Highway 10 (56 Avenue) and 176A Street.
- Dedicate 3.0 x 3.0 metre corner cut at the intersection of Highway 10 (56 Avenue) and Lane.
- Dedicate 3.0 x 3.0 metre corner cut at the intersection of 56A Avenue and 176A Street.
- Dedicate 1.0 x 1.0 metre corner cut at the intersection of 56A Avenue and Lane.
- Provide a 0.5 metre Statutory Right-of-Way along 56A Avenue and along 56 Avenue for the portion fronting the heritage wall.

Works and Services

- Construct a curb bulge along 56A Avenue at the intersection of 176A Street.
- Construct 8.0 metre lane to Commercial/Industrial Lane Standard.
- Construct driveway letdowns along 176A Street and 56A Avenue to City Standards.
- Provide water, storm, and sanitary service connections to service the development.
- Provide on-lot source controls in accordance with the Cloverdale-McLellan ISMP.
- Provide a water quality/sediment control inlet chamber as a component of the on-site drainage system.
- Abandon all existing service connections no longer required or are greater than 30 years.
- Abandon all existing mains within the east/west lane to be consolidated with the development and within E1986-1636.

A Servicing Agreement is required prior to Subdivision.

DEVELOPMENT PERMIT

There are no Engineering requirements related to the Development Permit beyond those noted above.

DEVELOPMENT VARIANCE PERMIT

There are no Engineering requirements related to the Development Variance Permit beyond those noted above.



Rémi Dubé, P.Eng.
Development Services Manager
CE4

NOTE: Detailed Land Development Engineering Review available on file



Advisory Design Panel Minutes

2E - Community Room B
City Hall
13450 - 104 Avenue
Surrey, B.C.
THURSDAY, JANUARY 26, 2017
Time: 4:03 pm

Present:

Chair – Leroy Michelson
M. Enns
M. Lesack
D. Nelson
A. Scott
R. Solivar
D. Staples

Guests:

M. Yip, PMG Landscape Architects
S. Bouchard, Quadra Homes
W. Friesen, Points West Architecture
H. Bronson, Points West Architecture
Robert Salikan, Salikan Architecture Inc.
David Jerke, Van der Zalm & Associates Inc.
Sao Sim Kuan and David Tam
P. Joyce, City of Surrey
E. Ng, HCMA Architecture and Design
S. Rothnie, HCMA Architecture and Design
J. Cutler, space2place
M. Frappier, acdf* architecture/design
urbain/interior
R. Virani, Avanni Investment Group
D. Stoyko, Connect Landscape Architecture
S. Bose, Architecture 49
A. Page, Core Project Management
J. Henderson, Core Project Management

Staff Present:

M. Rondeau, Acting City Architect,
Planning & Development
N. Chow, Urban Designer, Planning &
Development
L. Luaifoa, Legislative Services

B. NEW SUBMISSIONS

3. **5:30 PM**

File No.:	7916-0431-00
New or Resubmit:	New
Last Submission Date:	N/A
Description:	Development Permit to allow for a 1,277 square metre addition to the Surrey Museum, a Development Variance Permit to reduce the required number of on-site parking spaces from 182 to 99 and a Heritage component for the relocation of Anderson Cabin (on Heritage Inventory) on the site and a subdivision for consolidation.
Address:	17675 – No. 10 (56 Avenue) Highway 17735 – No. 10 (56 Avenue) Highway 5642-176A Street 17671- No. 10 (56 Avenue) Highway 17710-56A Avenue, Cloverdale
Developer:	Peter Joyce, City of Surrey
Architect:	Stuart Rothnie, HCMA Architecture and Design
Landscape Architect:	Jeff Cutler, space2place
Planner:	Jennifer McLean
Urban Design Planner:	Mary Beth Rondeau

The Urban Design Planner provided the following comments:

- Staff has no specific comments or issues but note that a significant parking reduction is being requested.

The **Project Architect** presented an overview of the site plan, building plans, elevations, cross sections, and streetscapes and highlighted the following:

- The project is an expansion of the existing museum which is comprised of three different elements; two expansions and other site improvement.
- The expansions will repair the organization of the public realm and create a civil identity as the current building is challenged by architectural form and does not represent a museum.
- The expansion to the north of the site will provide space for an exhibit space, children's gallery, a feature gallery and space for public events. This space will also include a new reception area and administrative offices.
- One of the key components is the accommodation of three heritage sites to create a heritage park.
- The current storage facility will be taken down and added to the museum.
- Most of landscape work is marked for the second phase.
- The primary treatment around the building will be lawn and retained trees, heavy timber boardwalk as the first phase of construction.
- Where possible, trees will be transplanted and new trees planted.

The **Landscape Architect** responded to questions and clarifications.

ADVISORY DESIGN PANEL STATEMENT OF REVIEW

Development Permit to allow for a 1,277-square metre addition to the Surrey Museum, a Development Variance Permit to reduce the required number of on-site parking spaces from 182 to 99 and a Heritage component for the relocation of Anderson Cabin (on Heritage Inventory) on the site and a subdivision for consolidation.

File No. 7916-0431-00

It was
Moved by R. Solivar
Seconded by D. Nelson
That the Advisory Design Panel (ADP)
recommends A – that the applicant address the following issues to the satisfaction of the Planning & Development Department.
Carried

STATEMENT OF REVIEW COMMENTS

The Panel supported the overall proposal with recognition of good moves for the master planning, building siting and architectural resolution.

Building Form and Character

- Simple form and is a nice balance to the existing building.
- Like the use of the dark brick.
- The west elevation could be opened up to be lighter.

(The West Elevation is to be animated by museum banners for featured exhibitions and programs. The brick will also feature corbelling and racking for visual relief. This wall cannot have glazing for programmatic reasons, and the project masterplan calls for an extension to the building at this location in the next phase hence the elevation is kept simple.)

Landscaping

- Concern with the “link” between buildings as it seems to cut the site in half. Consider ensuring visual and physical east/west connection. If “link” building is necessary consider two distinct precincts on site (east and west); each with their own character, materials and uses.

(Well noted - the “link” will be fully glazed for visual connection, while large double doors with automatic operators on either side of the “link”, centered on the main East-West axis and walkway provide physical connection. Two distinct precincts as suggested are indeed proposed: the East precinct is focused on Heritage and the West precinct is focused on events, particularly those with synergies with Cloverdale Library and Surrey Archives. The distinction is part of the masterplan with landscape scope in the next phase of the project.)

- Concern with berm on south side (56 Avenue). The berm weakens the public presence along that street and gives the “back of the house” type of feel.

(This concern is well noted - the “berm” is not part of the current project scope and remains a concept in the masterplan, which will be developed in the next phase. The design intent will be to have a landscape element, not necessarily a berm, with the following features: a) informal seating facing Veterans’ Square b) a clear element along the length of 56 Avenue to visually identify the museum in its new branding c) provide integrity to complete all sides of Veterans’ Square d) provide a sense of intrigue and curiosity for events within Veterans’ Square to vehicles passing by as opposed to being a hard visual barrier.)

- Suggest the majority of pedestrian access of parking be to the east and not from 56 Avenue.

(Provided.)

- Ensure the big ideas of program find their way to the detailed level of material.

(Provided with visual emphasis on the Nail laminated timber roof, and detailing around the brickwork.)

- Suggest more visual/drawing development for how the proposed spaces support the museum expansion.

(This was presented briefly and also shown on drawings, to summarize: the East precinct focuses on Heritage Education with strong connections to the Children's Gallery on the East; the West precinct focuses on Special Events with strong connections to the Foyer and Feature Gallery on the West, which will hold community events and traveling exhibitions; and finally the existing museum building on the South will host programs, permanent collections and staff functions, all of which connect back to the central reception.)

- More articulation would be encouraged for the re-use of existing trees, ground plant materials, hierarchy of open spaces and program elements.

(All landscaping elements proposed in the current project are kept consistent with the principles established in the masterplan, including those noted by the Advisory Design Panel, which will be more fully implemented in the next phase.)

CPTED

- No comments provided specific to CPTED.

Accessibility

- Recommend lobbying TransLink for accessible transit by the site.

Sustainability

- No comments pertaining to Sustainability.

(the "City")

DEVELOPMENT VARIANCE PERMIT

NO.: 7916-0431-00

Issued To: CITY OF SURREY

(the "Owner")

Address of Owner: 13450 - 104 Avenue
Surrey, BC V3T 1V8

1. This development variance permit is issued subject to compliance by the Owner with all statutes, by-laws, orders, regulations or agreements, except as specifically varied by this development variance permit.
2. This development variance permit applies to that real property including land with or without improvements located within the City of Surrey, with the legal description and civic address as follows:

Parcel Identifier: 024-0740-551

Parcel A (Reference Plan 1914A) South West Quarter Section 8 Township 8 New Westminister District

17675 – No. 10 (56 Ave.) Hwy

Parcel Identifier: 008-351-431

Lot 11 Block 15 Section 8 Township 8 New Westminister District Plan 2107

17735 – No. 10 (56 Ave.) Hwy

Parcel Identifier: 017-339-774

Lot B (BE64647) Block 15 Section 8 Township 8 New Westminister District Plan 2107

5642 - 176A Street

Parcel Identifiers: 024-874-116, 024-874-124, 024-874-132, 024-874-141 and 024-874-159
Lots 12 - 16 Block 1 Section 8 Township 8 New Westminister District Plan 668

17671 – No. 10 (56 Ave.) Hwy

Parcel Identifier: 025-960-024
Lot 1 Section 8 Township 8 New Westminster District Plan BCP11474

17710 - 56A Avenue

(the "Land")

3. (a) As the legal description of the Land is to change, the City Clerk is directed to insert the new legal description for the Land once title(s) has/have been issued, as follows:

Parcel Identifier:

- (b) If the civic address(es) change(s), the City Clerk is directed to insert the new civic address(es) for the Land, as follows:

4. Surrey Zoning By-law, 1993, No. 12000, as amended is varied as follows:
- (a) In Table C.1 Part 5 Off-Street Parking and Loading/Unloading, the minimum number of off-street parking spaces is reduced from 182 to 104.
5. This development variance permit applies to only that portion of the buildings and structures on the Land shown on Schedule A which is attached hereto and forms part of this development variance permit. This development variance permit does not apply to additions to, or replacement of, any of the existing buildings shown on attached Schedule A, which is attached hereto and forms part of this development variance permit.
6. The Land shall be developed strictly in accordance with the terms and conditions and provisions of this development variance permit.
7. This development variance permit shall lapse if the Owner does not substantially start any construction with respect to which this development variance permit is issued, within two (2) years after the date this development variance permit is issued.
8. The terms of this development variance permit or any amendment to it, are binding on all persons who acquire an interest in the Land.

9. This development variance permit is not a building permit.

AUTHORIZING RESOLUTION PASSED BY THE COUNCIL, THE DAY OF , 20 .
ISSUED THIS DAY OF , 20 .

Mayor – Linda Hepner

City Clerk – Jane Sullivan

SURREY'S HERITAGE EVALUATION WORKSHEET

Address	6022 – 176 Street
Local Area	Cloverdale
Building / Natural Feature	Anderson Cabin
Construction Date	c. 1872
Current Owner / Applicant	City of Surrey
Evaluated By	Hugh McLean
Inspection Date	September 1997

SECTION 1 - HISTORY (If Known)

Excellent = 9-10 Very Good = 7-8 Good = 5-6 Fair = 3-4 Poor = 1-2

<p>Historical Context / Association with Events/People: Eric Anderson was born in Sweden in 1852. He was a sailor on a British sailing ship when, in 1872, it put into what is now Vancouver harbour for repairs, after a battering trip around the Horn. We may suppose the ship discharged cargo at New Westminster harbour first and the young man heard of land open for pre-emption in the area. Tired of rigorous life at sea under a harsh captain, he must have dreamed of beginning a new life for himself as a farmer. His chance came when he and two others were put ashore to cut timbers for repairing the ship. Supplied with an axe, gun and ammunition, the three sailors trekked through the bush to the town of New Westminster where they paid an Indian to take them across the Fraser River in a canoe. Once on the other side of the River, they felt free from any chance of capture. The young men separated and Anderson established himself on the bank of a small creek, still called Anderson Creek, where he hewed logs with the axe he brought from the ship and built him a small cabin. He walked to Chilliwack District and got a job on the farm of a Mr. Wells where he worked for three years, returning to his own place from time to time to do the work necessary to retain claim on his land. During this time, he met a young widow with two children and they were married in 1877 by the pioneer Presbyterian Missionary Reverend Alexander Dunn. In March 1877, the Andersons left Chilliwack and headed for their new home, the cabin in Surrey. Years of hard work followed, but the Andersons were happy. Two sons and a daughter were born to them. Eric built a fine new home and the original cabin was used to house poultry and pigs. Later, when farm buildings were added, the cabin stood empty. Sarah Anderson died in 1901 and Eric married again sometime later. Two more children, boy and a girl, followed with the second marriage to Sigvorn. Eric died in 1911 and the old homestead was sold after the death of Sigvorn in 1921. The farm changed hands several times. The Wasnaars, the Cambrays and the Frews owned it until it was sold to Mr. and Mrs. William Sweet in 1967. In 1970, the Sweets donated the cabin to the Surrey Museum.</p> <p>Cultural Association / Community Importance: From a cultural and community perspective, the Anderson Cabin is one of the most significant early buildings in Surrey. It is associated with many pioneers, who contributed to the earliest growth and development of Surrey, and it is preserved as a historic monument to the memory of all the pioneers.</p>	10 /10
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<p>Ownership / Construction History: Eric Anderson built the Cabin around 1872. A lean-to was added in 1876. He built a new home and used the Cabin to house poultry and pigs. Out-buildings were eventually built for farming purposes and the Cabin stood vacant for many years. After he died in 1911, Sigvorn Anderson (nee Sigurdson), Eric's second wife, ran the farm until 1921 when she rented it to John Hopper. She died the following year, on November 29th, and the land was sold to the Wasnaars and changed hands several times since. The Cambrays, and then the Frews, owned it until Mr. and Mrs. Sweet purchased the property in 1967. Recognizing the historical value of the Cabin, the Sweets donated it to the Museum to be preserved as a historic monument to all pioneers.</p> <p>Designer / Builder (If Known):</p>	10 /10
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**SECTION 2 - SITE AND CONTEXT
EVALUATION**

