

NO: **R049**

COUNCIL DATE: **March 11, 2013**

REGULAR COUNCIL

TO: **Mayor & Council**

DATE: **March 11, 2013**

FROM: **General Manager, Planning and Development
General Manager, Engineering**

FILE: **6520-20 (Fleetwood
Enclave)**

SUBJECT: **Fleetwood Enclave Infill Area Concept Plan**

RECOMMENDATION

The Planning and Development Department and the Engineering Department recommend that Council:

1. Receive this report as information;
2. Approve the "Infill Area Concept Plan" for the Fleetwood Enclave Neighbourhood as contained in Appendix II;
3. Approve amendments to the Road Classification Map and Road Allowance Map in the Subdivision & Development By-law, 1986, No. 8830 to reflect the road network illustrated in the Infill Area Concept Plan as contained in Appendix II;
4. Approve amenity contributions as specified in the Infill Area Concept Plan contained in Appendix II to support community amenities and services that benefit the Fleetwood Enclave neighbourhood;
5. Instruct the City Clerk to bring forward the necessary by-law to amend Surrey Zoning By-law, 1993, No. 12000 (the "Zoning By-law") to include amenity contributions for the Fleetwood Enclave Infill Area Concept Plan all as documented in Appendix III; and
6. Instruct the City Clerk to bring forward the necessary by-law to amend the Surrey Land Use and Development Application Fees Imposition By-law, 1993, No. 11631 (the "Fees Imposition By-law") to require the payment of additional application fees to recover the costs of preparing the Fleetwood Enclave Infill Area Concept Plan all as documented in Appendix IV.

INTENT

The purpose of this report is to obtain approval of:

- the Fleetwood Enclave Infill Area Concept Plan; and
- related funding mechanisms for amenities proposed for the Fleetwood Enclave Infill Area.

A separate report will be forwarded to Council from the Engineering Department that will describe the engineering servicing, transportation and infrastructure strategy (Section 2) for development concept proposed in this report.

BACKGROUND

On October 17, 2011, Council considered Land Use Application 7911-0085-00. This application was to rezone a site located at 7626 – 155 Street and 7628 - 156 Street to allow for the development of 24 small suburban single family lots at a gross density of 2 units per acre. The development application included a significant open space component (50%) and a buffer between the development and land within the Agricultural Land Reserve ("ALR"). The related rezoning bylaw was given final adoption by Council on July 26, 2012.

On December 12, 2011 as part of its consideration of the referenced development application (no. 7911-0085-00) Council passed Resolution no. R11-2140, by which Council requested that staff prepare a comprehensive land use plan for the larger "Fleetwood Enclave" area within which the referenced application is located (see Appendix I). This plan was to address key issues including:

- Land uses and densities;
- Interface with the ALR and with adjacent established neighbourhoods;
- Environmental protection, tree retention and wildlife habitat;
- Parks, trails and recreation opportunities, including the interface and connections with Fleetwood Park and Surrey Lake Park;
- Transportation, traffic and pedestrian networks; and
- Infrastructure servicing, financing, and amenity contributions.

Context

The Fleetwood Enclave Infill Plan area includes approximately 26.1 hectares (64.7 acres) of land that is currently subdivided into rural residential properties averaging 0.85 to 3.0 acres in area. The Plan Area is bordered by an established residential neighbourhood to the north, the unopened 76 Avenue right-of-way to the south, the ALR boundary through the Eaglequest (Coyote Creek) Golf Course to the west and Fleetwood Park to the east.

The Area is currently designated Suburban in the Official Community Plan (OCP). Road access into the area is limited to two entry points from the north through an established residential neighbourhood.

DISCUSSION

Planning Process and Public Consultation

The planning process for the Area began with an Environmental Study to determine ecosystem values and environmental protection priorities. The study found that there are no Class A or Class B watercourses in the study area and that the most significant environmental values are associated with second-growth mixed-species woodlands that augment more extensive forests within Fleetwood Park. These woodlands also provide continuous tree cover that facilitates wildlife movements between Fleetwood Park, Surrey Lake Park, agricultural lands and the BC Hydro corridor extending through the Bear Creek watershed to the west. The Environmental Study also identified a number of high-quality specimen trees.

The preliminary findings of this study were presented and discussed at initial landowner and public meetings in May 2012 at Fleetwood Park Secondary School, where input into the neighbourhood plan was also received. Approximately 25 landowners attended the first meeting, and approximately 88 residents attended the public meeting on May 14, 2012.

After analyzing the recommendations of the Environmental Study and the feedback received at the public meetings, a staff team involving staff of the Planning & Development Department, the Parks, Recreation and Culture Department and the Engineering Departments prepared a draft land use concept. This land use concept provided the basis for undertaking a traffic impact analysis (TIA) study and for the development of engineering infrastructure concepts (drainage, sanitary sewer and water) to support the proposed land uses.

Following the completion of the TIA and the development of the engineering infrastructure concepts, a second public meeting was held on January 23, 2013 at Fleetwood Park Secondary School. Approximately 80 residents attended this meeting, where the draft Infill Area Concept Plan was presented and discussed, along with the findings of the traffic impact analysis and the engineering, financing and implementation strategies. Appendix V to this report summarizes the public comments that were received at this second public meeting.

The draft plan was also presented to the Environmental Advisory Committee (EAC) and the Agricultural and Food Security Advisory Committee (AFSAC).

Planning Principles

The planning principles guiding the development of the Infill Area Concept Plan are:

- to create a liveable, comprehensively planned residential neighbourhood based on sustainable and efficient development patterns;
- to provide opportunities for new family-oriented housing within Fleetwood;
- to provide appropriate interfaces between the new neighbourhood and surrounding areas;
- to protect significant wildlife habitats and specimen trees; and
- to provide linear parks, greenways and trail connections linking Fleetwood Park and Surrey Lake Park.

Overview of the Proposed Infill Area Land Use Concept Plan

Housing Types and Densities

The proposed land use plan for the Fleetwood Enclave Infill Area is characterized as a single-family residential neighbourhood, with gross densities of 4 to 5 units per acre, which is compatible with the character of the adjacent established neighbourhood and recognizes the limited access into the area. At full build-out it is expected that there will be approximately 224 new houses in the area with secondary suites being permitted in these houses. Based on an average of 3.2 persons per dwelling unit and 1.8 persons per secondary suite the build-out population of the Fleetwood Enclave will be approximately 1200. Analysis by Surrey School District staff indicates that the local Coyote Creek Elementary School and Fleetwood Park Secondary School have sufficient capacity to accommodate the expected number of school-aged children that will be generated in the area at full build-out.

The housing areas are proposed as gross density subdivisions with a higher net density permitted on a portion of the site while providing for areas of parkland and green spaces. In general, lot sizes are expected to range from 5,000 to 6,000 sq. ft. (465 to 570 sq.m.), with frontages averaging 15 metres (50 ft.). The densities shown in the Land Use Plan (Appendix V) are slightly lower in areas closer to the ALR and in areas where there is less dedication of green space to retain higher environmental values.

A table summarizing the land uses, housing units and population estimate statistics is attached as Appendix VI.

Parks and Trail Networks

The Fleetwood Enclave Plan includes an open space and trail network that provides passive and natural area parkland, as well as trail connections between the plan area and adjacent community amenities. The nature park "Open Space" areas on the west, south and east edges of the neighbourhood will be maintained as a natural woodland habitat with opportunities for low impact recreational paths along the eastern edge of 157 Street and the western edge of 154 Street. Existing tree stands in all park areas will be retained where appropriate.

A proposed east-west wildlife corridor along 77 Avenue will be maintained as linear parkland with significant tree and vegetation species retained or replanted where appropriate. A pedestrian pathway will meander through the wildlife corridor area connecting with trails in Fleetwood Park to the east and future greenway connections to the west toward 152 street and south along the existing 156 street pathway connections to Surrey Lake Park. Any future trail expansion through the ALR along 156 Street will require Agricultural Land Commission (ALC) approval.

Passive parkland in the form of Open Space and Habitat Corridor areas occupy 2.5 hectares (6.2 acres) of land within the plan area.

Street Network and Road Classifications

The local street network is designed as a modified grid to provide flexible connectivity within the infill area and to connect with the existing transportation network in areas adjacent to the Area.

Most of the proposed local roads conform to City of Surrey standards, including a 20-metre right-of-way width to accommodate two travel lanes along with on-street parking on both sides and standard boulevards with street trees and sidewalks on each side of these streets.

156 Street between 80 Avenue and the unopened 76 Avenue right-of-way is proposed to be reclassified as a collector road, with a standard 24-metre right-of-way width and bicycle lanes in addition to two travel lanes, on-street parking on each side, sidewalks and street trees. The east side of the 156 Street cross-section will be modified to incorporate a north-south greenway including a 4-metre wide multi-use path and landscaping, partly within the road allowance and partly within a statutory right-of-way on adjacent private lands. This greenway is a continuation of the 156 Street greenway north of 80 Avenue, and will eventually link to Surrey Lake Park as well as providing a connection to the recently constructed section of the Fleetwood Greenway between 156 Street and 160 Street.

In addition to standard roads, the Plan includes several special streets:

- 77 Avenue, a local road, is proposed to incorporate a 15-metre wide habitat corridor with a greenway trail in a wide median between one-way travel lanes. This unique design will allow for an east-west wildlife corridor and trail connecting across the neighbourhood into Fleetwood Park and allowing future greenway linkages to the west; and
- 154 Street on the west edge of the neighbourhood and 157 Street on the east edge of the neighbourhood are modified local streets with housing on one side only, facing towards parkland to create a public interface along parks while maximizing opportunities for tree retention and ecosystem network health. These streets will have sidewalks and street trees on one side, with parkland trails and landscaping on the "park side" of the street.

Future Road and Pedestrian Connection to 152 Street

Based on the findings of the Traffic Impact Analysis, a roadway connection from the Plan area to 152 Street at 76 Avenue is desirable. Although there is an existing 20 m wide unopened road allowance on the 76 Avenue alignment, this connection is challenging to achieve in the short term due to the intervening Eaglequest (Coyote Creek) golf course lands, which are located within the Agricultural Land Reserve. Any future construction of a road in the 76 Avenue road allowance would require Agricultural Land Commission approval. This future connection is shown in the Infill Area Concept Plan. The realization and timing of this connection will depend on possible future re-development of some portions of the Eaglequest (Coyote Creek) golf-course lands. The connection will also provide a future pedestrian link through the plan area to 152 Street along the roadway and by means of the Habitat Corridor trail system. In advance of any new road connection being achieved, the 156 Street connection to the Surrey Lake Trail system will provide pedestrian access to 152 Street.

Transit

There are no existing or planned transit service stops within the Fleetwood Enclave Area. A local bus service is available at 80 Ave and 156 Street providing service to Fleetwood Town Centre. For regional transit service, bus services along 152 Street provide service to North and South Surrey.

The network of planned greenways, pathways, and the public road system will support effective circulation routes for walking and cycling within the community and to/from adjacent communities.

Servicing Strategy

An engineering servicing analysis and financial plan for the Fleetwood Enclave Infill Area Plan is currently being developed. In addition, the Traffic Impact Analysis (TIA) is also being reviewed, which identifies transportation infrastructure improvements to the network beyond the neighbourhood, which are partially attributable to development in the study area.

Only those works that normally form part of the City's development cost charge (DCC) program, such as major trunk sewer and grid water mains, collector and arterial roads, and major stormwater management infrastructure, will be included in the servicing strategy. Local engineering services will be addressed on a site-by-site basis during the development application

review process, which is the usual practice of the City for development in infill areas. A copy of the servicing strategy will be forwarded to Council by way of a separate report in the future.

Transportation Infrastructure

A transportation and traffic servicing analysis for the Fleetwood Enclave Infill Area Plan is under development. A copy of this servicing strategy will be forwarded to Council as part of the servicing strategy report noted above.

Traffic Impact Analysis Study Findings and Recommendations:

Local Traffic Projections

At full build-out, development in the Fleetwood Enclave is expected to generate about 266 vehicles per hour (66 in, 200 out) during the AM peak hour and 358 vehicles per hour (225 in, 133 out) during the PM peak hour. Based on traffic forecasts, the development in the Fleetwood Enclave will have limited impact on the performance of the surrounding transportation network. Additional improvements are necessary; however, to address existing operational problems at 152 Street associated with background traffic increases.

Short-term off-site improvements

The following short term traffic improvements are currently planned for the areas around the Fleetwood Enclave Infill Area:

- A traffic signal at the 84 Avenue and 158 Street intersection is identified within the 10-Year Servicing Plan, with implementation anticipated in the medium term which will improve the operation of this intersection; and
- New roundabouts are planned at 82 Avenue and 156 Street and 82 Avenue and 160 Street to improve the safety and operation of each of these intersections. The local community was consulted on both projects in 2012 and these improvements will be constructed in 2013 subject to property acquisition.