Excellent = 7-8 Very Good = 5-6 Good = 3-4 Fair = 1-2 Poor = 0

<p>Landmark Value: The cabin has a prominent position at the entrance to the Museum, although it is set back from 60 Avenue and it is not visible from 176 Street.</p>	<p>6 /8</p>
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Excellent = 4 Very Good = 3 Good = 2 Fair = 1 Poor = 0

<p>Context / Streetscape: It is totally out of context, having been moved from its original location in Surrey Centre into the Museum. The surroundings consist of a large unattractive parking lot and the museum and other buildings on the Fairgrounds site such as Shannon Hall.</p>	<p>1 /4</p>
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<p>Setting / Site Description and Features: From an educational/museum context, the setting is very appropriate and it allows the general public to tour and appreciate its significance. However, the surrounding parking lot is not very attractive or appropriate, and the concrete block museum building is also unattractive and uninspiring. The Cabin could have been placed in a more attractive setting with some more thought and resources, particularly landscaping and interpretive signage.</p>	<p>1 /4</p>
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<p>Environmental Value: This rating is not applicable.</p>	<p>N / A</p>
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**SECTION 3 - ARCHITECTURAL
EVALUATION**

Excellent = 9-10 Very Good = 7-8 Good = 5-6 Fair = 3-4 Poor = 1-2

<p>Stylistic Influence: Unstyled log cabin</p>

<p>Design Features: <i>Exterior (Cladding, Trim, etc.):</i> The massing is a side gabled box, one storey, one room measuring 20' x 13'. <i>Windows - Type, Assembly:</i> There are no windows. <i>Roof Type, Material:</i> The roof is pitched covered with hand split cedar shakes.</p>	<p>8 /10</p>
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<p>Unique Details or Design Elements: <i>Exterior:</i> The Cabin features vertical weatherboard on gable ends, notched corners, and chinking. <i>Interior:</i> Not assessed.</p>	<p>7 /10</p>
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SECTION 4 - PHYSICAL ASSESSMENT

Excellent = 9-10 Very Good = 7-8 Good = 5-6 Fair = 3-4 Poor = 1-2

<p>Architectural Integrity: <i>Exterior:</i> The integrity is intact and the building has been well restored and maintained. <i>Interior (Where Applicable):</i> Not assessed.</p>	<p>10 /10</p>
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Excellent = 5 Very Good = 4 Good = 3 Fair = 1-2 Poor = 0

<p>Architectural Condition: <i>Foundation / Structure:</i> The foundation is concrete, and since it was poured when the cabin was moved to the Museum site, it remains in excellent condition. The foundation would have originally been stone or none at all. The structure is hand-hewn logs, in very good condition.</p>	<p>4 /5</p>
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Excellent = 5 Very Good = 4 Good = 3 Fair = 1-2 Poor = 0

<p>Additions / Alterations: The only apparent change to the building is the new foundation.</p>	<p>5 /5</p>
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SECTION 5 - USABILITY

Excellent = 9-10 Very Good = 7-8 Good = 5-6 Fair = 3-4 Poor = 1-2

<p>Compatibility With Context: The Cabin is entirely compatible with the museum, and benefits from being set in its own context as a feature heritage building. However, the surrounding parking lot is not very attractive or appropriate, and the concrete block museum building is also unattractive and uninspiring. The Cabin could have been placed in a more attractive setting with some more thought and resources, particularly landscaping and interpretive signage.</p>	<p>6 /10</p>
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<p>Potential for Use / Future Use (If Known): The potential for continued use as a public building is excellent. At this time, there are no plans for redevelopment</p>	<p>10 /10</p>
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SECTION 6 - OVERALL HERITAGE SIGNIFICANCE

Summary and Recommendations:

The Cabin has mainly historical significance, although its lack of any architectural style is what makes the building unique. It is recommended for addition to the Heritage Register.

Note: Heritage Designation By-law, 1982, No. 7289, protects a corner of the larger property on which the Museum is located, and the Cabin is located immediately next to the Museum. It would be presumed that the Cabin is also protected. However, comparing the legal description contained in the By-law with an air photo, it reveals that the Cabin is located outside this protected area. The By-law only protects the 1881 Municipal Hall located inside the Museum. It may be worth re-considering the portion of protected property to include the Cabin.

Documentation and Reference Material:

Surrey's Heritage, Volume 1. Site visit and photographs. Heritage Designation By-law 1982, No. 7289.

TOTAL: 78/96¹
or 81%

¹ Where information is not available or not applicable, scoring must be left blank and the maximum of 100 must be discounted by an equivalent amount. A site may be considered for the Heritage Register with a score of at least 70/100 or equivalent percentage if discounted.

CITY OF SURREY

BY-LAW NO.

A by-law to enter into a heritage revitalization agreement
.....

WHEREAS:

- A. The Council may by by-law pursuant to Part 15 of the Local Government Act, R.S.B.C. 2015, Chapter 1, as may be amended from time to time, enter into a heritage revitalization agreement with the owner of heritage property;

- B. The Council considers that certain lands, premises and improvements have *heritage value* and *heritage character* and ought to be conserved, which are situate within the City and described as:

Parcel Identifier: 025-960-024
 Lot 1 Section 8 Township 8 New Westminster District Plan BCP11474

17710 - 56A Avenue

(the "Lands");

- C. The owner of the Lands and the City of Surrey have agreed on the nature, character and extent of the *heritage value* and *heritage character* of the Lands and on the nature, extent and form of conservation necessary to protect the *heritage value* and *heritage character* of the Lands;

NOW THEREFORE, the City Council of the City of Surrey, in open meeting assembled, enacts as follows:

- 1. The City is authorized hereby to enter into that certain Heritage Revitalization Agreement appended to this By-law as Schedule "I" (the "Heritage Revitalization Agreement") in respect of the Lands.

- 2. The Mayor and the City Clerk are authorized on behalf of the Council to sign the Heritage Revitalization Agreement and to register a notice on the title of the Lands.

- 3. Schedule "I" forms a part of this By-law.

4. This By-law may be cited for all purposes as "City of Surrey Heritage Revitalization Agreement By-law, 20 , No._____."

PASSED FIRST READING on the ____ day of _____.

PASSED SECOND READING on the ____ day of _____.

PASSED THIRD READING on the ____ day of _____.

RECONSIDERED AND FINALLY ADOPTED, signed by the Mayor and Clerk, and sealed with the Corporate Seal on the ____ day of _____ 20 .

_____ MAYOR

_____ CLERK

SCHEDULE "I"

[To City of Surrey Heritage Revitalization Agreement By-law, 20 , No._____]

HERITAGE REVITALIZATION AGREEMENT

This Agreement made the ___ day of _____, 20

BETWEEN:

CITY OF SURREY,
as represented by Realty Section, Engineering Department
13450 104 Avenue
Surrey, British Columbia V3T 1V8

(the "Owner")

OF THE FIRST PART

AND:

CITY OF SURREY, a municipal corporation,
and having offices at 13450 104 Avenue
Surrey, British Columbia V3T 1V8

(the "City")

OF THE SECOND PART

WHEREAS:

- A. The Owner is the registered owner in fee simple of the following lands and premises situate in the City of Surrey, British Columbia and described as:

Parcel Identifier: 025-960-024
Lot 1 Section 8 Township 8 New Westminster District Plan BCP11474

17710 - 56A Avenue

(the "Lands");

- B. The improvements and features on the Lands identified in and described by text, photographs, plans and drawings attached to this Agreement as Appendix "A" (the "Conservation Plan") and Appendix "B" (the "Relocation and Restoration Plan") as the Anderson Cabin (the "Anderson Cabin") are considered by the City and the Owner to have *heritage value* and *heritage character*;
- C. The Anderson Cabin is listed on the Surrey Heritage Register and the Owner and the City desire to conserve the *heritage value* and *heritage character* of the Anderson Cabin;
- D. For the purpose of conservation of the *heritage value* and *heritage character* of the Anderson Cabin, the Owner and the City have agreed to enter into this Agreement

setting out the terms and conditions of continuing protection for the *heritage value* and *heritage character* of the Anderson Cabin;

- E. The Owner has agreed to the terms for compensating the City for the loss in *heritage value* in accordance with Section 2(f) of this Agreement in the event the Anderson Cabin is moved or destroyed other than through natural causes or those causes outside of the Owner's reasonable control; and
- F. All provisions of this Agreement applicable to the Lands also apply to the Anderson Cabin.

NOW THEREFORE THIS AGREEMENT WITNESSES that in consideration of the mutual premises of the parties hereto and for other good and valuable consideration (the receipt and sufficiency of whereof is hereby by the parties acknowledged) the Owner and the City covenant and agree with one another pursuant to Section 610 of the Local Government Act, R.S.B.C. 2015, Chapter 1, as amended, re-enacted or consolidated from time to time and any successor statute (the "Local Government Act"), as follows:

Conservation Plan

- 1. (a) The Conservation Plan and the Relocation and Restoration Plan form part of this Agreement. To the extent that the text, photographs, plans and drawings constituting the Conservation Plan or Relocation and Restoration Plan require interpretation, the City shall be, in the first instance, the interpreter of the Conservation Plan and Relocation and Restoration Plan and shall determine the matter. If the Owner is dissatisfied with the City's interpretation, then Section 15 of this Agreement shall apply.
- (b) Part I of the Conservation Plan identifies, details and describes the character, extent and nature of the improvements and features on the Lands that have *heritage value* and *heritage character*. Part II of the Conservation Plan sets out the maintenance strategy, general standards and exemptions for the *conservation* and maintenance of all improvements and features on the Lands that have *heritage value* and *heritage character*. Part III of the Conservation Plan sets out the standards and specifications for relocation, restoration, rehabilitation, replication, repair, replacement or maintenance of the Anderson Cabin on the Lands to be undertaken and completed pursuant to this Agreement, including, but not limited to: foundation; building envelope and exterior; and roof structure and cladding.

Owner's Obligations to Protect, Conserve, Maintain and Rebuild

- 2. The Owner covenants and agrees that:
 - (a) The Anderson Cabin shall not be *altered*, including alterations required or authorized by this Agreement, except as agreed to in writing by the City.
 - (b) Each action of relocation, restoration, rehabilitation, replication, repair, replacement or maintenance required by Parts I, II, and III of the Conservation Plan and the Relocation and Restoration Plan shall be commenced and completed in accordance with the timing, phasing, standards and specifications set out the Conservation Plan.

- (c) The Anderson Cabin shall be maintained to the minimum standards and in accordance with the guidelines and requirements set out in the Conservation Plan and the Relocation and Restoration Plan.
- (d) In the event the Anderson Cabin is damaged, other than through causes outside of the Owner's reasonable control, the Owner of the Lands accepts the obligation to undertake all necessary construction to restore the damaged portion or portions of the Anderson Cabin to its original condition. The Owner is required to apply for and to hold a heritage alteration permit specifying the measures to be taken to restore the damaged portion or portions of the Anderson Cabin. The heritage alteration permit shall be subject to review and approval by the Heritage Advisory Commission. The restoration of the Anderson Cabin shall reflect the character-defining elements and design components including, but not limited to: location on public land; simple form, modest scale and rectangular plan massing; hand-hewn square logs with notched corners and chinking; large original growth logs; gabled roof, clad with split cedar barn shakes; and lapped wooden siding in gable ends, all as subject to approval by the City Architect or designate.
- (e) In the event the Anderson Cabin is destroyed, other than through natural causes or those causes outside of the Owner's reasonable control, the Owner of the Lands accepts the obligation to undertake all necessary construction to create a replica of the Anderson Cabin. The Owner is required to apply for and to hold a heritage alteration permit specifying the measures to be taken to create a replica of the Anderson Cabin. The heritage alteration permit shall be subject to review and approval by the Heritage Advisory Commission. The construction of the replica of the Anderson Cabin shall reflect the character-defining elements and design components as described in Section 2(d), all as subject to approval by the City Architect or designate.
- (f) In the event that the Anderson Cabin is destroyed, other than through natural causes or those causes outside of the Owner's reasonable control, in addition to the construction of a replica described in 2(e), the Owner covenants and agrees to compensate the City for the loss in *heritage value* to the community in the amount of \$28,890.65 indexed to the Vancouver Consumer Price Index (CPI) with 2017 being the base year, except that if the Anderson Cabin is destroyed through natural causes, including but not limited to, flood, earthquake and accidental fire as determined by the City in its sole discretion, and a replica is constructed by the Owner that is acceptable to the Heritage Advisory Commission or any successor decision making body in its sole discretion, then payment of compensation by the Owner to the City is not required.
- (g) The Owner shall do or cause to be done all such things, and shall take or cause to be taken all such actions, as are necessary to ensure that the restrictions and requirements set out in Parts II and III of the Conservation Plan and in the Relocation and Restoration Plan are fully observed, and the Owner shall not do, cause or allow to be done anything that would be in breach of the restrictions and requirements of this Agreement.

- (k) Where required by the City in a heritage alteration permit, the Owner shall provide security to the City to guarantee the performance of the terms, requirements and conditions contained in the Conservation Plan and the Relocation and Restoration Plan.
- (l) The Owner may apply to the City for funding including, but not limited to, monies for exemption from taxes, or any provision for assistance as specified in Section 25 of the Community Charter, S.B.C. 2003, c.26 (the "Community Charter").

Variations to By-laws

- 3. Not applicable to the Lands.

Construction and Maintenance

- 4. Wherever under this Agreement the Owner relocates, restores, rehabilitates, replicates, repairs, replaces, maintains or in any way alters the Anderson Cabin or constructs or maintains other works to protect or conserve the Anderson Cabin, all such work shall be done at the Owner's sole expense strictly in accordance with the Conservation Plan and the Relocation and Restoration Plan and as agreed by the City in writing and all improvements or features shall be diligently and continuously maintained in good repair and efficient operating condition by the Owner at the Owner's sole expense in accordance with good engineering, design, heritage and conservation practice.

No Liability to City

- 5. In no case shall the City be liable or responsible in any way for:
 - (a) any personal injury, death or consequential damage of any nature whatsoever, howsoever caused, that may be suffered or sustained by the Owner or by any other person who may be on the Lands; or
 - (b) any loss or damage of any nature whatsoever, howsoever caused to the Lands or any improvements or personal property thereon belonging to the Owner or to any other person,

arising directly or indirectly from compliance with the restrictions and requirements of this Agreement, wrongful or negligent failure or omission to comply with its restrictions and requirements, or refusal, omission or failure of the City to enforce or require compliance by the Owner with the restrictions or requirements or with any other term, condition or provision of this Agreement.

Reasonable Care and Risk

6. The Owner shall at all times, in complying with the restrictions or requirements of this Agreement, take reasonable care not to injure any person or cause or allow damage to any property, and shall take reasonable care not to cause, suffer, permit or allow any condition to exist that might reasonably lead to, cause or result in injury to any person or property including persons and property on adjacent lands. It shall be the sole responsibility of the Owner to comply and maintain compliance with the restrictions and requirements in a safe manner, and without reasonably foreseeable risk to person or property. Compliance with the restrictions and requirements in this Agreement shall be at the sole and exclusive risk and cost of the Owner.

Modification

7. If, in fulfilling its responsibilities and obligations pursuant to this Agreement, the Owner perceives or becomes aware of any unreasonable risk of injury to persons or damage to property or other potential loss that cannot be reasonably avoided, alleviated, reduced or eliminated except by measures that would be a breach of the restrictions, requirements of this Agreement, the Owner shall notify the City in writing of the nature and extent of the risk and of the measures proposed by the Owner to be undertaken at its sole cost to reduce, alleviate, avoid or eliminate the risk. Risk shall remain with the Owner.

Indemnity

8. The Owner shall at all times indemnify and save harmless the City and its elected and appointed officials, employees, contractors and agents of and from all loss and damage, and all actions, claims, costs, demands, expenses, fines, liabilities and suits of any nature whatsoever by whomsoever brought for which the City shall or may become liable, incur or suffer by reason of existence and effect whether direct or indirect of the restrictions or requirements of this Agreement, or breach or non-performance by the Owner of any covenant, term or provision hereof, or by reason of any work or action of the Owner in performance of this Agreement, or by reason of any work or action of the Owner in performance of its obligations, or by reason of any wrongful act or omission, default or negligence of the Owner.

Alternative Remedies

9. Any performance by the City pursuant to a statutory right to perform the obligations of an Owner arising out of this Agreement may be exercised fully in accordance with the Local Government Act and the Community Charter, and shall be without prejudice to any and all other remedies at law and equity available to the City, and no reference in this Agreement to, or exercise of any specific right or remedy by the City, shall preclude the City from exercising any other right or remedy.

Damages

10. The Owner covenants and agrees that the measure of damages for any breach of the restrictions or requirements of this Agreement shall include, but shall not be limited to, the actual cost and expense of all administration, labour, materials, equipment, services and work required for all remedial acts necessary to fully restore, rehabilitate, replace, repair or maintain the Anderson Cabin. The nature and extent of any breach of the said restrictions and requirements, and the nature and extent of any relocation, restoration, rehabilitation, replacement, maintenance or remedial work or action of any nature required to remedy such breach shall be determined by the City by reference to the Conservation Plan and the Relocation and Restoration Plan, and Sections 2 and 4 of this Agreement.

No Waiver

11. No restrictions, requirements or other provisions in this Agreement shall be deemed to have been waived by the City unless a written waiver authorized by resolution of the Council and signed by an officer of the City has first been obtained, and without limiting the generality of the foregoing, no condoning, excusing or overlooking by the City on previous occasions of any default nor any previous written waiver shall be taken to operate as a waiver by the City of any subsequent default or in any way to defeat or affect the rights of remedies to the City.

Statutory Authority and Proprietary Rights

12. Nothing in this Agreement shall limit, impair, fetter, or derogate from the statutory powers of the City all of which powers may be exercised by the City from time to time and at any time to the fullest extent that the City is enabled and no permissive by-law enacted by the City, or permit, license or *approval*, granted, made or issued hereunder, or pursuant to statute, by the City shall stop, limit or impair the City from relying upon and enforcing this Agreement.

Compliance with Laws

13. Despite any provision of this Agreement, the Owner shall comply with all laws, including by-laws of the City and all regulations and orders of any authority having jurisdiction, and to the extent only that such laws, regulations and orders are mandatory and necessarily require the breach of any restriction or positive obligation of this Agreement to be observed or performed by the Owner, or less than strict compliance with the terms hereof, then the Owner upon sixty (60) days' written notice to the City shall be excused from complying with such restrictions or performing such obligation and such restriction or obligation shall be suspended but only to the extent and for the time that such mandatory law, regulation or order is inconsistent with compliance with the said restrictions or obligations.

Notice

14. Any notice to be given under this Agreement shall be in writing and may be either delivered personally or sent by prepaid registered mail and if so mailed shall be deemed to have been given five (5) days following the date upon which it was mailed. The address of the parties for the purpose of notice shall be as follows:

If to the City:

Attention: City Clerk
CITY OF SURREY
13450 104 Avenue
Surrey, British Columbia V3T 1V8

If to the Owner:

Attn: Realty Section, Engineering Department
CITY OF SURREY
13450 104 Avenue
Surrey, British Columbia V3T 1V8

Any party may at any time give notice in writing to the other of any change of address and after the third day of giving of the notice, the address specified in the notice shall be the address of the party for the giving of notices.

If title to the Lands is transferred to a new Owner, the new Owner shall provide notice in writing to the City within 15 days of such a transfer providing the name of the new Owner, the contact for notice if it is different than the Owner and the new Address to which notices are to be sent.

Arbitration

15. The Owner, if dissatisfied with the City's interpretation of the Conservation Plan and the Relocation and Restoration Plan and any determination pursuant to Section 1(a) of this Agreement may require that the matter be decided and determined by binding arbitration as follows:
- (a) The Owner must, within thirty (30) days of any exercise of discretion by the City, give notice to the City of its intention to dispute and in such notice shall name a member in good standing of the Architectural Institute of British Columbia who has agreed to act as an arbitrator;
 - (b) The City shall within thirty (30) days of receipt of the aforesaid notice either accept the Owner's arbitrator, or name another with the same qualifications willing to act, and shall give notice of the same to the Owner;
 - (c) Where each of the Owner and the City have named an arbitrator, the two arbitrators shall within thirty (30) days of the City's notice pursuant to Section 15(b) appoint a third arbitrator having the same qualifications and the three arbitrators shall decide the dispute;
 - (d) Where the City accepts the arbitrator first selected by the Owner, that arbitrator shall act as a single arbitrator and forthwith decide the dispute;

- (e) Any arbitrator's decision in respect of the exercise of discretion by the City shall be final, conclusive and binding on all parties;
 - (f) The arbitrator shall award the prevailing party full compensation for all costs and expenses of the arbitrator, costs and fees of the proceedings and solicitor-client costs and expenses; and
 - (g) The arbitrator shall issue a final decision regarding the dispute within twenty-five (25) business days after the arbitrator's appointment, subject to extension of that time by agreement of the parties.
16. Without limiting the City's power of inspection conferred by statute and in addition to that power, the City shall be entitled at all reasonable times and with reasonable notice to enter onto the Lands from time to time for the purpose of ensuring that the Owner is fully observing and performing all of the restrictions and requirements in this Agreement to be observed and performed by the Owner, and wherever possible, when an inspection of the Lands is undertaken, the City shall provide reasonable notice to the Owner.

Headings

17. The headings in this Agreement are inserted for convenience only and shall not affect the construction of this Agreement or any of its provisions.

Schedules

18. All schedules to this Agreement are incorporated into and form part of this Agreement.

Number and Gender

19. Whenever the singular or masculine or neuter is used in this Agreement, the same shall be construed to mean the plural or feminine or body corporate where the context so requires.

Interpretation

20. Terms used in this Agreement that are italicized are defined in the Local Government Act, and the Heritage Conservation Act, R.S.B.C. 1996, Chapter 187, as amended, re-enacted or consolidated from time to time and any successor statute, and shall take their meaning from those Acts.

Successors Bound

21. All restrictions, rights and liabilities imposed upon or given to the respective parties under this Agreement shall extend to and be binding upon their respective heirs, executors, administrators, successors and assigns. When the Owner is more than one party they shall be bound jointly and severally by the terms, covenants and agreements on the part of the Owner.

Notice to be Filed

22. Notice of this Agreement and amendments to it will be filed in the Land Title Office and once filed, this Agreement and amendments will be binding on all persons who acquire an interest in the Lands.

IN WITNESS WHEREOF the Owner and the City have executed this Agreement as of the date first above written.

CITY OF SURREY

CITY OF SURREY

Linda Hepner
Mayor

Jane Sullivan
City Clerk

Appendix "A"

CONSERVATION PLAN

PART I – HISTORICAL AND ARCHITECTURAL BACKGROUND

1. Description of Historic Place

The Anderson Cabin is a small single-storey log pioneer building, rectangular in plan with a side gable roof, currently located on the lands of the Surrey Museum, at 17710 56A Avenue.

2. Heritage Value of Historic Place

The Anderson Cabin is valued as Surrey's oldest extant pioneer building and is an outstanding example of an early pioneer shelter. The cabin measures six metres by four metres and is constructed of hand-hewn, original growth timber, illustrating the simple and challenging life of the early pioneers.

This log cabin is of significant value to the people of Surrey for its strong cultural association with the first pioneers of the area. Eric Anderson, who died in 1911 at the age of fifty-nine, built this log cabin by hand in approximately 1873. After he married Sarah Morrison in New Westminster in 1879, they built a larger farmhouse for their family and this cabin was used for livestock.

The heritage value for this modest dwelling also lays in its educational value as a pioneer structure. The cabin is an important cultural feature for the interpretation of Surrey's heritage to the public. It was donated to the City of Surrey in 1970 by Mr. and Mrs. William Sweet, who owned the farm where it was located and had the foresight to recognize the value of the cabin. In its current location, the cabin now serves as an illustration of pioneer domestic life and the home building resources and early technology of a pioneer settler's home.

It is also a fine example of an early commitment to preservation, both privately and by the local government. The heritage designation of this site demonstrates the commitment of the City of Surrey in recognizing and preserving its built and natural heritage.

3. Character Defining Elements

Key elements that define the heritage character of the Anderson Cabin include its:

- Location on public land;
- Simple form, modest scale and rectangular plan massing;
- Hand-hewn square logs with notched corners and chinking;
- Large original growth logs;
- Gabled roof, clad with split cedar barn shakes; and
- Lapped wooden siding in gable ends.

PART II – MAINTENANCE STANDARDS AND PERMIT APPROVALS

1. General

A. Requirement to Commence Renovations

Moving of the Anderson Cabin may commence any time following the adoption of the by-law to enter into this Agreement and authorized by the issuance of a building permit to move the cabin to a temporary shored up location on the parking lot of the Lands during the construction of the Surrey Museum expansion.

The permanent relocation and restoration of the Anderson Cabin, including works that are consistent with Part III – Restoration Standards and Specifications, must commence within 1 year of the final occupancy permit being issued for the Surrey Museum expansion.

B. Maintenance Strategy

The strategy to ensure ongoing conservation of the Anderson Cabin shall consist of a Maintenance Plan and a Funding Strategy.

The Maintenance Plan shall be prepared with input from a conservation architect or qualified heritage consultant that is acceptable to the City. Issues to be addressed in the Maintenance Plan include water penetration and damage from sun, wind, weather and animals. Maintenance includes, but is not limited to, painting or staining, sealing, weather-stripping and the like.

The Funding Strategy shall include, but is not limited to, whether or not the Owner intends to absorb all the costs, undertake fundraising or seek government financial incentives, including those available from the City.

The Owner shall submit a Maintenance Plan and Funding Strategy for review and approval by the General Manager, Planning and Development and , if deemed necessary by the General Manager, Planning and Development, the Heritage Advisory Commission within one (1) year of the adoption of a by-law to enter into this Agreement.

The Maintenance Plan and Funding Strategy for the Anderson Cabin shall include, but is not limited to, the following:

- (a) A description and a time schedule for the renovations, repair, and replacement of the exterior elements, *landscaping* or other identified works on the Lands that constitute the character-defining elements and as identified in Part III – Renovation Standards and Specifications;
- (b) A description and time schedule for the ongoing maintenance of the elements, *landscaping* or other identified works on the Lands and other relevant details. Maintenance includes: painting, staining and sealing of the exterior cladding and trims, weather stripping, re-roofing, replacement of windows, doors and exterior cladding or trims to match the existing materials;
- (c) Ongoing maintenance of *landscaping*;
- (d) A description of any matters noted in Part III – Renovation Standards and Specifications or in the plans attached to this Agreement as requiring further details; and

- (e) A financial plan detailing the funding for the renovation and maintenance outlined above, including corporate sponsorships, annual budgets by the Owner or tenant, applications for government grants, strata fees, and other relevant details.

C. Amending an Established Maintenance Strategy

An Owner may apply to the City to amend an existing Maintenance Plan and Funding Strategy. Any amendment is subject to approval by the General Manager, Planning and Development and, if deemed necessary by the General Manager, Planning and Development, the Heritage Advisory Commission.