Longer term off-site improvements

As indicated above, the future connection of 156 Street to 152 Street at 76 Avenue is desirable to improve traffic distribution and intersection performance throughout the larger area around the Fleetwood Enclave. The realization of this important link is dependent on future development of portions of the Eaglequest Golf Course.

Water

The existing water infrastructure has sufficient capacity to supply the Infill Plan area. The draft servicing strategy proposes connections to two existing water mains along 155 Street and 156 Street to service the Infill Plan area. New local distribution mains are required to service new development in the area. These mains are local infrastructure and will not be added to the City's 10-Year Servicing Plan, nor will these mains be eligible for reimbursement from DCCs.

Sanitary Sewer

The existing sanitary sewer infrastructure has sufficient capacity to convey wastewater from the Infill Plan area. The draft servicing strategy proposes new local collection sewers to convey wastewater from the Infill Plan area to the City's trunk sewer. These sewers are local infrastructure and will not be added to the City's 10-Year Servicing Plan, nor will these sewers be eligible for reimbursement from DCCs.

Stormwater (Drainage)

The draft servicing strategy recommends a combination of onsite and offsite works to manage stormwater runoff from the Infill Plan area. Onsite infiltration measures within the building lots within Infill Plan area are required to reduce stormwater runoff. A system of local storm sewers is proposed to drain runoff from lots and streets and will discharge into existing ditches to the south of the Infill Plan area, leading to the major detention facility in Surrey Lake Park. These sewers are local infrastructure and will not be added to the City's 10-Year Servicing Plan, nor will these sewers be eligible for reimbursement from DCCs.

Implementation of Development in the Infill Area Plan

Development Applications

The current OCP designation and the existing zoning of the area do not permit the development of the densities envisioned in the Infill Area Plan. As such, the Plan densities will occur through individual land development applications brought forward by developers that will include an Official Community Plan amendment, rezoning and subdivision consistent with the land uses and densities set out in the Land Use and Density Plan (Appendix V). The public elements of the Plan (parks, streets and greenways) will be secured through the development approval process by dedication or by purchase with funds generated from development in the area or through other agreements.

Land Consolidation Areas

In several areas, lot consolidation will be required to ensure the efficient development of property. These land consolidation opportunities can, in most circumstances, be determined on a case-by-case basis at development application stage. However, consolidation areas have been identified in the Infill Area Concept Plan to avoid creating remnant pieces that would not be developable on their own.

Land consolidation areas are identified to inform developers of the consolidation requirement to ensure feasible development areas and to achieve an equitable distribution of road dedication, parkland dedication, and construction costs across properties.

Design Guidelines

In the case of single-family residential developments in the Infill Area Concept Plan design guidelines will be implemented through the process of approving subdivision plans and by way building schemes for each separate subdivision in the area.

Infrastructure Summary and Financial Analysis

A comprehensive financial analysis will be included in the Infill Plan draft engineering servicing strategy report that will be forwarded to Council in the near future. The following table summarizes the projected DCC revenues and construction costs for the infrastructure needed to provide trunk engineering services to and in the Infill Plan Area. The revenues are based on the current DCC rates for each land use category as proposed in the Infill Plan. The revenues include the DCC municipal assist factor for each utility.

The following table summarizes the projected DCC revenues generated by development within the Infill Plan Area. These revenues are based on current DCC rates that came into effect on March 15, 2012. These DCCs do not include the Parkland Acquisition DCC rates. The revenues include the DCC municipal assist factor for each utility. As indicated below, these DCC revenues are sufficient to support the financing of the engineering infrastructure required to support the expected level of development in the Plan Area.

Services	Per Dwelling Unit Contribution (Approx. 240 Units)	Estimated DCC Revenue
Sanitary Sewer	\$ 2,238	\$ 537,120
Water	\$1,714	\$ 411,360
Drainage	\$5, 866	\$ 1,407,840
Non Arterial Roads	\$2, 435	\$ 584,400
Arterial Roads	\$11,147	\$ 2,675,280
TOTAL		\$5,616,000

Infill Area Amenity Contributions

In accordance with City policy, to address the amenity needs of the new development in the Fleetwood Enclave Infill Area, each development will at the time of rezoning or building permit issuance be required to make a monetary contribution toward the provision of new police services, new fire protection services and library materials and toward the development of the parks, open spaces and pathways in the Area.

The monetary contributions toward police services, fire services and library materials will offset the capital costs of providing these services to the new development and are applied on a standardized basis in all of Surrey's NCP areas and designated Infill areas. The monetary contributions toward parks, open spaces and pathway development are based upon an estimate of the capital costs of these improvements for this particular Infill area. The total cost is divided by the anticipated number of dwelling units to ensure an equitable contribution arrangement.

A summary of the applicable amenity contributions (per dwelling unit) and the estimated revenue that will be generated from the Fleetwood Enclave Infill area for amenities are documented in Appendix V.

Recovery of Infill Area Plan Preparation Costs

Several consultants were retained to assist with the preparation of the Fleetwood Enclave Infill Area Plan. The cost of the engineering and environmental consultant services that the City retained was \$85,711.22. It is recommended that the City's Fee Imposition By-law be amended to include a provision for the recovery of these Infill Area preparation costs through the payment of application surcharge fees by applicants for development in the Fleetwood Enclave Area. The necessary amendments to Schedule 8 of the Fee Imposition By-law are documented in Appendix IV.

CONCLUSION

As directed by Council a City project team, assisted by engineering consultants and environmental consultants and in consultation with the public, property owners, and representatives of the land development industry, has prepared an Infill Area Concept Plan for the Fleetwood Enclave neighbourhood. This Plan includes a land use plan and will include an engineering servicing strategy to guide future development in the area.

The Infill Area Plan is consistent with the policy framework of the City's OCP and the Sustainability Charter, and has support from the EAC and AFSAC. Strategies have been identified for funding various amenities required for the neighbourhood. Based on the above discussion, the Planning and Development Department and the Engineering Department recommend that Council:

- Approve the "Infill Area Concept Plan" for the Fleetwood Enclave Neighbourhood as contained in Appendix II;
- Approve amendments to the Road Classification Map and Road Allowance Map in the Subdivision & Development By-law, 1986, No. 8830 to reflect the road network illustrated in the Infill Area Concept Plan as contained in Appendix II;
- Approve amenity contributions as specified in the Infill Area Concept Plan contained in Appendix II to support community amenities and services that benefit the Fleetwood Enclave neighbourhood;
- Instruct the City Clerk to bring forward the necessary by-law to amend Surrey Zoning By-law, 1993, No. 12000 (the "Zoning By-law") to include amenity contributions for the Fleetwood Enclave Infill Area Concept Plan all as documented in Appendix III; and

- Instruct the City Clerk to bring forward the necessary by-law to amend the Surrey Land Use and Development Application Fees Imposition By-law, 1993, No. 11631 (the "Fees Imposition By-law") to require the payment of additional application fees to recover the costs of preparing the Fleetwood Enclave Infill Area Concept Plan all as documented in Appendix IV.

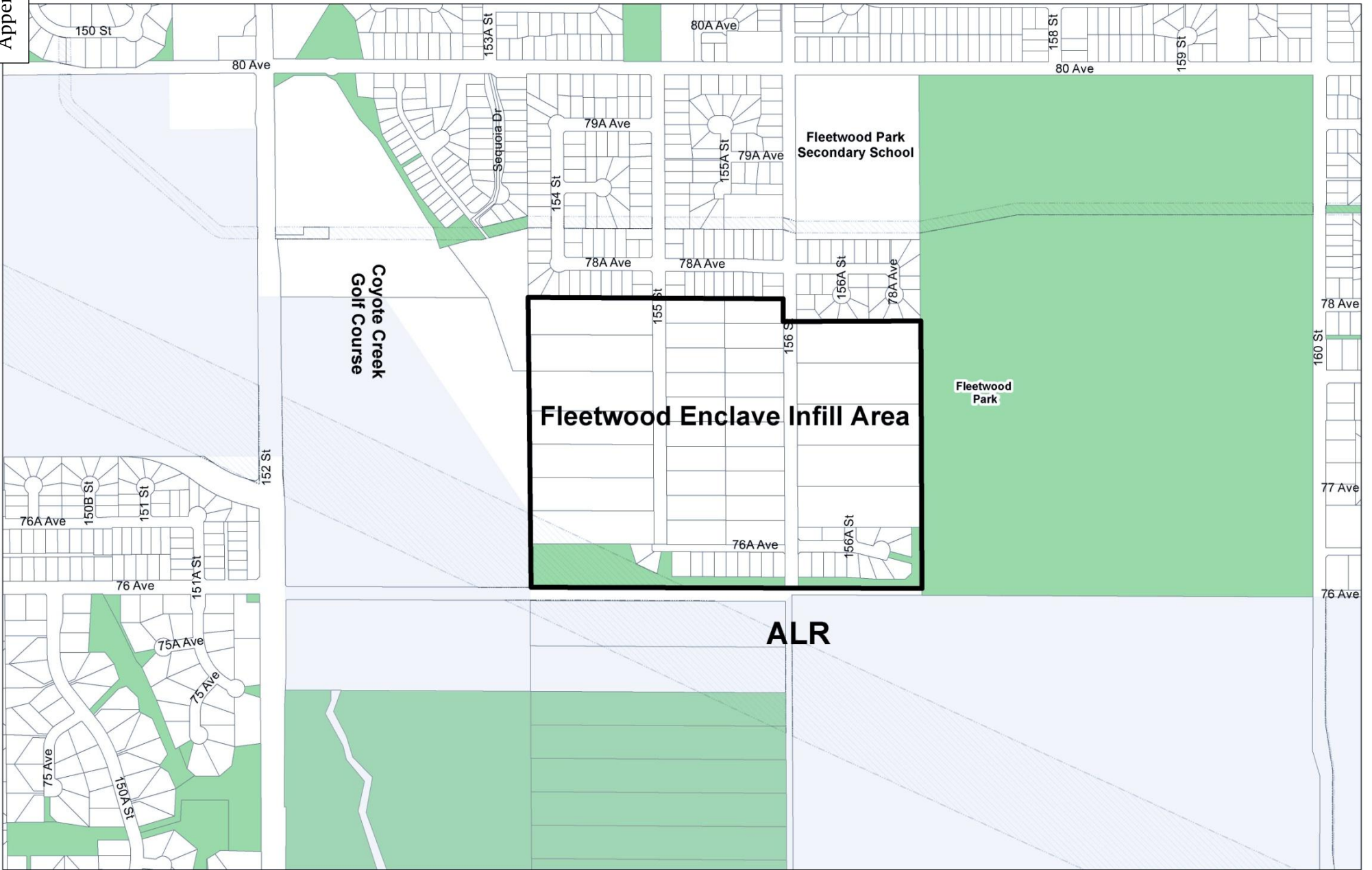
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Original signed by
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General Manager, Engineering

MK:saw

Attachments:

- Appendix I Fleetwood Enclave Plan Area
- Appendix II Fleetwood Enclave Infill Area Concept Plan
- Appendix III Proposed Amendments to Schedules F and G of the Zoning By-law
- Appendix IV Proposed Amendment to the Surrey Land Use and Development Fee Imposition By-law
- Appendix V Fleetwood Enclave Infill Area Land Use Concept Plan
- Appendix VI Public Feedback/Comment Summary