2. Standards

The "Standards and Guidelines for the Conservation of Historic Places in Canada", established under the Historic Places Program or successor guidelines as may be approved by the City are to apply to all construction, maintenance, restoration or renovation works undertaken under Parts II or III on the Anderson Cabin.

3. Timing and Phasing

With respect to the phasing or timing of commencement or completion of action applying to the Lands, temporary relocation of the Anderson Cabin shall commence within 60 days following the adoption of a by-law to enter into this Agreement and be carried out pursuant to a building permit issued by the City authorizing the works. The Owner shall insure that the final relocation and restoration of the Anderson Cabin shall be completed within one year of the Surrey Museum expansion being completed and a final occupancy permit issued.

4. Heritage Alteration Permit(s) Approval

- A. Changes to the building, structure, exterior appearance of the Anderson Cabin, features on the Lands identified in the Conservation Plan, the Relocation and Restoration Plan or character-defining elements may require the Owner to apply for a heritage alteration permit or obtain approval from the City.

Proposed changes shall be referred to the Planning & Development Department of the City prior to the commencement of any work to determine if the changes require or do not require a heritage alteration permit.

- B. A heritage alteration permit may not be required for alterations including, but not limited to, the following:
 - (a) changes to the Conservation Plan or the Relocation and Restoration Plan that are considered by the City Architect to be minor in nature and not affecting the character-defining elements of the Anderson Cabin;
 - (b) restorations considered by the City Architect to be consistent with the original design, being made to replace stylistically foreign elements and done in consultation with an independent conservation architect or qualified heritage consultant acceptable to the City; or
 - (c) simple repair and maintenance of existing elements not affecting the *building* structure, exterior or interior appearance of the Anderson Cabin.

- C. A heritage alteration permit shall be required for all but minor alterations including, but not limited to, the following:
 - (a) changes to the Anderson Cabin structure;
 - (b) changes to the exterior appearance of the Anderson Cabin;
 - (c) replacement of existing elements and/or construction of additions to the Anderson Cabin;
 - (d) changes to the external appearance of the Anderson Cabin due to interior renovations.

If a heritage alteration permit is determined to be required, the Owner shall apply to the City for a heritage alteration permit before undertaking any of the works listed in this Section 4.C.

After the heritage alteration permit application is submitted, the heritage alteration permit will be considered for issuance by City Council upon the recommendation of the General Manager, Planning and Development and the Heritage Advisory Commission, or by a City official delegated by City Council.

5. **Building Permit Approval**

Construction, alterations or other actions to be authorized by a building permit shall be consistent with the provisions of the Conservation Plan, Relocation and Restoration Plan, and with heritage alteration permits sanctioning construction, alterations or other actions.

As the Anderson Cabin is recognized as a historic site, Building Code equivalencies may be used to lessen visual impacts on the historical appearance or authenticity of the *building*. To utilize Building Code equivalencies, the Owner shall retain a qualified architect that is acceptable to the City Architect.

6. **Conditions**

- A. The works specified in Part III and attachments to this Conservation Plan shall be supervised by a conservation architect or qualified heritage consultant acceptable to the City.
- B. The works specified in Part III and attachments to this Conservation Plan shall be approved by a conservation architect or qualified heritage consultant acceptable to the City prior to the City granting final building approval.

PART III – RESTORATION STANDARDS AND SPECIFICATIONS

1. Foundation:

See Section 5.2.i “Foundation Construction” of the Relocation and Restoration Plan.

2. Building Envelope and Exterior:

See Section 5.2.ii “Base Log Dutchman Repair” of the Relocation and Restoration Plan.

3. Roof Structure and Cladding:

See Section 5.2.iii “Eave Screening” of the Relocation and Restoration Plan.

4. Other:

The general intent is to promote restoration and retention of existing materials and elements wherever possible. If restoration is not feasible, replacements shall be constructed to match existing in terms of form, detailing and materials. Where original features have already been removed, altered or replaced by stylistically foreign elements, new replacements shall be consistent with the original design and done in consultation with an independent conservation architect or qualified heritage consultant acceptable to the City.

Minor changes to the provisions of Part III that do not affect the character defining elements or that improve the authenticity of the restorations, may be approved by the City Architect, in consultation with the Heritage Advisory Commission.

(Note: Terms used in Appendix "A" of this Agreement that are italicized are defined in Surrey Zoning By-law, 1993, No. 12000, as amended, and shall take their meaning from the By-law.)

Appendix "B"

**HERITAGE CONSERVATION PLAN
Anderson Cabin, 17710 56A Avenue, Surrey Relocation and Restoration Plan
McGinn Engineering & Preservation Ltd., March 2017**

(The "Relocation and Restoration Plan")

(Attachment beginning on the next page)



relocation & restoration plan

ANDERSON CABIN

SURREY, BC

JUNE 2017



TDM PROJECTS INC.
6420 Rosebery Ave.
West Vancouver, BC. V7W 2C6



McGinn Engineering &
Preservation Ltd.
Barry McGinn Architect.



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Mrs. Sheila Hill, Surrey Museum staff member next to Anderson Cabin, after the move to the Museum grounds in 1971



1 HERITAGE VALUE ASSESSMENT

1.1 History/Description of Anderson Cabin

The Anderson Cabin is a small single-storey log pioneer building, rectangular in plan with a side gable roof, currently located on the front lawn of the Surrey Museum.

1.2 Summary of Heritage Value

The Anderson Cabin is valued as Surrey's oldest extant pioneer building and is an outstanding example of an early pioneer shelter. The cabin measures six metres by four metres and is constructed of hand-hewn, original growth timber, illustrating the simple and challenging life of the early pioneers.

This log cabin is of significant value to the people of Surrey for its strong cultural association with the first pioneers of the area. Eric Anderson, who died in 1911 at the age of fifty-nine, built this log cabin by hand in approximately 1873. After he married Sarah Morrison in New Westminster in 1879, they built a larger farmhouse for their family and this cabin was used for livestock.

The heritage value for this modest dwelling also lays in its educational value as a pioneer structure. The cabin is an important cultural feature for the interpretation of Surrey's heritage to the public. It was donated to the City of Surrey in 1970 by Mr. and Mrs. William Sweet, who owned the farm where it was located and had the foresight to recognize the value of the cabin. In its current location, the cabin now serves as an illustration of pioneer domestic life and the home building resources and early technology of a pioneer settlers home.

It is also a fine example of an early commitment to preservation, both privately and by the local government. The heritage designation of this site demonstrates the commitment of the City of Surrey in recognizing and preserving its built and natural heritage.

1.3 Character Defining Elements

Key elements that define the heritage character of the Anderson Cabin include its:

- location on public land;
- simple form, modest scale and rectangular plan massing;
- hand-hewn square logs with notched corners and chinking;
- large original growth logs;
- gabled roof, clad with split cedar barn shakes; and
- lapped wooden siding in gable ends.

2 POLICY FRAMEWORK

2.1 Heritage Designation

Anderson Cabin is on the City of Surrey Heritage Register as Site # 153, with its significance noted as "Historical". Anderson Cabin is also recognized on the National Register of Canada's Historic Places.



2.2 Surrey Museum Expansion & Anderson Cabin Relocation

The Surrey Museum is expanding to offer improved facilities to residents and visitors to Surrey. The expansion includes an increased capacity of collections storage to store and document Surrey’s material collections, a fabrication workshop and humidity and climate controls to best preserve artifacts. A large atrium and moving walls are planned to encourage museum goers and the community alike to use the space for their leisure.






A rendering of the proposed Surrey Museum Expansion. The proposed location of Anderson Cabin is shown at the left. (rendering by HCMA Architecture + Design)




This proposed expansion will occupy the area where the Anderson Cabin currently sits. A new location, not far from its existing location, has been selected for Anderson Cabin that integrates it into landscape plan. The Cabin was moved from its original site to the old Museum site at 176th and 60th Ave in 1971 and then moved to its current site in 2005, on to a new foundation. The proposed location will keep Anderson Cabin on its current site and continue to serve its current function as a museum exhibit.





3 CONDITION ASSESSMENT

CONDITION ASSESSMENT TABLE			
REF.	ELEMENT	DESCRIPTION/CONDITION	IMAGE REFERENCE
	Form, Scale & Massing	The cabin retains its original massing, scale and form.	
	Log Construction	<p>One of the central character-defining aspects of this building is its robust log construction. The log joinery at the corners is very well executed and dovetail joints are tight and well seated. The logs have been squared. A base log at the front of the building is the only log showing deterioration. Although there is some expected drying and checking of the logs, they are in remarkably good condition after 144 years of service.</p> <p>The base log at the southwest corner of the cabin has a deteriorated base to it. A skilled log craftsman can execute a Dutchman repair along the base, thereby keeping this original log largely intact.</p>	 





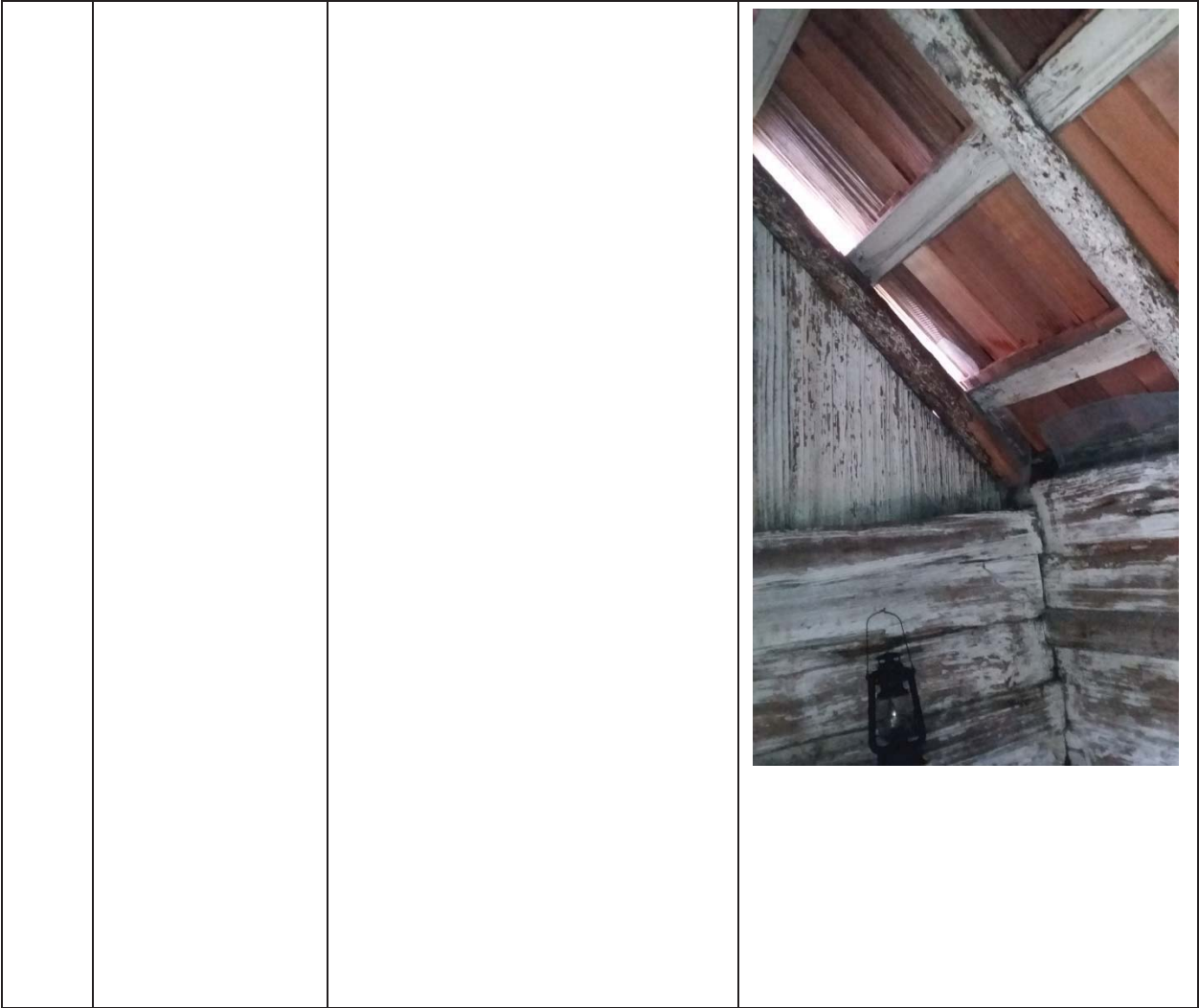
	<p>Concrete Chinking</p>	<p>The concrete chinking is cracking and, in some areas, has spalled away from the log joints. This chinking is likely to crack and spall during the move or craning operation, and a renewal of the concrete chinking is expected.</p>	
	<p>Roof Framing</p>	<p>The roof framing pole rafters, hewn collar tie and roof boards are all in excellent condition. Some scabbed board connectors have been added over time at the ridge. No apparent separation of the rafters at the ridge is visible.</p>	
	<p>Roof Cladding</p>	<p>The roof has recently been re-shingled, and is in excellent condition.</p>	



	<p>Gable Cladding</p>	<p>The vertical plank siding at the both gables is in fair condition. There is some discolouration & deterioration visible due to weathering but, given its function, this adds to the historic character of the building. There are no cladding boards that need to be replaced. The 2 x 6 nailers are in good condition.</p>	
	<p>Soffits</p>	<p>The original wood soffits are in good condition. The wall is open at between the purlins at the end walls and between the rafter beams at the eaves, and have been somewhat screened-in to control rodent entry. This screening has proven insufficient and a new edge screening consisting of three off-set layers of chicken wire fastened with close-spaced No. 8 wood screws and flat washers is recommended.</p>	



	<p>Floor Surface</p>	<p>The existing wood plank floor surface is in good condition.</p>	
	<p>Interior Finishes</p>	<p>The white-washed interior finish is peeling and deteriorated, but from a conservation standpoint, not important to renew. If original, it is likely a calcite paint.</p>	



4 PROPOSED CONSERVATION STRATEGIES

The Parks Canada Standards & Guidelines for the Conservation of Historic Places in Canada (2010) has been adopted by the City of Surrey, for the assessment of the treatment of historic places. Under the Standards and Guidelines, the conservation strategies proposed for the proposed restoration and relocation of Anderson Cabin may include aspects of preservation, restoration and rehabilitation, as defined below:

Preservation: The action or process of protecting, maintaining and/or stabilizing the existing materials, form and integrity of an historic place or of an individual component, while protecting its heritage value.

Restoration: The action or process of accurately revealing, recovering, or representing the state of a historic place, or of an individual component, as it appeared in a particular period in its history, while protecting its heritage value.



Rehabilitation: The action or process of making possible a continuing or compatible contemporary use of an historic place or an individual component, through repair, alterations, and/or additions, while protecting its heritage value.

In reality, all three of these conservation approaches will come into play in the proposed restoration, rehabilitation and relocation of Anderson Cabin.

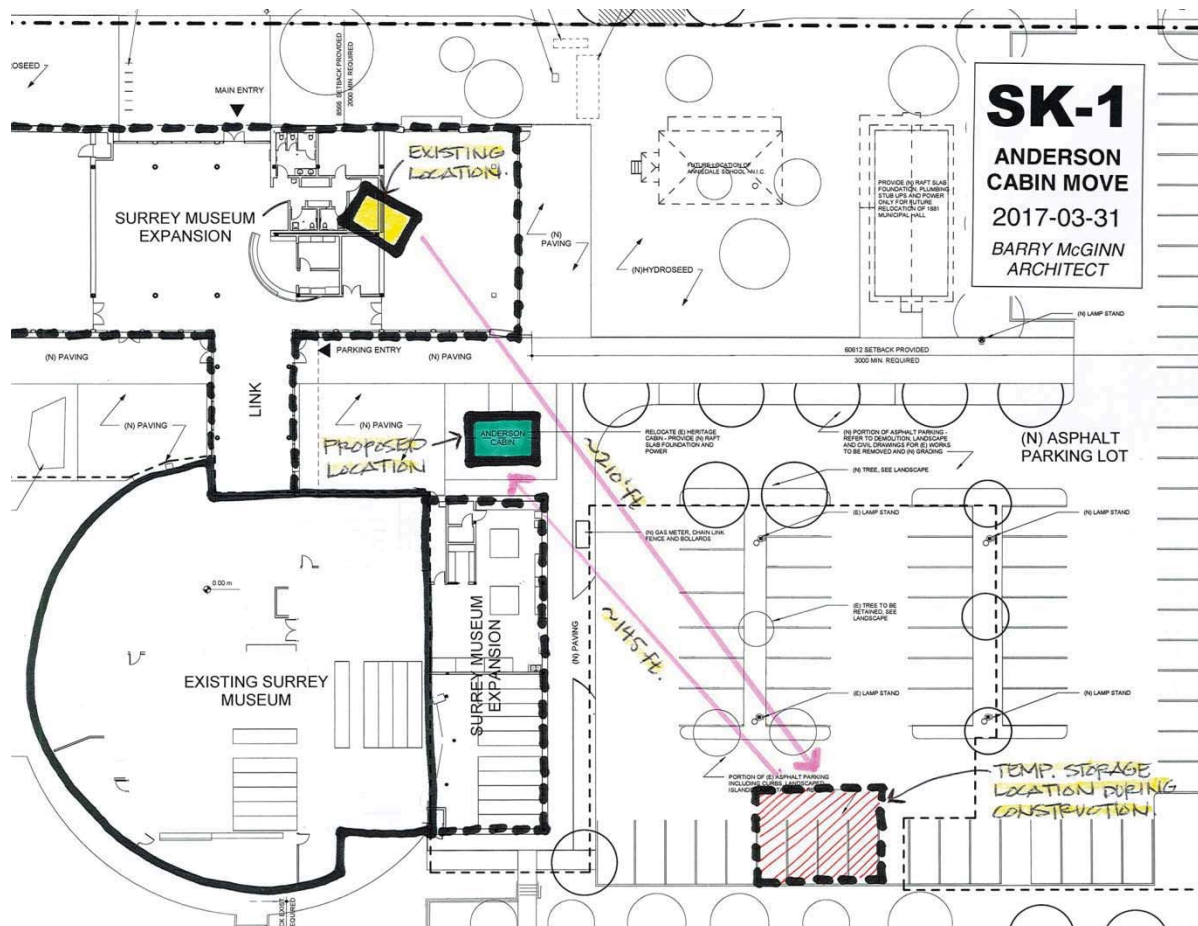
5 RELOCATION AND REHABILITATION PLAN

The Relocation Plan has been organized under the following sections:

- Building Move Strategy and Bracing for Move
- Upgrades and Rehabilitation at New Site

5.1 Building Move Strategy and Bracing for Move

The consulting team has coordinated with Nickel Bros. Moving Ltd. and GWL Industries Ltd. to investigate two move strategies, one being a translational move on move dolly's to a temporary shored up location on the parking lot, and then back again to the final site, and the other being a similar approach, except using a crane lift. These two approaches are outlined below.





Translational Move – Nickel Bros. Ltd.

The key elements of the log building bracing and move strategy are:

- i) **Temporary Bracing/Building Move** - The log structure would be temporarily braced as outlined in Drawings TA5-00 and TA5-01. This would involve temporary vertical exterior steel strapping on the exterior walls, interior corner bracing, and wire rope diagonal bracing between the corner braces in the horizontal plane at the top and bottom of the wall. Two large lateral steel I-needle beams would be inserted through the current knock-outs in the existing concrete perimeter foundation beneath hewn log joists to allow the building to be lifted free of the masonry. The log structure would then be dragged in the lateral direction onto an industrial moving dolly to the temporary parking lot storage area. The log cabin would be temporarily supported on wood blocking and 4"X8" timbers on the flat supporting the base logs. The shoring situation also provides ready access for the Southwest corner base log Dutchman Repair.
- ii) **New Foundation/Final Move** – A new concrete perimeter strip footing will be developed to provide full support to the log structure – Refer to Drawing PRA5-00. It will be detailed for an interior haunch to support new 4"X6" floor joists for support of the existing timber flank flooring. The new perimeter strip footing will be fitted with slots to allow the needle I-beams to slide into position and set the log structure on the new foundation. Then the needle beams can be extracted and the slots filled in with concrete.

Crane Move – GWL Industries Ltd.

The key elements of the log building bracing and move strategy are:

- i) **Temporary Bracing/Building Move** - The log structure would be temporarily braced as outlined in Drawings TA5-00 and TA5-01 and as described for the translational move. Two large lateral steel I-needle beams would be inserted through the current knock-outs in the existing concrete perimeter foundation beneath hewn log joists to allow the building to be lifted free of the masonry. A rectangular steel fabricated spreader bar assembly, pretty much the floor plate dimensions of the cabin, would need to be constructed and utilized for the cabin lift to ensure lift cables attached to the needle beams clear the building eaves. Additional steel braces between the needle beams will be needed to brace the 'cradle'. The log structure would then be lifted in the three separate moves (70' maximum radius for a 90 ton crane) to the temporary parking lot storage area. The log cabin would be temporarily supported on wood blocking and 4"X8" timbers on the flat supporting the base logs. The shoring situation also provides ready access for the Southwest corner base log Dutchman Repair. All tree obstructions will need to be removed for the crane move, which is apparently part of the museum addition project.



- ii) **New Foundation/Final Move** – A new concrete perimeter strip footing will be developed to provide full support to the log structure as noted for the translational move – Refer to Drawing PRA5-00.

5.2 Upgrades and Rehabilitation at New Site

i) Foundation Construction – Rough excavation and the new reinforced concrete foundation would be constructed, as per the preliminary schematic detail PRA5-00. A 2” ground slab is recommended, as well as four distributed bird-hole vents w/screens, two per long side. The perimeter foundation drain can be tied into the foundation drainage line for the adjacent new addition.

ii) Base Log Dutchman Repair – The base log Dutchman Repair can be completed at the storage area prior to the final move. This repair requires a careful cutting out of the base deterioration and gluing in a wood patch to the same exterior profile, generally, of the base log. Utilizing a log section for the patch works best.

iii) Eave Screening – Discreetly install 3 layers of folded chicken wire around the open beam and barge board areas, attached with No. 8 wood screws and flat washers at 12” o.c.

6 CONSERVATION STANDARDS

The following are the standards that define the principles of good conservation practice, and an assessment of how they relate to the proposed interventions for Anderson Cabin.

CONSERVATION STANDARD		CONSERVATION STRATEGY
GENERAL STANDARDS FOR ALL PROJECTS		
1	Conserve the heritage value of a historic place. Do not remove, replace or substantially alter its intact or repairable character-defining elements. Do not move a part of a historic place if its current location is a character-defining element.	Circumstances require the slight relocation of this historic cabin, but this was not the original location to begin with as it was moved in the 1970s and again in 2005, so its current location is not a character-defining element. Character-defining elements will remain intact or be repaired as required.
2	Conserve changes to a historic place, which over time, have become character-defining elements in their own right.	There has been very little alteration to Anderson Cabin over time. The roof was replaced in kind to improve the conditions of the interior and the current roof will remain.



3	Conserve heritage value by adopting an approach calling for minimal intervention.	Although this log structure could, conceivably, be dismantled as part of a move, this would compromise the character-defining elements and reconstruction would not allow it to be rebuilt exactly as it is existing. As such, the decision has been made to move the structure in as close to its current form as possible.
4	Recognize each historic place as a physical record of its time, place & use. Do not create a false sense of historic development by adding elements from other historic places or other properties, or by combining features of the same property that never coexisted.	Proposed interventions will comply with this standard, but also allow respectful rehabilitation to meet modern standards for its continued use as a historic and cultural destination.
5	Find a use for a historic place that requires minimal or not change to its character-defining elements.	The proposed continued use as a historic cultural attraction of its former pioneer cabin use respects this principle.
6	Protect, and if necessary, stabilize a historic place until any subsequent intervention is undertaken. Protect and preserve archaeological resources in place. Where there is potential for disturbance of archaeological resources, take mitigation measures to limit damage and loss of information.	The building is being moved to safe and secure location during the Surrey Museum expansion project and will be protected while in this location by construction fences. Once construction is complete, the proposed location is safe and secure, in a small side court area of the proposed Surrey Museum expansion.
7	Evaluate the existing condition of character-defining elements to determine the appropriate intervention needed. Use the gentlest means possible for any intervention. Respect heritage value when undertaking an intervention.	This report identifies deficiencies and proposed interventions which retain or restore character-defining elements, and allow respectful rehabilitation to allow continuing use.
8	Maintain character-defining on an on-going basis. Repair character-defining elements by reinforcing the materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving prototypes.	A maintenance plan in compliance with this standard will be implemented.
9	Make any intervention needed to preserve character-defining elements physically and visually compatible with the historic place and identifiable upon close inspection. Document any intervention for future reference.	This relocation plan will provide guidance on future interventions to ensure maintenance of the building's heritage character-defining elements. Implementation measures will allow for the oversight of an independent heritage consultant.
ADDITIONAL STANDARDS RELATING TO REHABILITATION		
10	Repair rather than replace character-defining elements. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them	The proposed intervention approach will comply with this Standard.



	with new elements that match the forms, materials, and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the historic place.	
11	Conserve the heritage value and character defining elements when creating any new additions to a historic place and any related new construction. Make the new work physically and visually compatible with, subordinate to, and distinguishable from the historic place.	No new additions are proposed at this time. It is recommended to not consider any new additions to this building in the future as it functions well as a stand-alone pavilion structure within the context of the Surrey Museum grounds.
12	Create any new additions or related new construction so that the essential form and integrity of a historic place will not be impaired if the new work is removed in the future.	No new additions are proposed at this time. It is recommended to not consider any new additions to this building in the future as it functions well as a stand-alone pavilion structure within the context of the Surrey Museum grounds.
ADDITIONAL STANDARDS RELATING TO RESTORATION		
13	<i>Repair rather than replace character-defining elements from the restoration period. Where character defining elements are too severely deteriorated to repair and where sufficient physical evidence exists, replace them with new elements that match the forms, materials, and detailing of sound versions of the same elements.</i>	The proposed intervention approach will comply with this Standard.
14	<i>Replace missing features from the restoration period with new features whose forms, materials and detailing are based on sufficient physical, documentary and/or oral evidence.</i>	The deteriorated southwest corner base log is to be repaired with a Dutchman Patch. Improved rodent screening is to be installed around the perimeter eave/barge board areas.

7 PRELIMINARY OPINION OF PROBABLE RELOCATION/CONSERVATION COSTS

Below is the summary chart from the Opinion of Probable Relocation/Conservation Costs for the translational move utilizing Nickel Bros. Moving Ltd. and the crane move, with costing from GWL Industries Ltd.