INFILL AREA PLAN

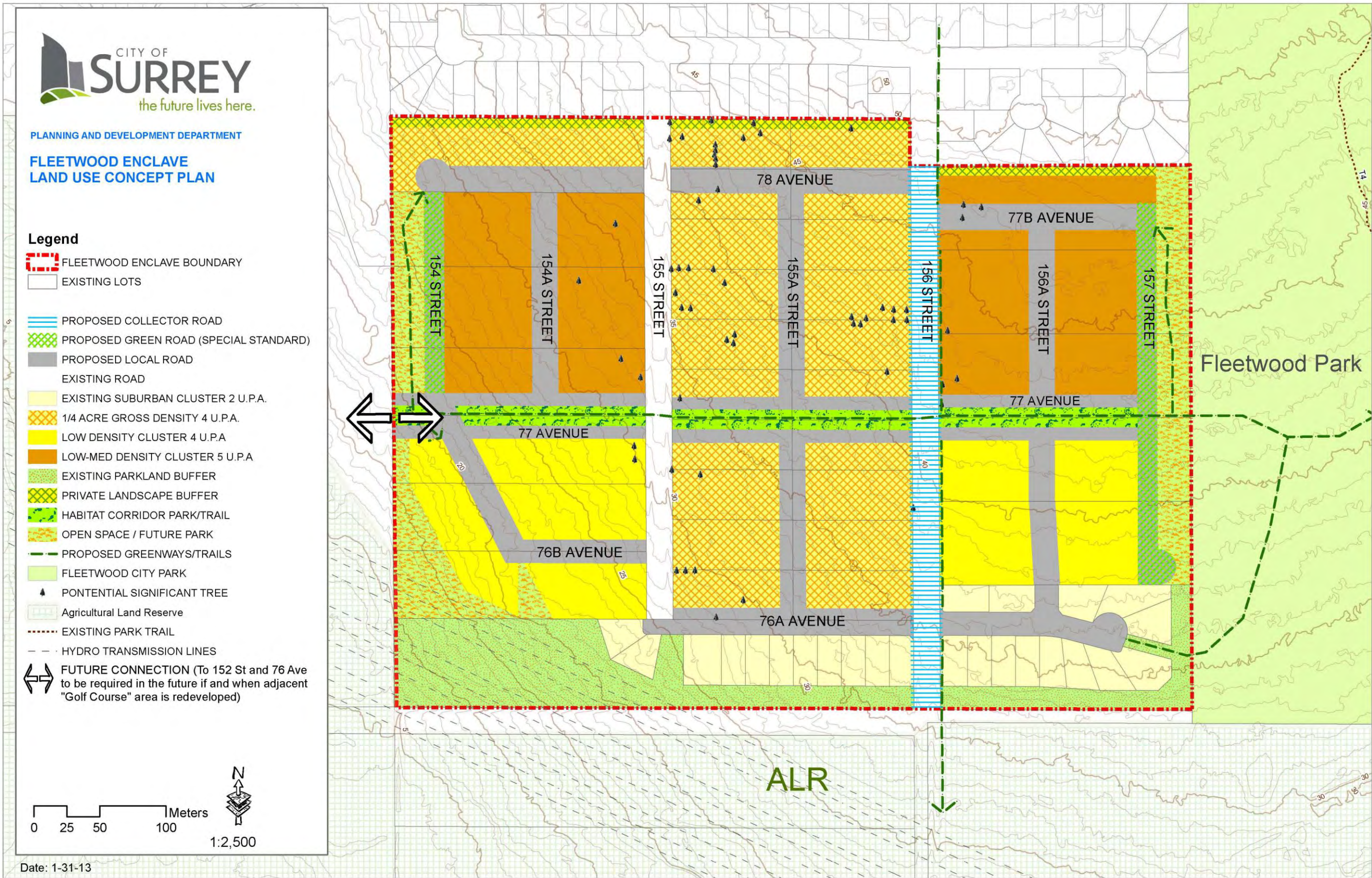


[FLEETWOOD ENCLAVE]

This Infill Plan provides a framework for development in the Fleetwood Enclave including servicing requirements, transportation designs, parks, trails and amenity contribution rates; and will introduce new requirements at time of development application, along with the provisions for increased neighbourhood service amenities and development potential in the Fleetwood Enclave Neighbourhood.

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Acknowledgements

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City of Surrey

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Contents

SECTION 1: PLANNING AND DEVELOPMENT STRATEGY

- Part 1: Background 7**
 - 1.1 Context7
 - 1.2 The Plan Area7
 - 1.3 Initiation of the Infill area plan.....9
 - 1.4 Policy Framework11
 - 1.5 Opportunities and Constraints11
 - 1.6 The Planning Process.....15

- Part 2: Objectives, Land Uses and Policies 19**
 - 2.1 Planning Vision19
 - 2.2 Specific Objectives.....19
 - 2.3 Planning Principles20
 - 2.4 Planning considerations22
 - 2.5 “Infill Area” Land Use Plan31
 - 2.7 Land Use And Population Statistics58
 - 2.8 Land Consolidation Areas.....59
 - 2.9 Zoning Bylaw Variations61

- Part 3: Design Guidelines 62**
 - 3.1 Overall Design Principles62
 - 3.2 Design Guidelines for Streets63
 - 3.3 Design Guidelines for Street Trees63
 - 3.4 Design Guidelines for Single Family Detached Housing64
 - 3.5 Design Guidelines for Single Family Semi-Detached Housing66
 - 3.6 Design Guidelines for Development Abutting Parks & Open SPace66
 - 3.7 Design Guidelines for Buildings.....66
 - 3.8 Design Guidelines for Park Areas67

- Part 4: Implementation..... 68**
 - 4.1 Official Community Plan amendments68
 - 4.2 Rezoning.....68
 - 4.3 Subdivision68
 - 4.4 Design Guidelines.....69
 - 4.5 Community Services and Amenity Contributions69
 - 4.6 Cost Recovery of Infill Area Preparation72

List of Figures

FIGURE 1 - AERIAL PHOTO OF STUDY AREA.....	8
FIGURE 2 - FLEETWOOD ENCLAVE INFILL AREA BOUNDARIES.....	10
FIGURE 3 - TOPOGRAPHY	14
FIGURE 4 - HIGH ARBORICULTURE VALUES	24
FIGURE 5 - FLEETWOOD ENCLAVE NATURALNESS VALUES.....	26
FIGURE 6 - STREAMS AND WATERSHEDS	28
FIGURE 7 INFILL AREA LAND USE CONCEPT PLAN.....	32
FIGURE 8 - TRANSITION AREA CROSS-SECTIONS	39
FIGURE 9 - FLEETWOOD ENCLAVE HABITAT CORRIDORS ASSESSMENT.....	51
FIGURE 10 - ECOSYSTEM MANAGEMENT STUDY GREEN INFRASTRUCTURE NETWORK AREAS	51
FIGURE 11 - PARKS, RECREATION, TRAILS AND OPEN SPACE	54
FIGURE 12 -PEDESTRIAN CIRCULATION PLAN	56
FIGURE 13 - FLEETWOOD ENCLAVE BLOCK PLAN DIMENSIONS	57
FIGURE 14 - LAND CONSOLIDATION AREA	60

List of Tables

TABLE 1 - SURROUNDING LAND USES	13
TABLE 2 - FLEETWOOD PARK ENROLMENT PROJECTIONS.....	17
TABLE 3 - COYOTE CREEK ELEMENTARY ENROLMENT PROJECTIONS	17
TABLE 4 – FLEETWOOD ENCLAVE PLANNING PRINCIPLES	21
TABLE 5 - SUMMARY OF RESIDENTIAL LAND USE DESIGNATIONS.....	34
TABLE 6- SUBURBAN CLUSTER (2 UPA) GUIDELINES	35
TABLE 7 - CLUSTER RESIDENTIAL (4 UPA) GUIDELINES	36
TABLE 8 - 1/4 ACRE RESIDENTIAL GROSS DENSITY (4UPA) GUIDELINES	37
TABLE 9 - LOW-MEDIUM CLUSTER RESIDENTIAL (5 UPA) GUIDELINES	38
TABLE 10 - PUBLIC, PARK AND NATURAL AREAS.....	53
TABLE 11 – STATISTICAL SUMMARY AND PROJECTED BUILD OUT SCENARIO FOR THE FLEETWOOD ENCLAVE PLAN AREA.....	58
TABLE 12 - AMENITY CONTRIBUTION SUMMARY	71

List of Drawings

CROSS SECTION 1 - TRANSITION B ADJACENT TO FLEETWOOD PARK.....	41
CROSS SECTION 2 – TRANSITION C- LANDSCAPE BUFFER ADJACENT TO EXISTING URBAN AREA.....	43
CROSS SECTION 3 - TRANSITION E -156 STREET GREENWAY.....	45
CROSS SECTION 4 – TRANSITION - F HABITAT CORRIDOR INTERFACE	47

SECTION 1: PLANNING AND DEVELOPMENT STRATEGY

Part 1: Background

1.1 CONTEXT

The overall objective of this Infill Area Plan is to provide general policies, a development concept and a financial strategy to guide future urban residential development in Southwest Fleetwood. The Fleetwood Enclave is expected to provide for additional new housing opportunities and is intended to create a liveable, comprehensively planned neighbourhood based on efficient development patterns that cluster housing and provides green space opportunities to preserve significant natural tree canopy areas, and on site rainwater management opportunities. The Infill Area plan will do this by identifying site appropriate land uses, designating the transportation system, and by providing for neighbourhood servicing in an environmentally friendly and staged manner.

1.2 THE PLAN AREA

The Fleetwood Enclave infill area includes approximately 25.7 hectares (63.5 acres) of land, as illustrated in **Figure 1**, with approximately 20.2 hectares (50 acres) of land forming part of the new infill area development concept plan. The entire plan area is bordered by urban single family homes along 78A Avenue to the north, the unopened 76 Avenue Road Right-of-Way to the south along the Agricultural Land Reserve (ALR), Eaglequest Golf Course to the west, and Fleetwood Park to the east.

There are currently 32 individual properties (lots) in this Infill area. The current zoning in the area is predominantly RA One-Acre Residential Zone (20 lots) and A-1 General Agriculture Zone (12 Lots). The current Official Community Plan (OCP) designation in the Infill area is entirely Suburban. 24 newly subdivided (2012) Small suburban lots are located directly adjacent to the ALR boundary, within a Comprehensive Development Zone approved in 2011. The Fleetwood Enclave Area contains primarily large acreage residential homes, and has a rural ambience characterized by, pastures, established mixed species second growth trees and south facing views towards the Serpentine Valley.

The area contains several large 20-35 year old single family homes or hobby farms, and treed areas. Unlike the surrounding Fleetwood area, this small pocket of lands has not experienced urbanization and growth of residential neighbourhoods to the north due mainly to poor road and transportation accessibility, and its location along the Agricultural Land Reserve (ALR). Vehicle access to this neighbourhood is from 80 Avenue to the north, along either 155 Street or 156 Street. No roads continue to the south, east or west do to Parks, ALR, and Golf Course uses. Private lot sizes range from 3,453 m² to a little over 2.6 hectares. Most commonly found lot sizes include 3,453 m² lots and 1.2 hectare lots.



Figure 1 - Aerial Photo of Study Area

1.3 INITIATION OF THE INFILL AREA PLAN

On October 17, 2011, Surrey City Council considered an initial Planning Report from the Planning and Development Department (**See Appendix A**) for a rezoning of a portion of the Southwest Fleetwood Enclave site, located at 7626 – 155 Street and 7628 - 156 Street, and a related Development Permit application, to allow for the development of 24 small suburban single family lots (zoned CD based on the Residential Cluster (RC) Zone, with significant open space and to establish buffering along the ALR boundary.

As part of the development report the applicant's consultant was required to prepare a development concept plan for the remaining Southwest Fleetwood Neighbourhood. Planning Staff and Council noted that further refinement was needed with respect to the distribution of open space and the general road pattern, which should be based on a modified grid with a high degree of internal connectivity and off sight traffic improvements to be determined by a Traffic Study.

The application subsequently proceeded to Public Hearing on November 7, 2011. At the Public Hearing, Council heard concerns from residents regarding the potential impact the proposed development could have on the neighbouring agricultural property to the south, wildlife in the area, and development of lands to the North. As part of the subsequent development approval, Surrey City Council directed City Staff work on creating a local development area concept plan for the larger Fleetwood Enclave Neighbourhood to the North.

Council therefore initiated the Fleetwood Enclave Infill Area Plan to address questions regarding the overall development concept and concerns regarding development impacts on:

- Agricultural lands to the south within the (ALR);
- Ecosystem and Biodiversity function and wildlife movement;
- Parks, Trails and Recreation Opportunities;
- Transportation, Traffic and Pedestrian Network Capacity;
- Land Use Types and Densities;
- Adjacent neighborhood Interface;
- Financing, Services and Amenity Contributions

The hubs and corridors identified within the Ecosystem Management Study were intended to provide overall guidance as to the delineation of open space, which has been reflected in this concept. The refinement of the development concept prepared by the applicant's consultant will involve input from various City Departments as well as public consultation with property owners in and adjacent to this area.

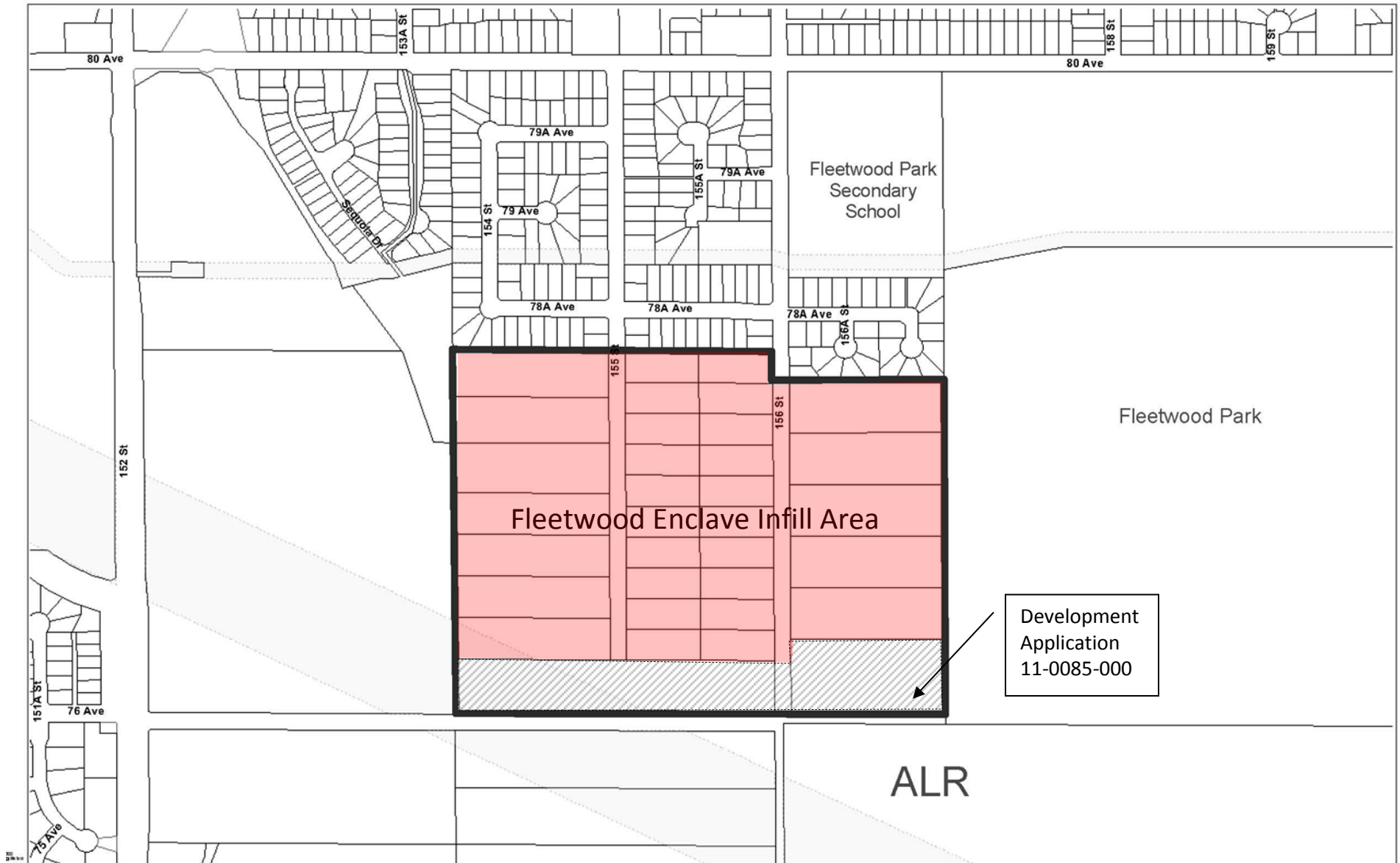


Figure 2 - Fleetwood Enclave Infill Area Boundaries

1.4 POLICY FRAMEWORK

Community Development in Surrey is conducted and administered within a series of plans, policies and by-laws. These include the Metro Vancouver Regional Growth Strategy, Surrey Official Community Plan, and in some specific areas Secondary and Neighbourhood Concept Plans, and/or Infill Area Plans. Finally the Zoning By-law, as well as several other city bylaws and provisional regulations guide land use and development within the City.

Within the hierarchy of these Plans, the Surrey Official Community Plan must conform to the Metro Vancouver Regional Growth Strategy (Urban Growth Boundary) while all the other Plans and By-Laws must conform to the Official Community Plan. Like the Regional Growth Strategy and the Official Community Plan, all other Secondary Land Use Plans, including infill area plans within the hierarchy generally address the physical, environmental, social and economic aspects of the area for which they are prepared.

1.5 OPPORTUNITIES AND CONSTRAINTS

The Fleetwood Enclave Infill Area has multiple opportunities as well as constraints including transportation network and access limitations, distance to local amenities and transit, proximity to the agricultural land reserve, and access to green space.

1.5.1 Transportation Network

The Infill area is bounded by urban single family homes along 78A Avenue to the north, the unopened 76 Avenue Road Right-of-Way to the south along the Agricultural Land Reserve (ALR), Eaglequest Golf Course to the west and Fleetwood Park to the east. There are two existing Local roads entering into the Infill area from the north at 155 Street and 156 Street.

All traffic flowing in or out the plan area must converge at the 156 Street intersection point at 80 Avenue ,as 156 Street ends 100 meters into the ALR ending at the Surrey Lake Trail head, with no east west connections out of the study area, due to the unopened 76 Avenue Road ROW to the south. The current subdivision format is based primarily on access to 155 and 156 streets with alternative access points extending in or out of the Study area. There is, therefore, only one existing access point which limits the range of densities and uses that can be provided. Transportation and traffic issues are discussed in Section 2, Part 5 and in full in **Appendix C** - Transportation Report.

1.5.2 Distance to adjacent Amenities and Transit Services

The Southwest Fleetwood Enclave areas proximity to rapid or regular bust transit and local amenities is limited. Therefore, limited densities and uses are proposed in the area.

1.5.3 Tree Hazards

Tree hazards do exist in the area. There is a potential risk of damage caused by falling trees or branches. Although the stand is predominantly composed of healthy, vigorous

trees, it is approaching an age when decay, breakage and incidence of other pathogens are becoming more common. Also, new forest edges will likely be created during future development. Trees exposed along these edges must adapt to increased exposure to wind.

1.5.4 Topography

As shown in **Figure 2**, Topography in the project area is generally flat to gently sloping (2 to 12% grade). Elevation ranges from 10 meters to 50 meters and slope aspect is southwest to southeast. The slopes present no difficulty for servicing and development other than the need to carefully design to minimize steeper roads, protect intersection sight lines, and manage drainage effectively.

1.5.5 Existing Zoning

Current zoning in the Fleetwood Enclave is RA (One-Acre Residential) and A-1 (General Agriculture); however, a development proposal for 24 lots (rezoning to Comprehensive Development based Cluster Residential) was given final adoption by Council on July 26, 2012 on the south edge of the Infill Area Plan.

1.5.6 Existing Land Use

A BC Hydro right-of-way borders the south end of the Study Area. Most existing use is residential and agricultural. A landscape nursery is located in the agricultural lands on neighbouring property to the south within the Agricultural Land Reserve (ALR).

1.5.7 Surrounding Land Uses

Land uses surrounding the Infill Area are summarized in **Table 1**, and include residential, recreational, and agricultural development. Agriculture land extends south to the Serpentine River and beyond. North and west of the SWFN are urban and suburban residential neighbourhoods. Two of the largest parks in the area (Fleetwood and Surrey Lake) are within 100 meters. These parks offer passive recreation opportunities including hiking, biking and nature appreciation. A golf course exists to the west a portion of which is within the Agricultural Land Reserve.

Table 1 - Surrounding Land Uses

Direction	OCP Designation	Existing Zone	Land Use Type
North	Urban	RF	Urban Residential Lots
East	Suburban	A-1	Fleetwood Park
South	Agricultural	A-1 in ALR	Agricultural Land Reserve
West (South)	Agricultural	CPG partially in ALR	Eaglequest Golf Course
West (North)	Suburban	CD	Strata Residential Subdivision

1.5.8 Environment

Environmental and tree preservation issues are discussed in Section 2.4 and in full in **Appendix B.:** Environmental Report.



Figure 3 - Topography

1.5.9 Servicing

Servicing issues are discussed in full in Section 2: Engineering & Servicing Strategy

1.5.10 Land Consolidation

Land consolidation areas have been identified in to advise future developers of consolidation requirements and to ensure feasible development areas which achieve an equitable distribution of road and open space dedication, land development, and construction costs. Consolidation Policies and are discussed in Section 2.8.

1.6 THE PLANNING PROCESS

1.6.1 Owners Meeting Consultation

Throughout the process, many landowners and their consultants met with the City individually and in groups to be apprised of the planning process and to be given an opportunity to discuss and resolve issues as they arose. In addition, a formal Land Owners meeting was held early on in the Planning process.

April 30, 2012 Land Owners Meeting

The formal land owners meeting was held prior to the first public open house at Fleetwood Park Secondary School. The presentation and display materials reviewed the Fleetwood Enclave boundaries, introduced the City's planning team, reviewed the key environmental study findings, discussed key opportunities and challenges, and presented how the planning process will take place.

Time was allotted to allow initial comments and suggestions about development in the area to provide input into the preparation of the Fleetwood Enclave plan and proposals for the future of the area. The overall Land Owners reaction was positive with some concern regarding density ranges and green space allocation. Approximately Twenty Five (25) Land Owner attended the meeting with a total of six (6) comment Sheets received by the City.

1.6.2 Public Meeting Consultation

There were two formal presentations to the Public made. One was held on May 15th, 2012 and the other on January 23rd, 2013 at Fleetwood Park Secondary School.

May 15, 2012 Open House

All property owners in the Infill area, as well as land owners north and south of plan area up to 80 Ave were directly notified by mail, with a Newspaper add advertising the Public Open House published in the Surrey Leader. Approximately 88 people attended the meeting. In addition to a sign-in sheet, an exit questionnaire was provided to obtain comments on issues to be addressed through the Infill area process. 21 Comment

sheets were received by the Planning and Development Department. City of Surrey Staff made a short presentation which was followed by a question and answer period.

These open house sessions attended by City of Surrey staff from Planning and Development, Parks and Recreation, Land Development, and Transportation Divisions. The overall Land Owners reaction was positive to the initiation of the plan.

January 23rd, 2013 Open House

All property owners in the Infill area, as well as land owners north and south of plan area up to 80 Ave were directly notified by mail, with a newspaper ad advertising the Public Open House published in the Surrey Leader. A notification email was also sent to all residents, land owners or interested parties who wished to be contacted after the first Public open house. Approximately 80 people attended the meeting. In addition to a sign-in sheet, an exit questionnaire was provided to obtain comments on issues to be addressed through the Infill area process.

1.6.3 Interagency Referrals

1.6.3.1 Surrey School Board

The estimated maximum 224 Single Family dwellings at time of build out of the Fleetwood Enclave are estimated to have the following impact on the following schools:

Projected number of Students for this Infill Area:

Elementary Students:	114
Secondary Students:	57

September 2011 Enrolment/School Capacity

Coyote Creek Elementary	
Enrolment (K/1-7):	73 K + 487
Capacity (K/1-7):	40 K + 600
Fleetwood Park Secondary	
Enrolment (8-12):	1345
Nominal Capacity (8-12):	1200
Functional Capacity*(8-12):	1296

The following tables (2 and 3) illustrate the enrolment projections (with current/approved ministry capacity) for the elementary and secondary schools serving the Fleetwood Enclave Infill area.

Fleetwood Park Secondary

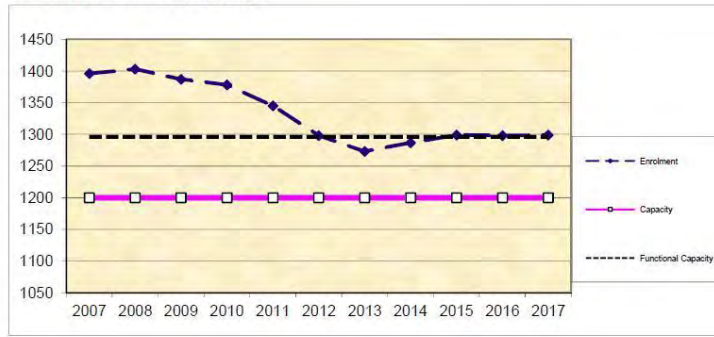


Table 2 - Fleetwood Park Enrolment Projections

Coyote Creek Elementary

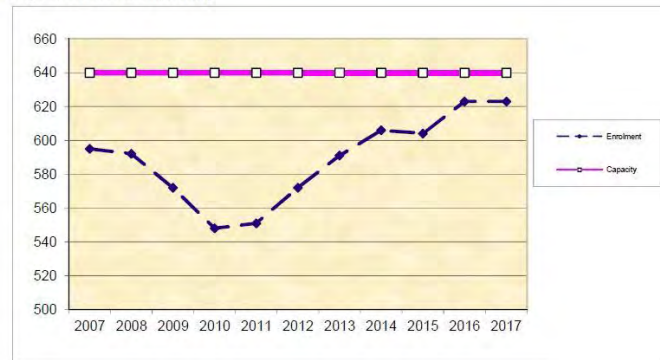


Table 3 - Coyote Creek Elementary Enrolment Projections

Capacity for Coyote Creek Elementary includes the main school building (40K + 500) plus a modular classroom complex with capacity of 100. There are no new capital projects proposed at the elementary school and no new capital projects identified for the secondary school.

The estimated number of new students has been adjusted higher for single family units with secondary suites. Depending upon actual student yield from dwellings with secondary suites, the projected enrolment shown in the tables above may be slightly higher.