7.1. Translational Move/Conservation Costs		
7.1.1.	Move Costs (all inclusive budget quote from Nickel Bros)	\$50,000.00
7.1.2.	Temporary Bracing for Transport	\$5500.00
7.1.3.	Temporary Storage cribbing/supports	\$1900.00
7.1.4.	Structural – Excavation/fill, new foundation, perimeter drainage, floor beams/planks, ground slab, re-instate plank flooring and baseboard	\$9,000.00
7.1.5.	Demolition – Remove existing foundation, allow for 1 ½ bin @ \$600.00/bin	\$600.00
7.1.6.	Dutchman Repair of Base Log	\$1900.00
7.1.7.	Installation of perimeter rodent screen at eave/gable	\$1100.00
		\$70,000.00



7.2. Crane Move/Conservation Costs		
7.2.1.	Crane Time, 1 st move, 18 hrs. @ \$350.00/hr.	\$6300.00
7.2.2.	Rigger, 1 st move, 18 hrs. @ \$70.00/hr.	\$1260.00
7.2.3.	Spreader Bar and Set-up, needle beams & placement, needle beam braces, floor removal	\$6500.00
7.2.4.	Ground Plane Load Spreading for Crane Staging – timber planking	\$3500.00
7.2.5.	Crane Time, 2 st move, 18 hrs. @ \$350.00/hr.	\$6300.00
7.2.6.	Rigger, 2 st move, 18 hrs. @ \$70.00/hr.	\$1260.00
7.2.7.	Spreader Bar and Set-up, needle beams & placement, needle beam braces– 2 nd move	\$4500.00
7.2.8.	Ground Plane Load Spreading for Crane Staging – timber planking – 2 nd move	\$2500.00
7.2.9.	Temporary Bracing for Transport	\$5500.00
7.2.10.	Temporary Storage cribbing/supports	\$1900.00
7.2.11.	Structural – Excavation/fill, new foundation, perimeter drainage, floor beams/planks, ground slab, re-instate plank flooring and baseboard	\$9,000.00
7.2.12.	Demolition – Remove existing foundation, allow for 1 ½ bin @ \$600.00/bin	\$600.00
7.2.13.	Dutchman Repair of Base Log	\$1900.00
7.2.14.	Installation of perimeter rodent screen at eave/gable	\$1100.00
		\$52,120.00

8 FINAL RECOMMENDATIONS AND COSTS

As per the chart above, the preliminary opinion of rehabilitation costs are **\$70,000.00** for the translational move utilizing Nickel Bros. Moving Ltd. and **\$52,120.00** for the crane move, based on GWL Industries Ltd. budget quotation. Neither of these preliminary costs include a contingency allowance and a 20% contingency allowance is appropriate at this stage of the project.

It should be noted that the crane move represents considerably more risk than the translational move, both in physical risk to the resource (much can go wrong) and in the risk of scope creep and cost escalation. Nickel Bros. Ltd. are in the business of moving buildings, and actually moved the cabin to its current location, so we have more confidence in their budget quotation and capabilities for this work. We also feel there is some downward movement potential to Nickel Bros. Ltd. budget quotation once the City/contractor move toward contract negotiation and issuance.

Consequently, we recommend the City consider the translational move with Nickel Bros. Ltd. at the preliminary opinion of probable relocation cost of \$70,000.00.



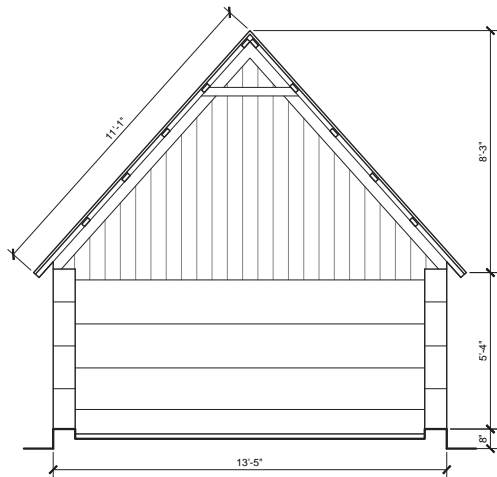
9 REFERENCES

Parks Canada (2010). Standards and Guidelines for the Conservation of Historic Places in Canada. Her Majesty the Queen in Right of Canada.

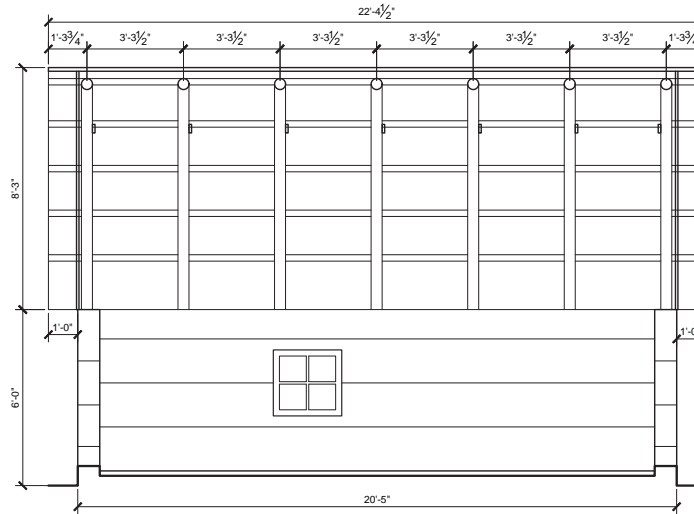
10 APPENDICES

10.1 Rehabilitation Drawings

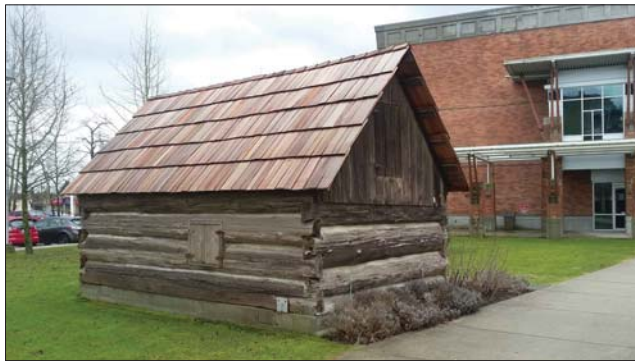
10.2 Translational Move and Crane Budget Quotations



1 SECTION 1
1/2" = 1'-0"

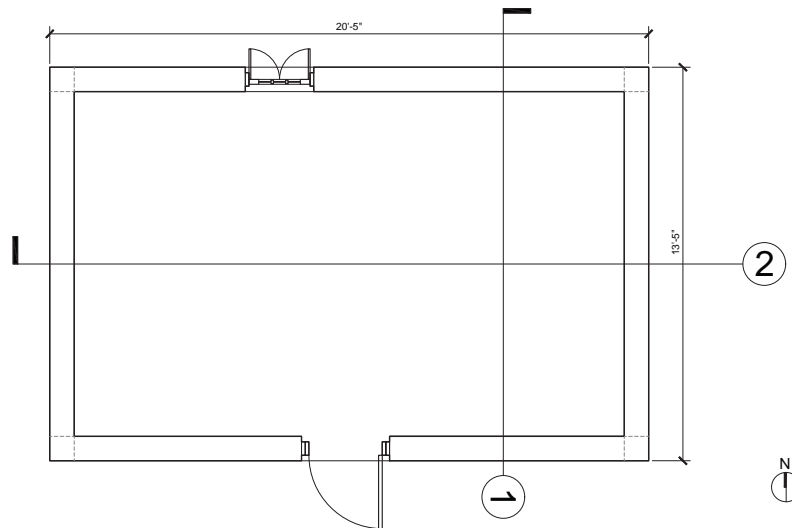


2 SECTION 2
1/2" = 1'-0"



ANDERSON CABIN

- 1A1-00 EXISTING PLAN AND SECTIONS
- 1A2-00 EXISTING ELEVATIONS
- TA5-00 TRANSPORT BRACING DETAILS
- TA5-01 TRANSPORT BRACING DETAILS
- TA5-02 SUPPORT AT TEMPORARY STORAGE
- PRA5-00 PROPOSED FOOTING AT NEW LOCATION



3 FLOOR PLAN
1/2" = 1'-0"

McGinn Engineering & Preservation Ltd.
Barry McGinn Architect.
#803-402 West Pender St. Vancouver, B.C.
Tel: 604-473-8866 Fax: 604-473-9877
Web: www.mcginn-engineering.com

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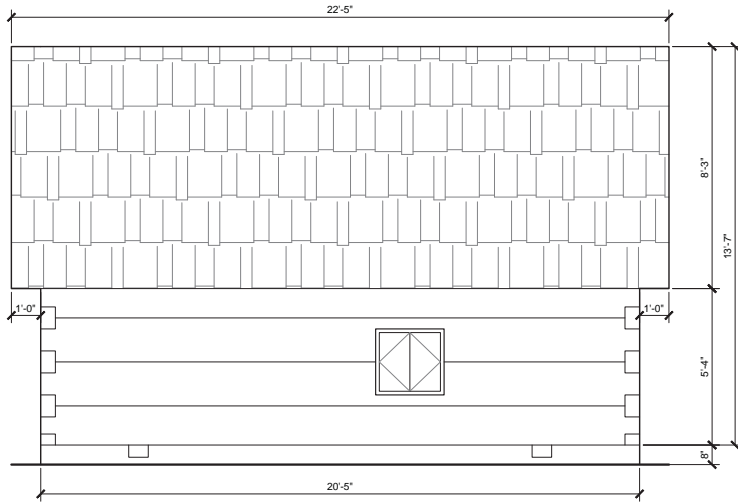
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ANDERSON CABIN REHAB. & RELOCATION
SURREY MUSEUM, SURREY, BC

TITLE:
EXISTING PLAN & SECTIONS

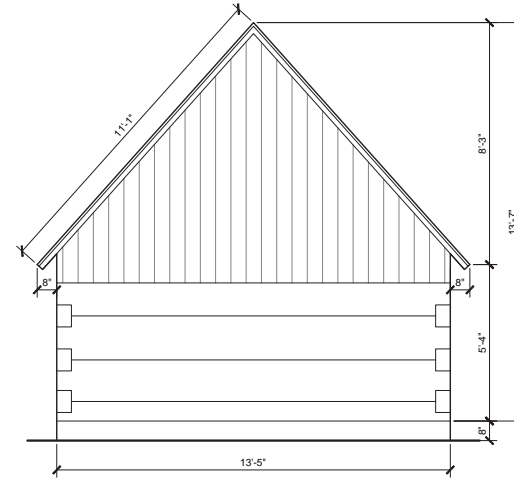
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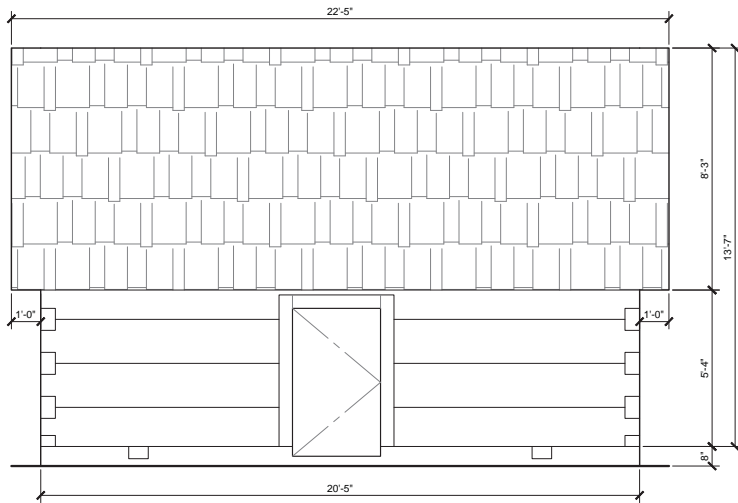
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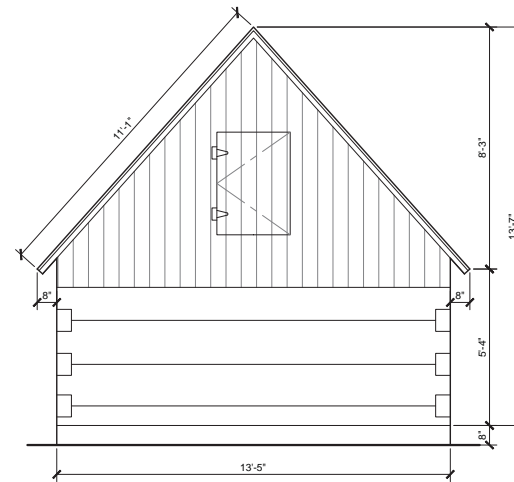
1 NORTH ELEVATION
1/2" = 1'-0"



2 EAST ELEVATION
1/2" = 1'-0"



3 SOUTH ELEVATION
1/2" = 1'-0"



4 WEST ELEVATION
1/2" = 1'-0"

REV.	DESCRIPTION	DATE

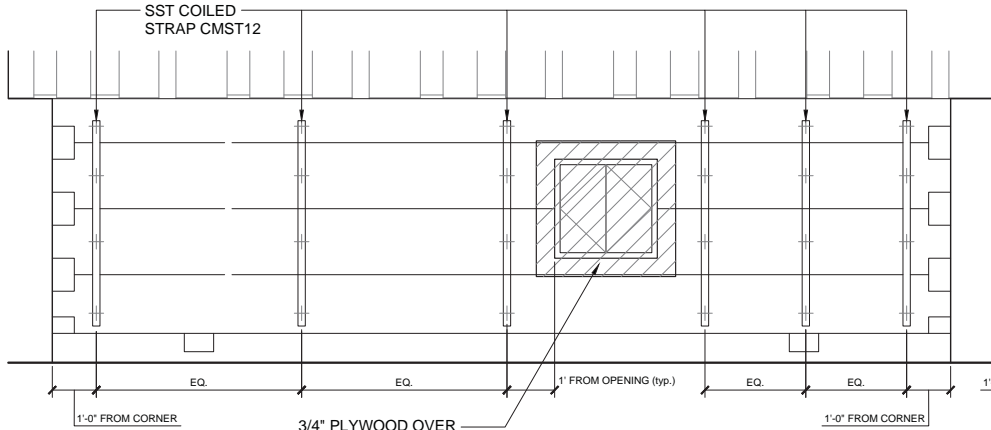
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**ANDERSON CABIN
REHAB. & RELOCATION**
SURREY MUSEUM, SURREY, BC