Due to an aging demographic in the existing urban areas surrounding Fleetwood Park and Coyote Creek School catchment area, enrolment is not expected to exceed functional capacity of the schools in this area due to increased enrolment from Fleetwood Enclave students.

1.6.3.2 City Advisory Committees

Agriculture and Food Security Committee (AFSAC)

On September 13, 2012 a presentation was provided to the City of Surrey Agriculture and Food Security Advisory Committee (AFSAC). Considerations were noted to ensure development permit area compliance and input from the committee regarding future development applications within 300 meters of the ALR boundary.

Environmental Advisory Committee (EAC)

On October 24, 2012 a presentation was made to the Environmental Advisory Committee. Committee commented that the Fleetwood Enclave plan has some of the most consideration for the environmental values of similar plans in the City, and were generally supportive of the concept plan.

1.6.3 Land Owner and Local Neighbourhood Concerns

The Infill area plan was refined in further discussions between City staff and landowners directly. A number of landowners and residents outside the infill area provided written comments, and emails received during the course of the preparation of this plan. All comments and concerns have been discussed at different times by City Staff and Landowners, and where possible changes have been made to the Infill Area Plan.

In many cases however, it has not been possible to amend or plan, particularly in regards to concerns and reservations about the location and manner of compensation for rights-of-way for roads, trails, or servicing corridors within individual properties.

Part 2: Objectives, Land Uses and Policies

2.1 PLANNING VISION

The neighbourhood will consist of more traditional urban residential buildings with landscaped open spaces containing a variety of vegetation, pathways and street trees. Residential pockets will be united by commonly shared open space and pathways connecting the existing city and park greenway network, and an established east west habitat corridor within a city greenway. Single detached housing will be front loaded and provided on medium sized lots, varying in character by block. Tree lined roads and buffers will help maintain a human scale to development.

The general road pattern is based on a modified grid with a high degree of connectivity within the plan, within a pocket of contained future urban lands. When combined, these components will provide a sense of cohesion, quality, and an overall feeling of community in the Fleetwood enclave. The lower density limits of Southwest Fleetwood will be about the quality of built form, appropriate scale of housing, and a high quality welcoming public realm.

2.2 SPECIFIC OBJECTIVES

A series of specific objectives, listed below, were developed with input from an interdepartmental City Staff team, local community members, and city advisory bodies.

Community

The focus is on a well-planned, pedestrian friendly, identifiable community that responds to the site and natural surroundings. The plan will provide a framework for delivering a high quality, comprehensively planned residential community.

Housing

Housing shall be designed for private detached homes for single families with ample front, side and rear yards. This will cater to a families, empty nesters, and seniors. Density of single family dwelling will remain low (4 - 5UPA) in the area, to transition away from the Agricultural Lands to the south. Overall densities diminish from 5 UPA and 4 UPA to 2 UPA densities to the south.

Safety

The detailed design of the community will incorporate principles of Crime Prevention through Environmental Design (CPTED). Housing will face the Green Open space along Fleetwood Park and Golf Course to encourage passive surveillance of open space areas.

Parks

The community will be served by a variety of existing parks outside the plan area including Surrey Lake and Fleetwood that provide for a mix of active and passive uses including habitat preservation. The neighbourhood will be linked to the overall park and trail system through a series of multiuse pathways and an east west wildlife corridor. Additional open space and natural area will be added to Fleetwood Park to buffer impacts on this area from

Urban Development and provide additional passive green space areas to enhance the park. Future pedestrian and greenway links into Fleetwood Park may be provided through these open space corridors.

Urban Design

Urban design principles will contribute to make the community attractive and liveable through design, provision of buffers and parks, landscaping, maintenance of views to the Serpentine Valley, and design guidelines for residential development. Residential development will face the green areas of the plan allowing for open and long term public enjoyment of these features.

Movement

The plan will provide an inter-connected circulation system that supports the land use patterns and urban design concept by providing safe optional routes for vehicles, bicycles, and pedestrians.

Services

The plan will foster an economical servicing system as a logical staged extension of existing systems to support the land use concept and minimize the environmental impact on receiving streams and the lowlands.

Transitions

There will be appropriate land use transitions and landscape treatments between adjacent uses, both within the land uses in the Infill area and between the Infill area land uses and surrounding uses such as urban areas to the north and parklands to the east and west. Environment Best practices, tree preservation, mitigation measures, and design will be used to minimize any environmental impacts on the hydrologic regime associated with redevelopment and provide for the integration of key woodlands into the neighbourhood.

To address the agricultural interface, a 17-metre (56 ft.) wide portion of land was dedicated to the City as Parkland along the 76 Avenue right-of-way, which is adjacent the ALR boundary and the neighbouring agricultural uses.

Implementation

The neighbourhood redevelopment will be implemented through a series of rezonings and subdivisions that match development and market demand. Plan changes and approvals will provide for community input and the interplay of market forces.

Financing

The Infill area will be primarily self-funded by the landowner/ developers through standard Development Cost Charges (DCCs), and amenity contribution fees. Front-enders of trunk services will recover costs.

2.3 PLANNING PRINCIPLES

A set of planning principles have been designed to guide development in the Southwest Fleetwood Enclave to promote and ensure development activities that yield improved quality of life, environmental sensitivity, and sense of community. These Planning

Principles have been grouped in six theme areas and will guide development within Southwest Fleetwood Enclave as described in the table below.

Table 4 – Fleetwood Enclave Planning Principles

THEME AREA	PLANNING PRINCIPLES
<p>2.3.1 Parks, Open Space, Recreation and Natural Areas</p>	<p>I. Retain significant environmental features, important vegetation and Ecosystem Hubs, Sites and Corridors:</p> <ul style="list-style-type: none"> a) Consider Open “Green” Space locations which incorporate protection of significant and/or valuable ecosystems; b) Minimize clear cutting vegetation and clearing lands during development were feasible and encourage the soil retention, and replanting of native trees; c) Encourage cluster development and open space contributions with specific site designs that respond to the area’s natural features. d) Establish an effective east-west habitat corridor connection. e) On east and west edges of plan have development ‘Face the Green’ along single loaded green streets. <p>II. Provide improved access to Community Park Amenities including Fleetwood Park, Surrey Lake Park, and Surrey Lake Greenway:</p> <ul style="list-style-type: none"> a) Provide trail/Multi-use Pathway connection into Fleetwood Park; b) Establish passive park nodes at the end of each side of the east-west habitat corridor; c) Provide a natural area pathway within the East-West habitat corridor, connecting to Surrey lake Greenway.
<p>2.3.2 Roads, Transportation, Pedestrian and Bicycle Circulation</p>	<p>III. Create opportunities for pedestrians and bicycle movement linked with adjacent community amenities.</p> <p>IV. Design local roads that provide for efficient internal movement of people while sustaining the quality and character of the neighbourhood.</p>
<p>2.3.3 Infrastructure and Servicing</p>	<p>V. Ensure the cost-efficient and adequate provisions for City services including sewer, drainage, water, roads and utilities without placing a financial hardship upon the City’s resources.</p> <p>VI. Ensure practices in the design of the drainage system, and the protection of water quality and resources.</p>

2.3.4 Sense of Place and Identity	<p>VII. Protect and maintain the natural beauty and distinctive natural heritage of the area by:</p> <ul style="list-style-type: none"> a) protecting view corridors, high arboriculture values and individual significant trees; b) enhancing view opportunities along the south facing slope in and out of the ALR.
2.3.5 Residential Housing and Density	<p>VIII. Protect the character and quality of life in existing and established residential areas during development:</p> <ul style="list-style-type: none"> a) avoid piecemeal re-development; b) provide guidelines and implementation policies such as phasing plans, landscape buffers, parcel consolidation plans, and minimum redevelopment parcel sizes. <p>IX. Designate densities that make serving feasible while also respecting environmentally sensitive and agricultural (ALR) edge transition areas.</p> <p>X. Locate cluster residential development with considerable open space components adjacent to Fleetwood Park, and other high ecosystem value;</p>
2.3.6 Agricultural Edge Transition	<p>XI. Recognize, protect and enhance the Agricultural Land Reserve (ALR) Boundary and its interface by reducing density near ALR and clustering development and density away from the ALR edge.</p> <p>XII. Ensure the establishment and long term maintenance of effective buffers (fences, planted landscaping, open spaces, natural vegetation), between residential development areas and the ALR.</p>

2.4 PLANNING CONSIDERATIONS

2.4.1 Place making and Identity

Place making includes creating a liveable residential community which has an identifiable character, sense of place, and provides for a high quality of life. Some public realm and design features will include a ‘green’ single-loaded local road right-of-way allowing for the natural area to enhance the community and provide an identifiable landscape that “faces the green” along the east and west borders of the plan area.

A garage set-back from building face will be required that provides clearly identifiable entrances and front porches to allow for social activities between homeowners and passers-by from the community. In addition, high quality design features, building materials and a strong orientation to the street is encouraged.

2.4.2 Green Infrastructure Network Opportunities

The Fleetwood Enclave area is strategically important as an ecosystem connector. Three potential corridors were identified in the Surrey Ecosystem Management Study (EMS) to act as physical or functional linkages between terrestrial hubs; all were ranked in terms of their ecological significance.

The east corridor connecting the Fleetwood Enclave to Fleetwood Park was ranked highest of the three. A southern corridor to Surrey Lake was next highest, followed by the third corridor that could potentially connect to Bear Creek/Enver Creek Parks to the northwest. The east Fleetwood corridor has significant natural forest cover and is the least fragmented of the three.

The southern corridor has a wide BC Hydro right-of-way separating the Infill Area and Surrey Lake. Vegetation management is required to protect overhead lines; therefore, no forest cover can be established in this zone (approximately 120 m). The proposed northwest corridor is the longest by far (>1 km) and is not be considered a priority. Much of the landscape has been modified; currently, a golf course and residential neighbourhoods predominate with relatively few trees to the west.

The draft City of Surrey Biodiversity Strategy also includes an East West Green Infrastructure 'Local Corridor Connection' from Fleetwood Park.

2.4.3 Tree Retention

Trees have numerous social, environmental and economic benefits which contribute to improved quality of life. Managing for these multiple values while taking advantage of the free ecosystem services provided by trees can significantly reduce costs related to infrastructure, maintenance and human health. Generally, larger trees have a proportionately higher value than small trees.

See Figure 4 – High Arboriculture Vale Plan

On an individual basis, conifers provide the most benefits and are the highest value trees in the Fleetwood Enclave. Large Douglas-fir in the dominant canopy layer are the most valuable, followed by the large Black Cottonwood and Western Red cedar. Red Alder and Big leaf Maple provide relatively fewer economic values; however, they are important tree species in early serial stages of forest development. Both species add large amounts of organic material and fix nutrients, helping to amend the soil to support the establishment of climax coniferous species. Red Alder is particularly important due to its nitrogen fixing abilities.



Figure 4 - High Arboriculture Values

Future development in the Fleetwood Enclave should attempt to preserve higher value habitat within open 'green space' area where practical, and individual significant trees on individual lots. Focus should be on supporting priority components of the green infrastructure network and maintaining the ecological integrity of existing hubs specifically along the edges of the plan on Eastern edge of plan adjacent to Fleetwood Park and along western edge of the plan along the golf course lands.

See **Figure 5 – Naturalness Values of Fleetwood Enclave**

Maintaining buffer habitat that is connected to Fleetwood Park and enhances connectivity to Surrey Lake Park is highest priority for this area, with the highest level of naturalness and best allocated to reduce edge effects for biodiversity of the park. Individual Significant trees and select wind firm Conifers should also be retained wherever possible trees within open green space areas and Landscape buffers should be protected during development, and vegetation cover should be enhanced with planting as determined by the Surrey Parks, Recreation and Culture department with limited passive recreational trail access in key locations.