TITLE:
**EXISTING
ELEVATIONS**

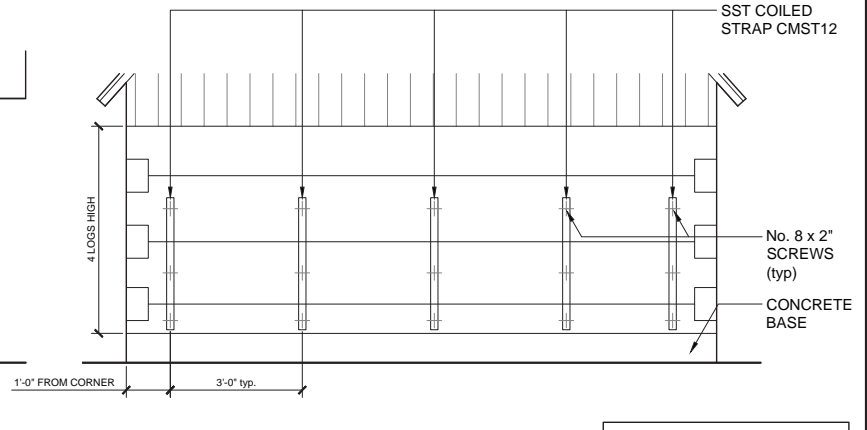
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DATE: **09/29/2017**
SCALE: **1/2" = 1'-0"**

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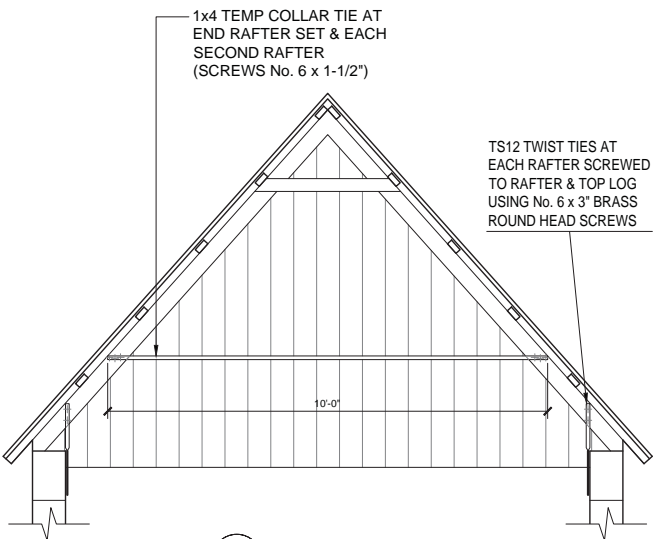


1 LOG STRAPPING: LONG WALLS
3/4" = 1'-0"

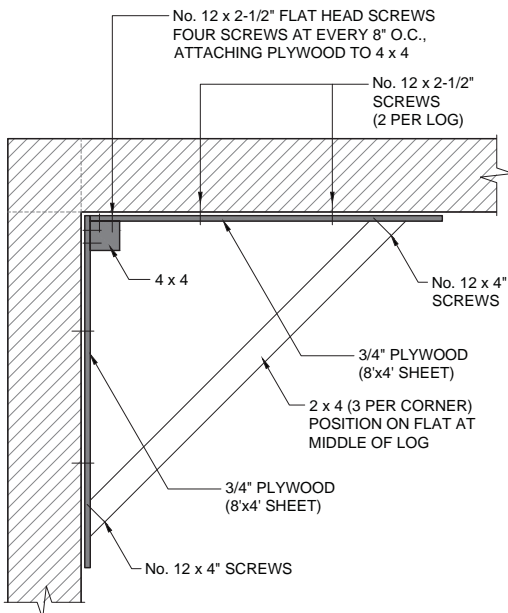


2 LOG STRAPPING: SHORT WALLS
EXTERIOR & INTERIOR
3/4" = 1'-0"

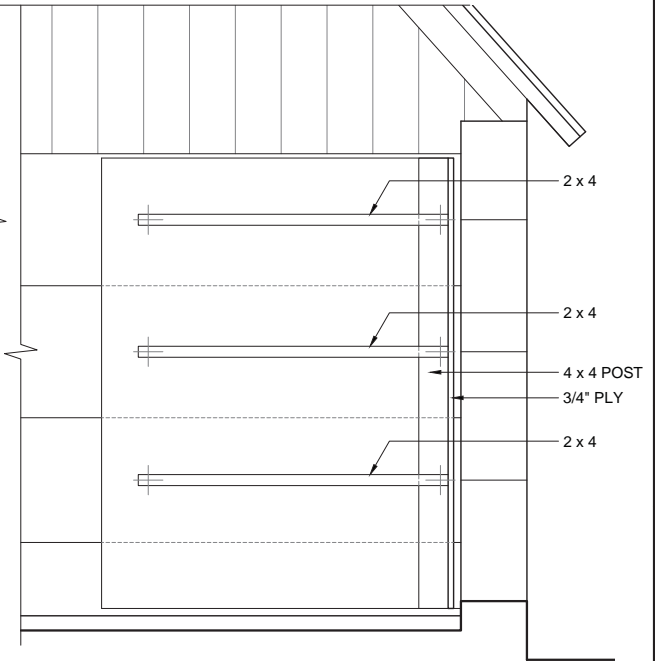
NOTE: STRAPPING TO BE INSTALLED ON ALL FACADES ON EXTERIOR, & ON THE SHORT WALLS (EAST & WEST) IN THE INTERIOR.



3 TEMPORARY COLLAR TIE
3/4" = 1'-0"



4 CORNER BRACE PLAN DETAIL
1 1/2" = 1'-0"



5 CORNER BRACE SECTION DETAIL
1 1/2" = 1'-0"

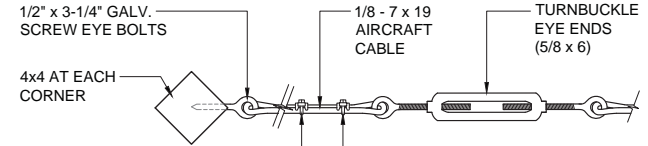
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ANDERSON CABIN
REHAB. & RELOCATION
SURREY MUSEUM, SURREY, BC

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TRANSPORT BRACING DETAILS

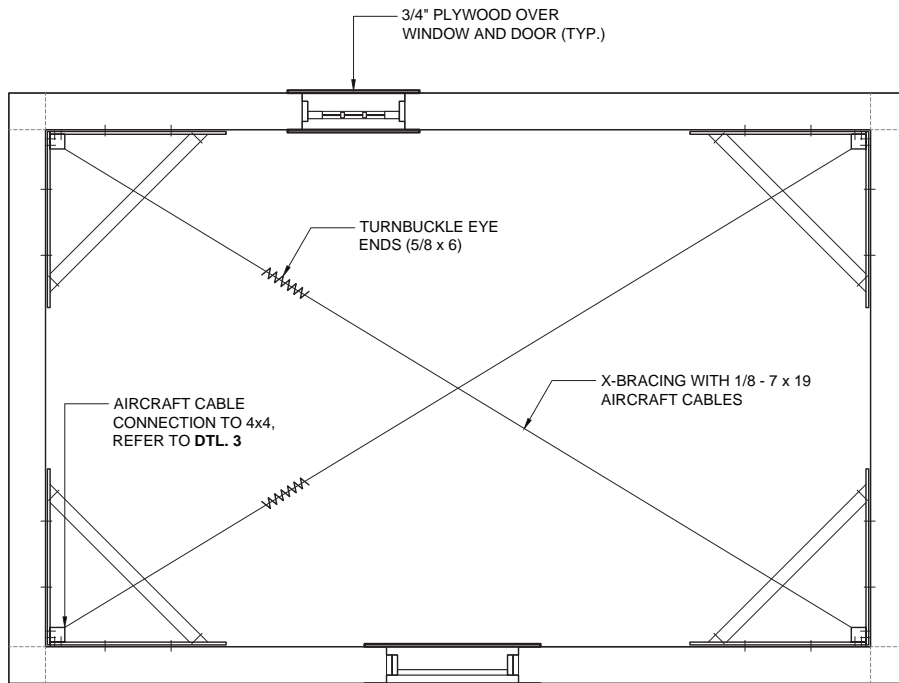
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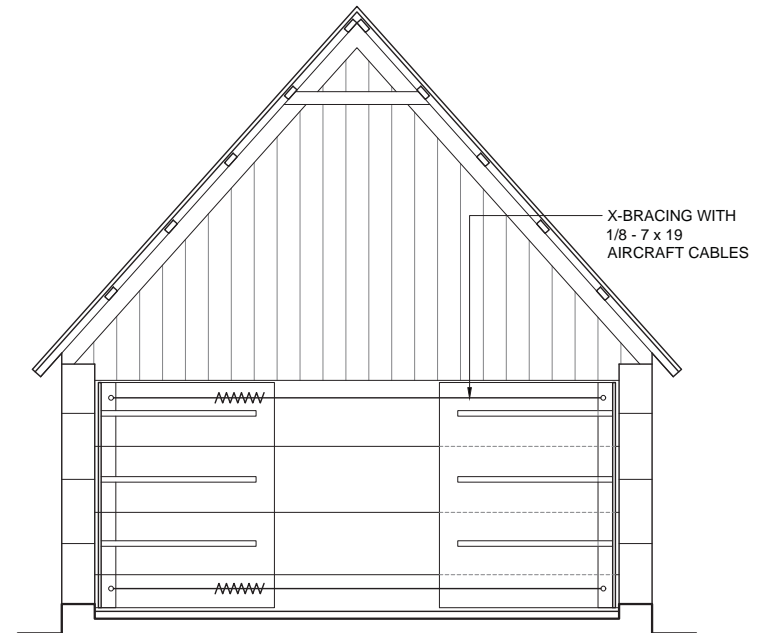
TWO CROSBY CLIPS. APPLY THE SECOND CLIP AS NEAR THE LOOP AS POSSIBLE. TIGHTEN NUTS EVENLY, ALTERNATING UNTIL REACHING THE DESIRED TORQUE. THE RETURN CABLE LENGTH SHOULD BE AT LEAST 4" min.

3 CROSS BRACING DETAIL
3" = 1'-0"



3/4" PLYWOOD OVER WINDOW AND DOOR (TYP.)

1 CROSS-BRACING: PLAN
3/4" = 1'-0"



2 CROSS BRACING: SECTION
3/4" = 1'-0"

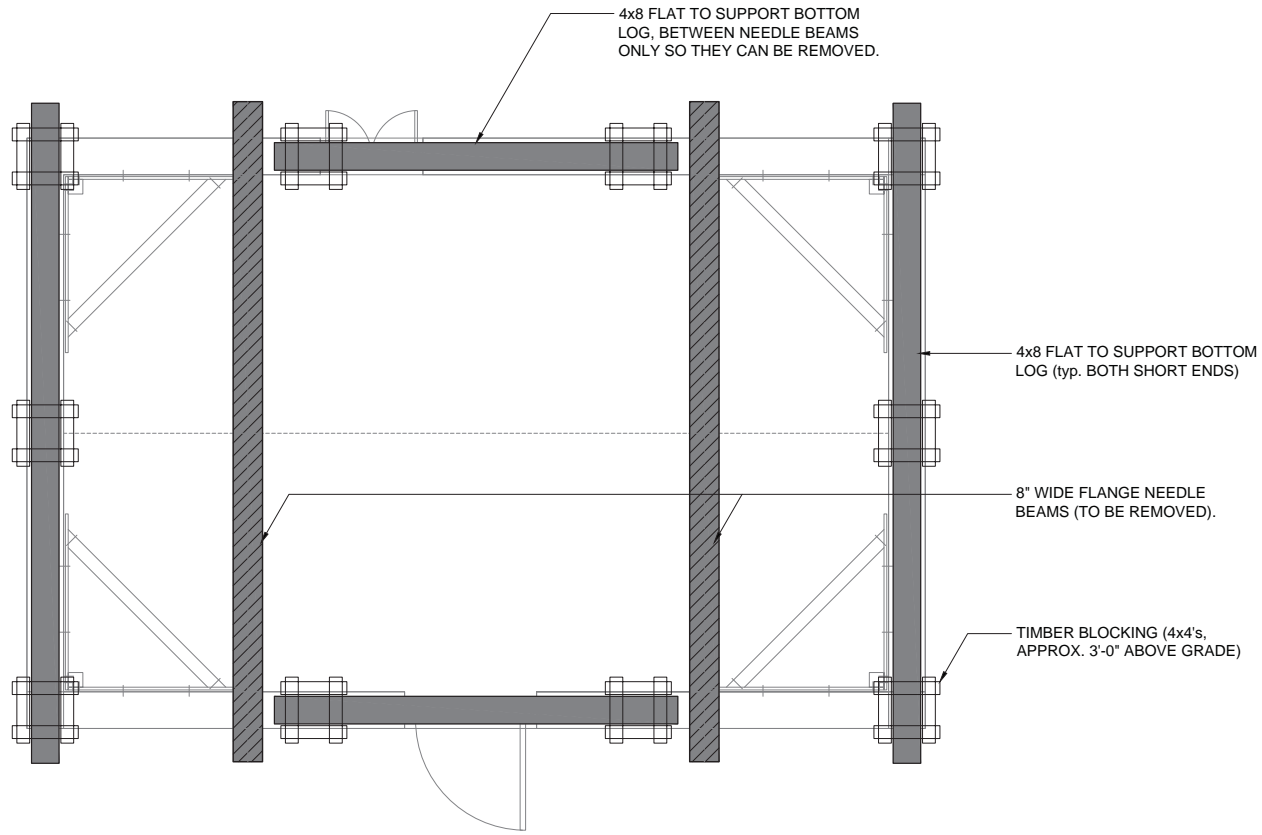
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SURREY MUSEUM, SURREY, BC

TITLE:
TRANSPORT BRACING DETAILS

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DATE: **03/29/2017**
SCALE: **3/4" = 1'-0"**