Figure 5 - Fleetwood Enclave Naturalness Values

2.4.4 Riparian Area Management

Riparian areas in the Fleetwood Enclave Area are not associated with any significant aquatic values. All classified watercourses in the neighbourhood are ditches (Class C). Three of these ditches are associated with roads. The fourth borders the golf course on the western edge of the study area. These ditches supply water and nutrients for part of the year to downstream watercourses; however, they do not support fish or a great diversity of benthic invertebrates. Seepage areas are present on the east and west sides of the area; they contain several small, shallow pools; however, the pools are unlikely to persist year round. **See Figure 6 – Streams and Watersheds.**

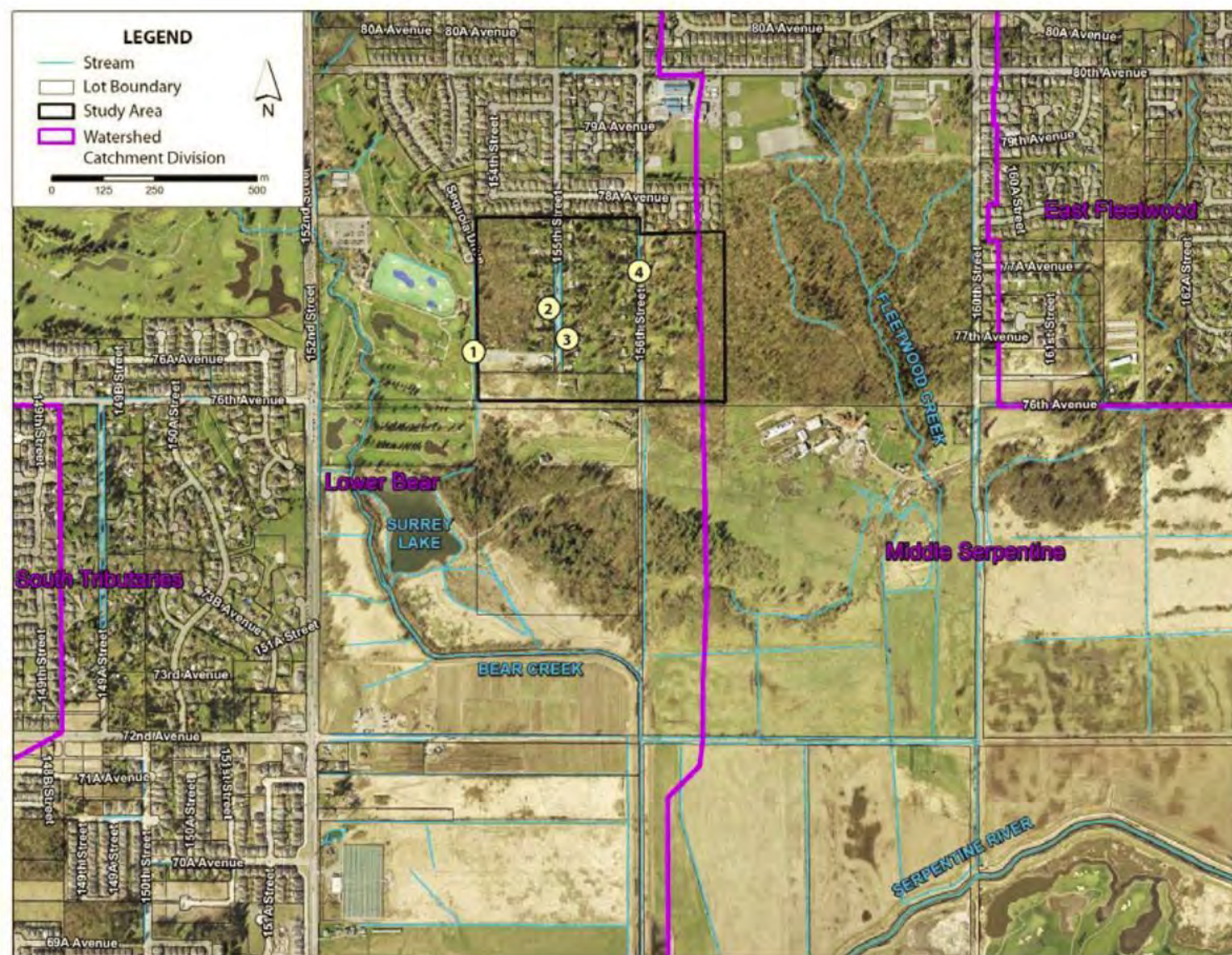


Figure 6 - Streams and Watersheds

2.4.5 Interface with Agricultural Areas

To address the agricultural interface, a 17-metre (56 ft.) wide portion of land was dedicated along the 76 Avenue right-of-way, which is adjacent, the ALR boundary and the neighbouring agricultural operations as part of a pre-existing approved development application. This land will separate the residential uses from the adjacent ALR. Parks, Recreation and Culture will maintain this portion of the park as a natural area. The developer will be required to provide any improvements to the parkland to augment the existing vegetation, to establish a maintenance fund, and to erect a black, chain-link fence along the southern property line of the residential properties as per the Surrey Agricultural and Food Security Advisory Committee (AFSAC) recommendation.

In addition, it was recommended by the AFSAC on September 12, 2012 that:

- Signage be provided as part of future development to indicate that you are entering ALR Lands along 156 Street south of unopened 76 Avenue ROW;
- As part of future development include Farm Practices Awareness:
 - Register a Covenant on property titles within 300 m of the ALR to inform potential home purchaser that farm operations are in the area and that farm practices such as noise, odour and dust should be expected during certain times of the year as part of acceptable farm practices.
 - Include a Subdivision Agreement as part of development approval
Example: *“The developer agrees to advise the purchaser, and future tenants, that noise, dust and odour associated with nearby farm operations may occasionally affect some activities of dwelling occupants”.*
 - Residential building construction to include triple-paned windows within 300 m of the ALR.
- Public access, even by a pathway, is non-farm use and must be approved by the Agricultural Land Commission (ALC).

2.4.6 Interface with Existing Urban Neighbourhood

In order to limit disturbance between existing Urban Neighbourhoods to the North, a minimum level of landscaping or tree preservation is required in each private lot along all rear yards shall be determined from the Minimum Landscape Buffer Treatment width proposed in the land use plan (**Figure 7**) and interface cross section for landscape Buffers described in 2.6.5.3 Transition C: Adjacent to Existing Urban Area Fronting 78 Ave. Landscape buffers area will form part of a Restrictive Covenant held on each lot.

All construction on-site must occur concurrently with erosion control measures to prevent the pollution, degradation, or siltation of natural areas and water courses. This includes the provision of temporary fencing prior to and during construction. Where the retention of native trees and ground cover is required or permitted, a letter from a registered professional landscape architect or registered professional forester shall be submitted, indicating the mitigation measures required during and after the construction to ensure the health of the vegetation to the satisfaction of the City.

2.4.7 Cluster Housing and Green “Open Space” Areas

Cluster housing guidelines apply to the “Low Density Cluster 4 UPA” designation and Low-Medium Density 5 UPA” areas within the Infill Plan. Green Space Transfer” areas refer to the areas of a site where preservation of open space occurs. The potential density from the green space transfer area is intended to be transferred to the “development area” of a site.

Portions of the Green Space Transfer areas may be “community space” and may be used for future public passive or active recreation, community gardens, or rainwater management facilities that meet all design, construction, maintenance, and public safety requirements set forth by the City of Surrey Engineering and Parks, Recreation and Parks Department.

If open space transfer areas are not allocated in a manner consistent with these Infill Area guidelines, density maximums will not consider lot area averaging in all Cluster Residential areas, and densities will not include undevelopable portions of the lot such as Road right-of-ways, and Utility Easement areas, densities should not be calculated on the Gross area, and should not exceed 2 Units Per Acre (UPA) within 200 meters of the ALR and 4 Units Per Acre within 400 meters of the ALR.

Developments should apply for a specific comprehensive development zone with a base zone containing density dimensional and building form standards most closely comparable to the total approved net density of the cluster development. Maximum density based on the entire lot shall not exceed 7 units per net acre.

The boundaries of designated green open space areas, recreation areas, rainwater management facilities, and natural areas shall be clearly delineated on plans, including subdivision plans, rezoning plans.

2.5 “INFILL AREA” LAND USE PLAN

2.5.1 Overview of Land Use Concept Plan

The intent of the Infill Area Land Use Concept Plan is to focus primarily on new residential development and green space preservation. The proposed land use concept is shown on **Figure 5: Infill Area Land Use Concept Plan**.

A central feature of the neighbourhood is the east-west connecting boulevard and central wildlife corridor between the divided 77 Avenue Local Roadway. Single detached housing on larger lots will pre-dominate the area. On the east side of the plan area, there will be a minimum 25 meter treed open space adjacent to Fleetwood Park along with Residential lots fronting onto the Open “Green” Space along a single loaded fronting road. The western portion of the plan contains a similar cluster housing density and form, with a treed open space landscape buffer adjacent to the gold course. The Central Spine of the Plan contains rows of single family homes facing 155 St, future 155A St, and 156 Street.

The structure in the western sector builds upon the existing road layout and provides a road over the existing sewer interceptor line in the southwest corner. In the eastern sector, a new local collector system is introduced in the middle of the lot to allow frontage lots. In both areas, the land use concept illustrates a network of potential local roads. A system of walkway/trail connections links the residential areas with parks, surrounding areas, and existing urban neighbourhood.

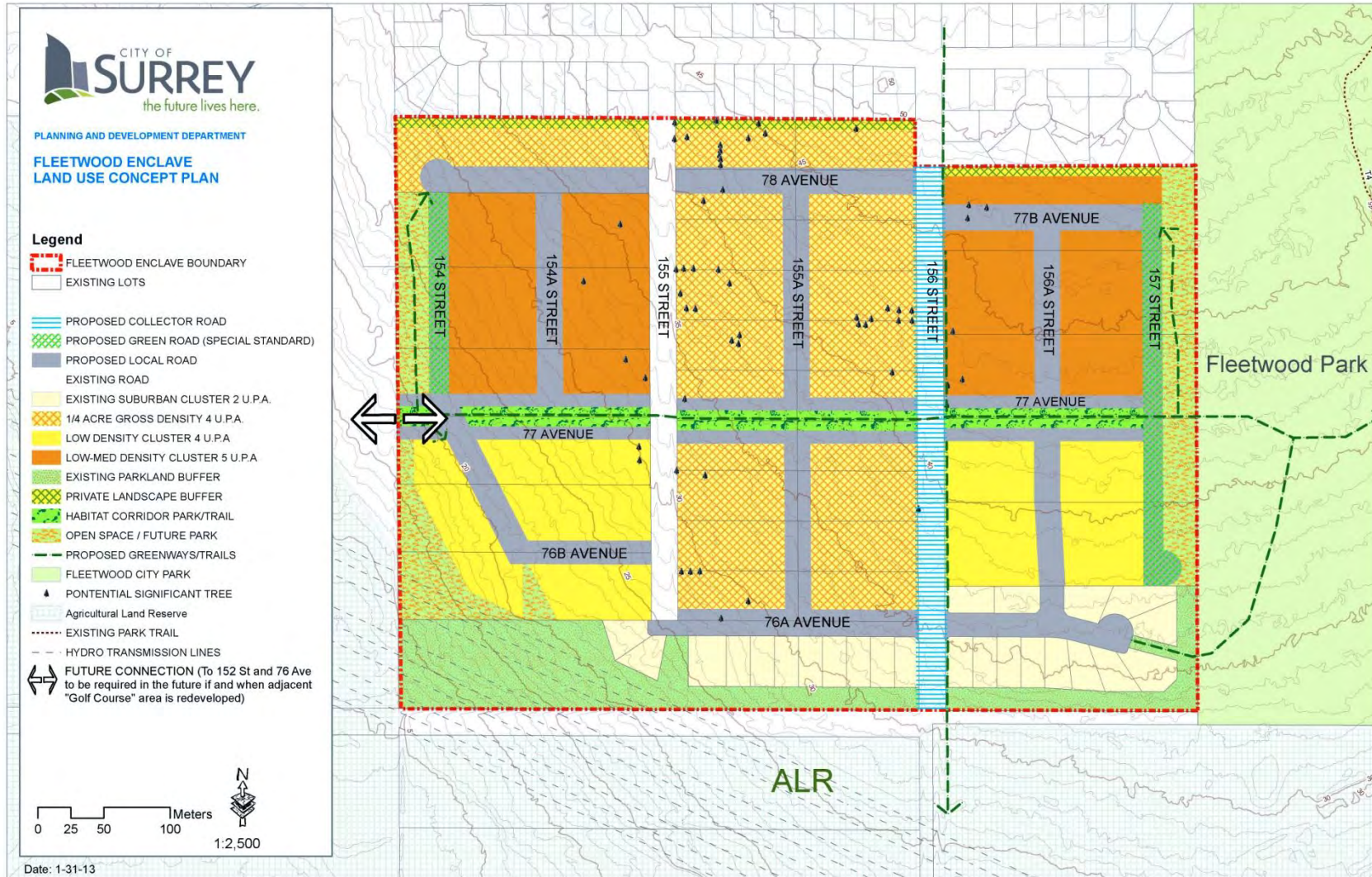






Figure 7 - Infill Area Land Use Concept Plan

2.5.2 General Residential Designation Policies

- The plan provides for traditional single family forms of housing on medium size urban lot sizes in the Infill Area and opportunities for semi-detached (duplex) lots on corners.
- Residential uses are located in a manner that ensures appropriate transitions between uses, both internally in the neighbourhood and externally to adjacent uses.
- Tree preservation for both clumps of high value trees and individual significant trees will be considered as part of Arborist reports and individual lot layouts.
- Variations in setbacks and lot sizes to accommodate tree preservation opportunities will be considered on site by site bases.

The **Table 5** below provides a Summary of the Residential Land Use Designations identified in **Figure 4**. Detailed development guidelines are provided in **Parts 2.5.2.1-2.5.2.3**.

Table 5 - Summary of Residential Land Use Designations

Residential Designation	Density Maximum	Open Space %	General Description	CD Base Zone
 *Suburban Cluster (Zoning Approved in 2011)	2 UPA	50% Open Space Dedication	Single family oriented development clustered away from the ALR, Parkland and Hydro Corridor edge. Lot area averaging using gross density and significant open space component with lot sizes no less than 700 sq m (7,500 sq ft). Minimum lot width of 18 meters.	Residential Cluster (RC) Zone
 Low Density Urban Cluster	4 UPA	13% Open Space Dedication as Passive Park	Primarily single family development, characterized by individual homes constructed in cluster subdivisions away from Fleetwood Park, Habitat Corridor and Golf course lands with lot sizes no less than 465 sq m (5,000 sq ft). Minimum lot width of 15 meters and Minimum lot depth of 30 meters.	Residential Cluster (RC) Zone or RF-G
 ¼ Acre Gross Density	4 UPA	8.5% Parkland Dedication for Habitat Corridor in 77 Ave Blvd	Primarily single family development, characterized by individual homes constructed with lot areas no less than 557 sq m (6,000 sq ft). Minimum lot width of 16 meters and minimum lot depth of 40 meters. Parkland dedication used toward Habitat Corridor Component of Plan. Duplex units on wider lots are permitted at street corners.	Residential Half-Acre Gross Density (RH-G)
 Low-Medium Density Urban Cluster	5 UPA	13 % Open Space Dedication as Passive Park	Primarily single family development, characterized by individual homes constructed in cluster subdivisions with lot areas no less than 371 sq m (4,000 sq ft). Minimum lot width of 13.4 meters for front loaded internal lots. Parkland dedication used toward Open Space Component of Plan. Duplex units may be considered on wider lots at designated street corners.	Residential Cluster (RC) Zone or RF-G

* Approved under Development 7911-0085--00

2.5.2.1 2UPA Suburban Cluster Residential

The 2 UPA Suburban Cluster Residential land use designation is intended to support a suburban life-style and provides a transition between agricultural land uses and more intensive urban land use. This designation is characterized by a range of larger sized lots with significant 50% open space and buffer allocation.

Table 6- Suburban Cluster (2 UPA) Guidelines

2 UPA Suburban Cluster Guidelines	Guidelines
Uses	<ul style="list-style-type: none"> • Single detached, with accessory secondary suite and home based business permitted as per zoning bylaw.
Density	<ul style="list-style-type: none"> • 2 UPA (Gross Density) • FAR 0.45 and maximum allowable floor area 360 sq. m (3,875 sq. ft)
Minimum Open Space to Achieve Gross Density	<ul style="list-style-type: none"> • 46.5% of gross site area(s), with additional <i>Compensation determined by Parks, Recreation and Culture</i>
Principal Building Setbacks	<ul style="list-style-type: none"> • Front: 7.5 m (25 ft) • Rear: 7.5 m (25 ft) • Side: 1.8 m (6 ft) • Flanking Side: 3.6 m (12 ft)
Landscape Buffer(s)	<ul style="list-style-type: none"> • a minimum 17-metre (56 ft.) wide portion of land be dedicated to the City as Parkland along the unopened 76 Avenue right-of-way.
Special Regulations	<ul style="list-style-type: none"> • Standard Secondary Suite Restrictions apply. • No driveway shall be permitted from 156 Street • Register a Restrictive Covenant on title of lots adjacent to ALR to advise of Farming practices in area.
Lot Area Averaging (Subdivision Minimums)	<ul style="list-style-type: none"> • Lot Area: 700 sq.m. (7,500 sq.ft.) • Lot Width: 18 m (60 ft.) • Lot Depth: 30 m (100 ft.)

2.5.2.2 4 UPA Low Density Urban Cluster Residential Lands

The 4-5 UPA Urban Cluster Residential designation is intended to support a urban residential life-style and provides a transition between more intensive urban and agricultural land uses. This designation is characterized by a range of large urban lots with a significant 13% open space allocation.

Table 7 - Cluster Residential (4 UPA) Guidelines

4 UPA Low Density Cluster Guidelines	Guidelines
Uses	<ul style="list-style-type: none"> • Single detached, with accessory secondary suite and home based business permitted as per zoning bylaw, with the potential of duplex buildings on corner lot where feasible.
Density	<ul style="list-style-type: none"> • 4 UPA Gross Density (Cluster) • FAR 0.45 and maximum allowable floor area 325 sq. m (3,500 sq. ft)
Parkland Dedication	<ul style="list-style-type: none"> • Open 'Green Space' areas
Minimum Open Space to Achieve Gross Density	<ul style="list-style-type: none"> • 13% of gross development site • <i>(Compensation to be determined by Parks for Habitat Corridor)</i>
Habitat Corridor	<ul style="list-style-type: none"> • 15 m (50 ft) dedicated Habitat Corridor Connection <i>(Can form portion of Minimum Open Space % to Achieve Gross Density)</i>
Principal Building Setbacks	<ul style="list-style-type: none"> • Front: 7.5 m (25 ft) • Rear: 7.5 m (25 ft) • Side: 1.8 m (6 ft) • Flanking Side: 3.6 m (12 ft) • Front 3.5 m (12.5 ft) Attached Garage
Special Regulations	<ul style="list-style-type: none"> • Standard Secondary Suite Restrictions • Register a Restrictive Covenant on title of lots adjacent to ALR to advise of Farming practices in area.
Lot Area Averaging (Subdivision Minimums)	<ul style="list-style-type: none"> • Lot Area: 465 sq. m. (5000 sq. ft.) • Lot Width: 15 m (49 ft.) • Lot Depth: 30 m (100 ft.)



2.5.2.3 4 UPA ¼ Acre Residential Gross Density Lands

The 4 UPA ¼ Acre Residential Gross Density designation is intended to support a urban residential life-style and provides a transition between more intensive urban lots to the north and suburban land use adjacent to the ALR to the south. This designation is characterized by large urban lots with 8.5% Parkland Dedication for Habitat Corridor Areas or cash in lieu allocation.

Table 8 - 1/4 Acre Residential Gross Density (4UPA) Guidelines

4UPA ¼ Acre Gross Density Guidelines	Guidelines
Uses	<ul style="list-style-type: none"> Single detached, with accessory secondary suite and home based business permitted as per zoning bylaw, with the potential of duplex buildings on corner lots where indicated on land use plan.
Density	<ul style="list-style-type: none"> 4 UPA Gross Density FAR 0.45 and maximum allowable floor area 360 sq. m (3,875 sq. ft)
Parkland Dedication	<ul style="list-style-type: none"> 8.5% of gross site area for wildlife corridor areas with cash in lieu <i>Compensation for lot dedication shortfall to be determined by Parks</i>)
Open Space Allocation	<ul style="list-style-type: none"> N/A
Habitat Corridor	<ul style="list-style-type: none"> 15 m (50ft) dedicated Habitat Corridor Connection <i>(Can form portion of Parkland Dedication)</i>
Principal Building Setbacks	<ul style="list-style-type: none"> Front: 7.5 m (25 ft) Rear: 7.5 m (25 ft) Side: 1.8 m (6 ft) Flanking Side: 3.6 m (12 ft)
Special Regulations	<ul style="list-style-type: none"> Standard Secondary Suite Restrictions
Lot Area Averaging (Subdivision Minimums)	<p>Single Family</p> <ul style="list-style-type: none"> Lot Area: 557 sq. m. (6000 sq. ft.) Lot Width: 16 m (52.5 ft.) Lot Depth: 40 m (131.2 ft.) <p>Semi-Detached</p> <ul style="list-style-type: none"> Lot Area: 743 sq. m. (8000 sq. ft.) Lot Width: 19 m (62.3 ft.) Lot Depth: 30 m (131.2 ft.)



2.5.2.4 5 UPA Low-Medium Density Urban Cluster Residential Lands

The 5 UPA Low-Medium Urban Cluster Residential designation is intended to support a urban residential life-style and provides a transition between more intensive urban and agricultural land uses. This designation is characterized by a range of urban lots with a significant 13% open space allocation on site.

Table 9 - Low-Medium Cluster Residential (5 UPA) Guidelines

5 UPA Low-Medium Density Cluster Guidelines	Guidelines
Uses	<ul style="list-style-type: none"> • Single detached front loaded lots, with accessory secondary suite and home based business permitted as per zoning bylaw, with the potential of duplex buildings on large corner lot where feasible.
Density	<ul style="list-style-type: none"> • 5 UPA Gross Density (Cluster) • FAR 0.45 and maximum allowable floor area 325 sq. m (3,500 sq. ft)
Parkland Dedication	<ul style="list-style-type: none"> • Open 'Green Space' areas
Minimum Open Space to Achieve Gross Density	<ul style="list-style-type: none"> • 13% of gross site area <i>Compensation for shortfall to be determined by Parks for Habitat Corridor</i>
Habitat Corridor	<ul style="list-style-type: none"> • 15 m (50 ft) dedicated Habitat Corridor Connection <i>(Can form portion of Minimum Open Space % to Achieve Gross Density)</i>
Principal Building Setbacks	<ul style="list-style-type: none"> • Front: 6.0 m (20 ft) • Rear: 7.5 m (25 ft) • Side: 1.2 m (4 ft) • Flanking Side: 2.4 m (8 ft)
Special Regulations	<ul style="list-style-type: none"> • Standard Secondary Suite Restrictions
Lot Area Averaging (Subdivision Minimums)	<p>Single Family</p> <ul style="list-style-type: none"> • Lot Area: 465 sq. m. (5000 sq. ft.) • Lot Width: 13.4 m (49 ft.) • Lot Depth: 30 m (100 ft.) <p>Semi-Detached</p> <ul style="list-style-type: none"> • Lot Area: 743 sq. m. (7000 sq. ft.) • Lot Width: 19 m (62.3 ft.) • Lot Depth: 30 m (131.2 ft.)

2.6.5 Land Use Transitions

There will be appropriate land use transitions and treatments between adjacent uses, both within the Infill Area and with the outside surrounding uses such as agricultural, parks and existing urban areas to the north. Transitions and locations of specific cross-sections are noted on **Figure 5 Transition Area Cross Sections** as described in **Parts 2.6.5.1-2.6.5.7**.