DWG#: **TA5-01**



1 SUPPORT AT TEMPORARY STORAGE
3/4" = 1'-0"

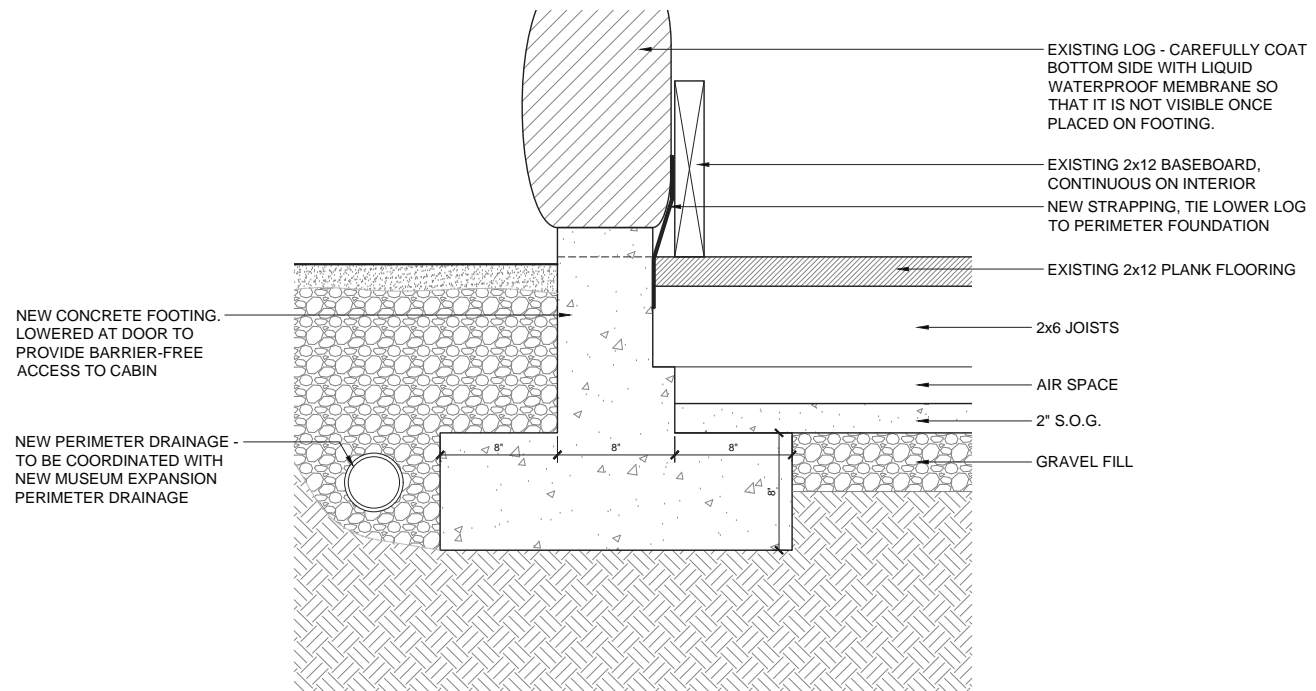
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REHAB. & RELOCATION**
SURREY MUSEUM, SURREY, BC

TITLE:
**SUPPORT AT
TEMP. STORAGE**

DRAWN BY: **KDV**
DATE: **09/29/2017**
SCALE: **3/4" = 1'-0"**

DWG#: **TA5-02**



1 PROPOSED FOOTING AT NEW LOCATION
3" = 1'-0"

REV.	DESCRIPTION	DATE

PROJECT:
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REHAB. & RELOCATION**
SURREY MUSEUM, SURREY, BC

TITLE:
**PROPOSED FOOTING
AT NEW LOCATION**

DRAWN BY: **KDV**
DATE: **03/29/2017**
SCALE: **3" = 1'-0"**

DWG#: **PRA5-00**



NICKEL BROS

March 14, 2017

TDM Projects Inc.
6420 Rosebery Avenue
West Vancouver, BC
V7W 2C6

Attention: Mr. Thomas Day

Tel: 604-277-2315

Email: tomalo@shaw.ca



Re: Lot moves on wheels at Surrey Museum, Heritage log cabin

Nickel Bros (NB) will provide all equipment, material, and labor to relocate the above-mentioned building in accordance with all relevant Health & Safety regulations including Nickel Bros own Safety in Motion (SIM) standards for lifting and moving buildings.

NB will install an engineered steel support frame and lift the building using a Unified Hydraulic Jacking System to ensure equal and level lift across the entire footprint of the building.

NB will then install hydraulic transporters under the building and move the building across the lot to the above-mentioned storage destination on the property.

NB will then return to site and move the building from storage and onto its new supports.

Our BUDGET pricing to move on wheels the above-mentioned building is as follows:

Completed for the BUDGET price of\$50,000.00 + GST

Safety. Precision. Integrity.

Vancouver:
1528 Broadway Street
Port Coquitlam, BC V3C 2M8
Toll-Free: 1.866.813.9430
Fax: 604.944.6082
E-mail: Vancouver@nickelbros.com

Vancouver Island:
1990 Balsam Road
Nanaimo, BC V9X 1T5
Toll-Free: 1.866.320.2268
Fax: 250.753.8215
E-Mail: vanisland@nickelbros.com

Seattle:
625 Riverside Road
Everett, WA 98201
Toll-Free: 1.866.920.2767
Fax: 425.257.2069
E-mail: Washington@nickelbros.com

Residential: www.nickelbros.com

Industrial: www.nickelbrosindustrial.com

Inclusions (Nickel Bros Responsibilities):

Administrative

- All business licenses

Insurance

- Cargo Insurance of \$300,000.00 for while the building is being moved and staged
- Riggers Insurance of \$300,000.00 for while the building is being lifted hydraulically
- General Liability Insurance of \$5,000,000.00
- Auto Liability Insurance of \$1,000,000.00
- Adding client as an additionally insured upon request

Health & Safety

- Workers Compensation coverage for all NB workers
- Working in accordance with all applicable health and safety regulations
- All workers certified in accordance with applicable Health & Safety requirements

Engineering

- Quality control oversight to ensure safe moving operations
- Arrangements for temporary shoring engineering as required

Building Move/ Lift/ Lower

- Labor, equipment and material required to move the building as described above
- Load building onto wheels
- Move the building across the lot
- Lower building onto new supports completed to NB satisfaction, ready to receive the building
- Remove all equipment and material from site, leaving a clean site

Exclusions (Customer Responsibilities):

Administrative

- All applicable taxes
- Providing Notice of Project as required to NB office 2 days prior to NB commencing work on site
- Allowing an NB signs to be placed on site for duration of project
- Providing NB with a copy of building/site plans showing current and final elevations of the building
- All cost associated with clearing over-head obstructions such as but not limited to tree limb, utility lines, etc.
- Cost of placing “no parking” signs, traffic plans and traffic control as/if required
- Obtaining all required permits and approvals to relocate the building including but not limited to:
 - Building permits as required
 - Demolition permits as required
 - Written permission to cross private and public property as required

Health & Safety

- **Providing Demolition permit, Hazmat Survey, and Clearance Letter for NB workspace to NB office 7 days prior to NB commencing work on site**
- Building security and weatherproofing as/if required
- Site security & fencing as/if required
- Ensuring all contractors/ workers/ guests on site during the lifting/ moving process or while building is supported on temporary shoring comply with relevant Health & Safety regulations
- Ensuring all contractors/ workers/ guests on site during the lifting/ moving process or while building is supported on temporary shoring do not impede, make contact with, or excavate near NB workers, equipment & shoring

Disconnects

- Disconnect all services and utilities to the building
- All pipes/ wires entering the structure must be cut prior to any work commencing by NB



De-Construction & Building Removal

- Remove basement walls as required for moving operation
- Removing obstructions in and out of the origin and destination site
- Removing trees and landscaping as required on both origin and destination site as required.
- Remove all masonry and concrete as required
- Remove tie downs that secure the building to the foundation
- Remove all building material and demolition debris potentially containing hazardous material from all NB work areas

Construction

- Repair floor joists as/if required prior to move
- Any bracing and temporary walls required to NB's satisfaction
- Install a 2x10" beams on all exterior walls to NB's satisfaction
- Road building required to move building to NB's satisfaction

Origin Site Preparations

- Identify and mark underground services where applicable.
- Site preparations required for NB to access site with heavy equipment
- Provide staging areas (as directed by NB) for crane access for steel beam loading and unloading
- Site work and roadwork required for move of building on site
- All site remediation as required

Lowering

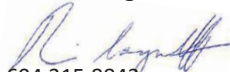
- Ensure new foundation/support systems are completed to adequately support the building while allowing for maneuvering and removal of all NB equipment from under building/ structure after it has been lowered
- Ensure adequate room and ground conditions around the perimeter of the building to allow the removal of all NB equipment
- Remove all obstacles and debris from around the building to allow the lowering and removal of NB equipment
- Ensure contractor is on site for the lowering procedure to ensure new foundation/ support system is adequate and to make adjustments as directed by mover
- Provide an excavator on site during the move process to assist NB equipment if required.

Please Note:

- Price is valid for 30 days and does not include applicable taxes.
- Price is subject to all NB standard contract terms & conditions.

Thank you for the opportunity to earn your business on this project.

Sincerely,
Dennis Langendorff


604.315.8843





March 15, 2017

Gwil Industries Inc.
5337 Regent Street
Burnaby BC V5C 4H4

TDM PROJECTS INC.
6420 Rosebery Ave.
West Vancouver BC
V7W 2C6

Tel (604) 291-9401
Fax (604) 291-2751

Attention: Thomas Day, M. Eng., P. Eng.

Re: Relocating log cabin structure weighing 8T in Surrey.

We are pleased to confirm our prices for the supply of the following cranes:

1. To supply a 60 ton hydraulic boom truck mount crane placing the cabin at a maximum radius of 40 ft.

Hourly Crane Rate fully operated.....	\$260
Rigger (if requested by customer)	\$70 per hour
Travel time charges at quoted hourly crane rate.....	one hour each way
Rigging cartage to and from site.....	\$400
Permit.....	\$65

2. To supply a 90 ton hydraulic boom truck mount crane placing the cabin at a radius beyond 40 feet up to 70 feet.

Hourly Crane Rate fully operated.....	\$350
Rigger (if requested by customer)	\$70 per hour
Travel time charges at quoted hourly crane rate.....	one hour each way
Rigging cartage to and from site.....	\$400
Permit.....	\$65

Overtime premium.....\$70 per hour per man
Overtime premium applies to all hours worked beyond eight, Monday through Friday plus all hours worked on Saturday, Sundays, and General holidays.

Free and easy access to the job site is to be provided at no cost to Gwil. The customer is to ensure that the supporting surface is capable of supporting all loads imposed by the cranes. All quotes are subject to no road weight restrictions being in effect. Traffic control is the responsibility of the customer. All engineering and lift procedures are subject to Gwil Crane's approval and acceptance.

GWIL will not be liable for:

- 1) loss, damage or delay resulting from inability to provide equipment and services quoted due to force majeure, including strike, lockout, civil commotion, governmental regulations or controls, acts of God, accident resulting in physical and mechanical damage to equipment or physical or mechanical breakdown of equipment;



- 2) consequential damages or economic loss, including overhead, standby charges, liquidated damages and loss of profit, related in any way to the provision of equipment and services as quoted or as agreed to be provided.

Federal, provincial, and municipal taxes are not included in quoted prices and will be charged extra at time of invoicing. Should our quote be successful please notify us as early as possible to ensure that the required equipment is available for your project. The terms of this quote will be incorporated into any purchase order, work order, contract or agreement between GWIL and the customer.

Accounts shall be due and payable 30 days after the invoice date (the "Due Date") and shall bear compound interest at the rate of 1.5% per month (19.56% per annum) from the Due Date until paid in full.

Please confirm your acceptance of this quote by signing and returning a copy by fax to 604-291-2751.

Yours truly,

Terry Gage
Sales Manager

Terms of quotation accepted and agreed:

Date: _____

Thomas Day, M. Eng., P. Eng.
TDM PROJECTS INC.

Per: _____

Name of Authorized Signatory

Signature

Present:

Councillor Woods, Chair
D. Evans
B. Hol
D. Plug
P. Priddy

Absent:

L. Tannen

Staff Present:

R. Gallagher, Parks, Recreation & Culture
D. Luymes, Planning & Development
A. Parsons, Planning & Development
P. Klassen, Parks, Recreation & Culture
L. Blake, Legislative Services

D. NEW BUSINESS**1. PLANNING & DEVELOPMENT**

**(a) Anderson Cabin (17710 - 56A Avenue)
Heritage Revitalization Agreement (HRA) Application
File: 6800-10**

Staff summarized the report dated May 12, 2017 regarding a Heritage Revitalization Agreement (HRA) application that has been received for the Anderson Cabin, located at 17710 56A Avenue.

Alec Page, Core Project Management, advised that the museum project consists of three phases: the construction of the museum; the expansion of the museum; and relocation of the Anderson Cabin. A Conservation Plan for the Anderson Cabin has been completed.

In response to questions from the Commission, staff provided the following information:

- The City of Surrey is the current owner of the cabin.
- An HRA is the best mechanism to legally attach a Conservation Plan to the building.
- Anniedale School and the 1881 City Hall both do not currently have an HRA; however, both are protected through a Heritage Designation Bylaw.
- A Conservation Plan was created for Anniedale School in association with a structural report that was completed for the building, commissioned and funded by the SHAC.
- Additional and alternative uses have been considered for all three heritage buildings. The City encourages community groups to utilize these buildings as affordable places to conduct meetings.

It was Moved by Commissioner Evans
Seconded by Commissioner Hol
That That the Surrey Heritage Advisory
Commission (SHAC) recommends that Council:

1. Receive the "**Anderson Cabin (17710 56A Avenue) Heritage Revitalization Agreement (HRA) Application**" report as information; and,
2. Recommend to the General Manager, Planning and Development Department that the Heritage Revitalization Agreement for the Anderson Cabin be forwarded to Council for consideration.

Carried