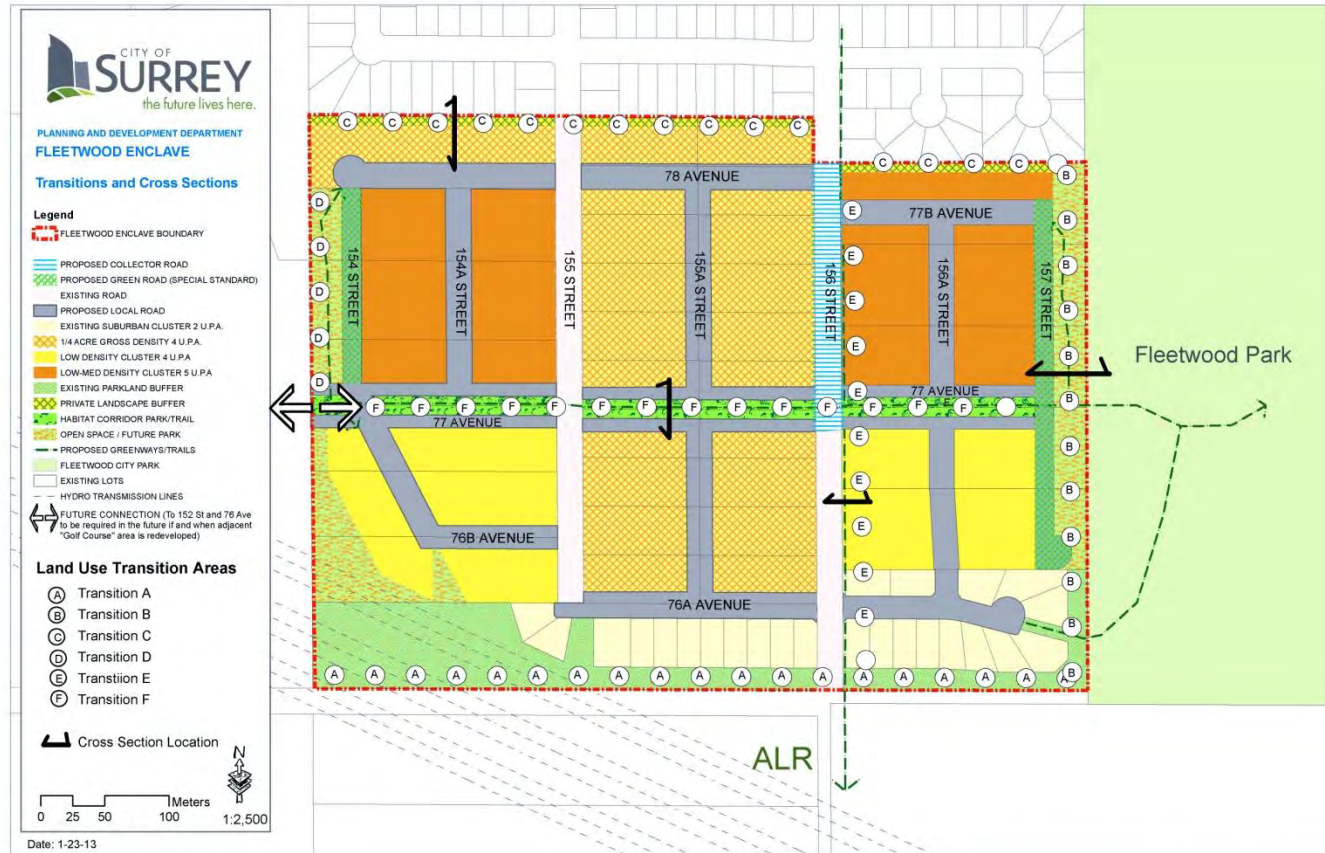


Figure 8 - Transition Area Cross-Sections

2.6.5.1 Transition A: Adjacent to the Agricultural Land Reserve

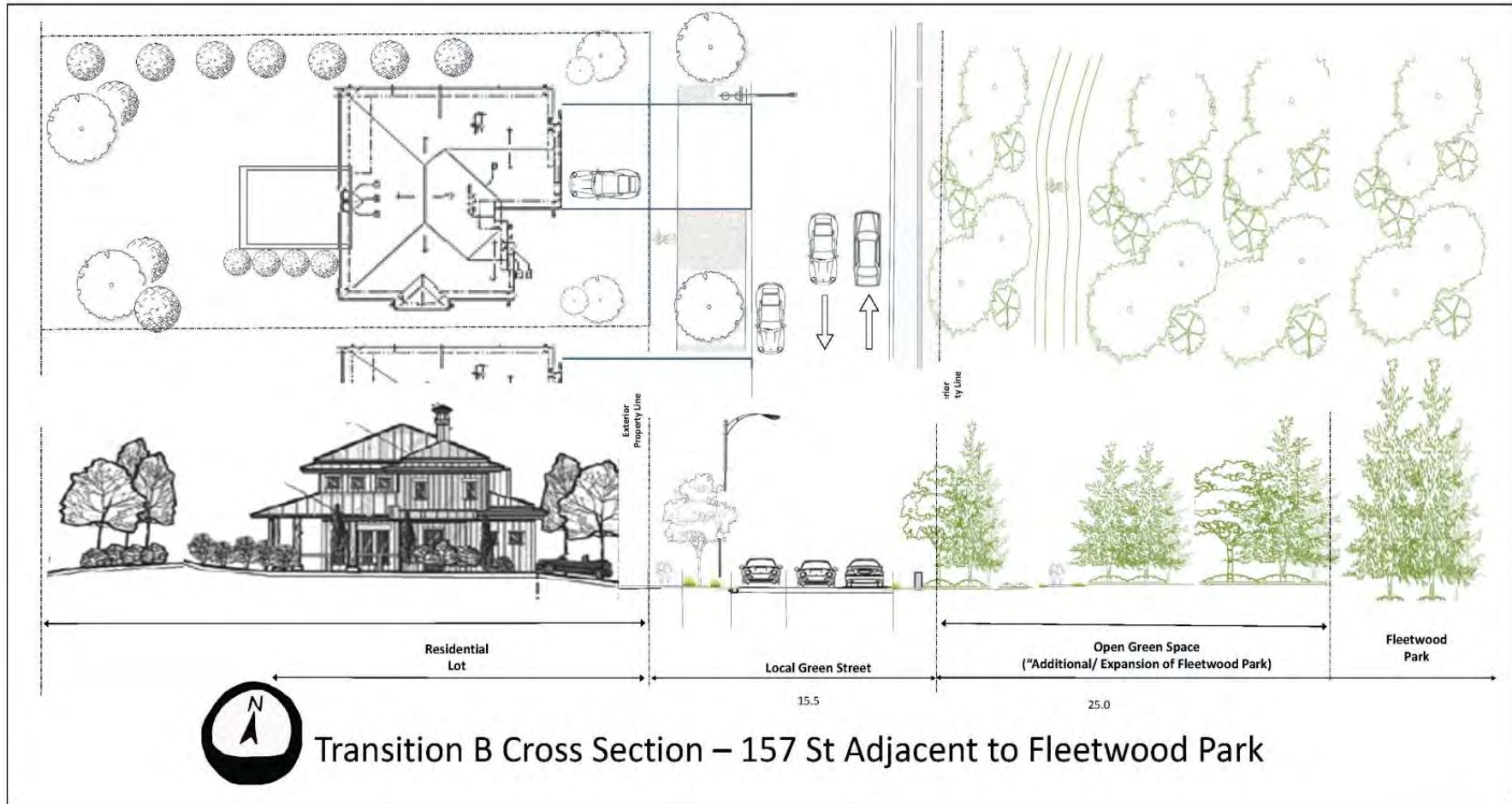
To address the agricultural interface, a 17-metre (56 ft.) wide portion of dedicated parkland is provided along the 76 Avenue right-of-way, which is adjacent to the ALR boundary. This land will separate the residential uses from the adjacent ALR, as required by *Policy No. O-23 "Residential Buffering Adjacent to the ALR/Agricultural Boundary"*. Parks, Recreation and Culture will maintain this portion of the park as a natural area. The applicant has been required to provide any improvements to the parkland to augment the existing vegetation, to establish a maintenance fund, and to erect a black, chain-link fence along the southern property line of the residential properties.

2.6.5.2 Transition B: Adjacent to Fleetwood Park

The transition between the Fleetwood Park area and residential will encourage clustering housing away from the edge to preserve green space and an open, integrated public interface which will 'face the green' and Fleetwood park, while respecting the transition between public and private realms.

A small 15.5 meter local road right-of-way (Green Street) is proposed 25 meters west of the Park Edge (The buffer area) with no parking or sidewalk along the eastern edge of the roadway. Natural areas pathway will be established by Parks within Fleetwood Park adjacent to 157 St. Low, open fencing and landscaping will be consistent with the City's CPTED principles and tree planting will be provided on both sides of the road. Future public access to natural area trails within Fleetwood Park should be carefully managed to avoid impacts to sensitive habitat. A formal park access will be completed by the City from the intersection of 77 Ave and 157 Street. Specific architectural guidelines for the single family lots will be developed during the rezoning process, that will specify low, open but continuous fencing, reduced front yard setbacks to a minimum 3.5 meters, and an encouragement for balconies overlooking parks. Finished floor elevations will be established 0.6 to 0.8 m above the park elevation where possible. The architectural guidelines will be implemented in the form of a building scheme enforced by covenant registered on title.

See **Cross Section 1 – Transition B Adjacent to Fleetwood Park Drawing** for details.



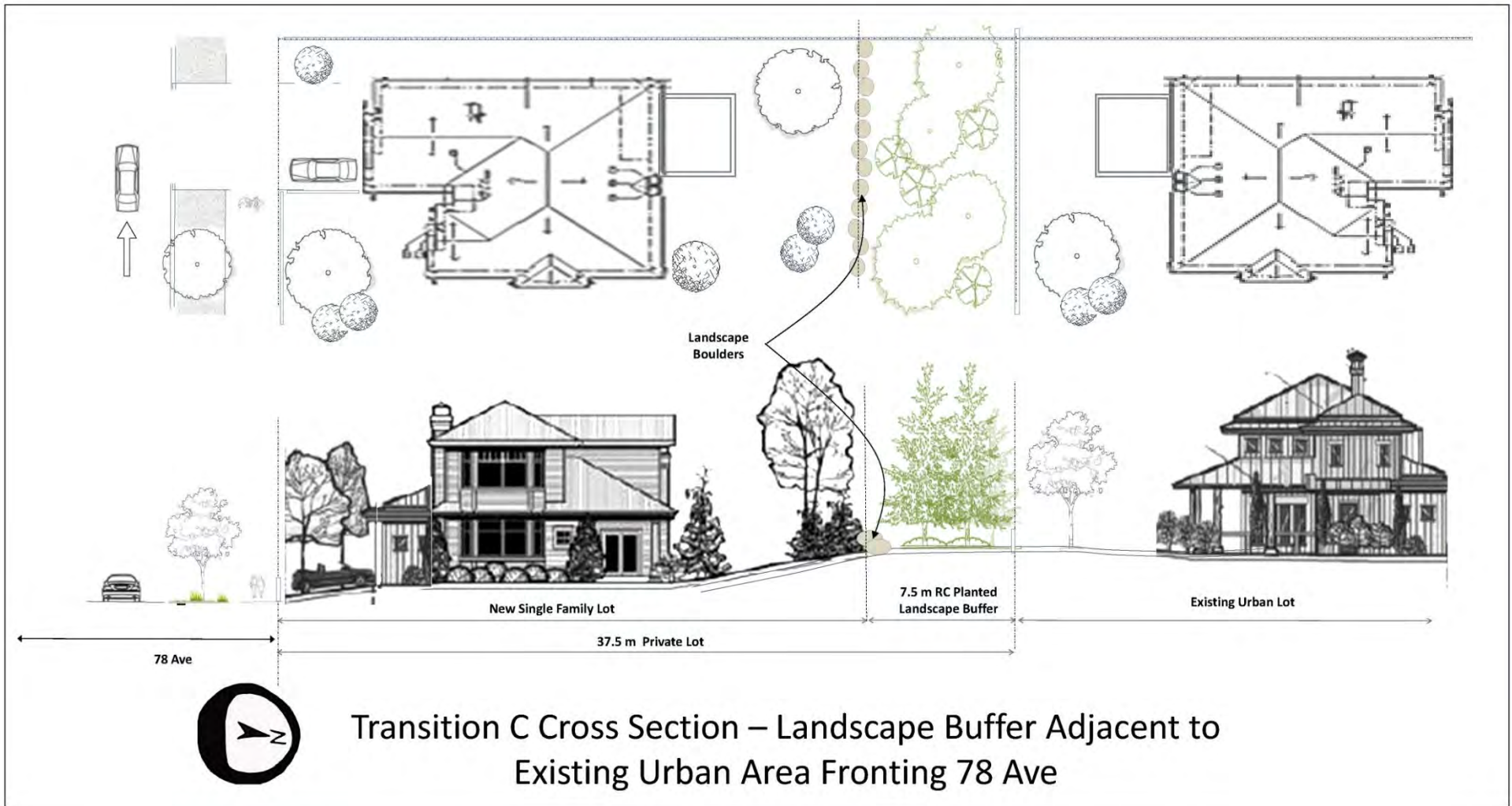
Cross Section 1 - Transition B Adjacent to Fleetwood Park

2.6.5.3 Transition C & D: Adjacent to Existing Urban Area Fronting 78 and 78 B Ave

Single detached housing fronting on 78 and 78 B Avenue will also be developed as a transition to the residential development to the north. The Registered Building Scheme for these lots will require additional year yard landscaping and a Restrictive Covenant Landscape Buffer registered on the rear 7.5 meter or 15 meters for lots fronting 78B Ave of the lot to soften the transition and include a treed rear yard.

Landscaped buffers will be required to be installed prior to the issuance of building permits related to new residential subdivisions and developments. Landscaping will include trees, including coniferous trees, as well as native vegetation, within the required landscaped buffer. Wherever possible, double rows of trees should be planted; with any existing mature trees within the buffer area is to be preserved. A majority of the plant material selected should include low maintenance, indigenous vegetation and should be able to survive with little or no fertilizers. A decorative rock wall or large stone boulders will be installed in front of the Landscape buffer to delineate the edge of the backyard.

See **Cross Section 2 – Transition C & D Landscape Buffer Adjacent to Existing Urban Area Fronting 78 and 78 B Ave Drawing** for details.



Cross Section 2 – Transition C- Landscape Buffer Adjacent to Existing Urban Area

2.6.5.4 Transition E: Adjacent to 156 Street Multiuse Pathway

A four meter wide multi use pathway will form part of 156 street, as part of the 156 Street Greenway. A 2.0 meter Statutory Right-of-way will be established along the east side of 156 from 78A Avenue to Surrey Lake Park as shown in the detail below and on the south side in the future Infill Area south of the unopened 70 Avenue right-of-way. The east and west side of 156 Avenue, after redevelopment, is expected to include front loaded single family homes.

See **Cross Section 2 – Transition E 156 Greenway Drawing** for details.



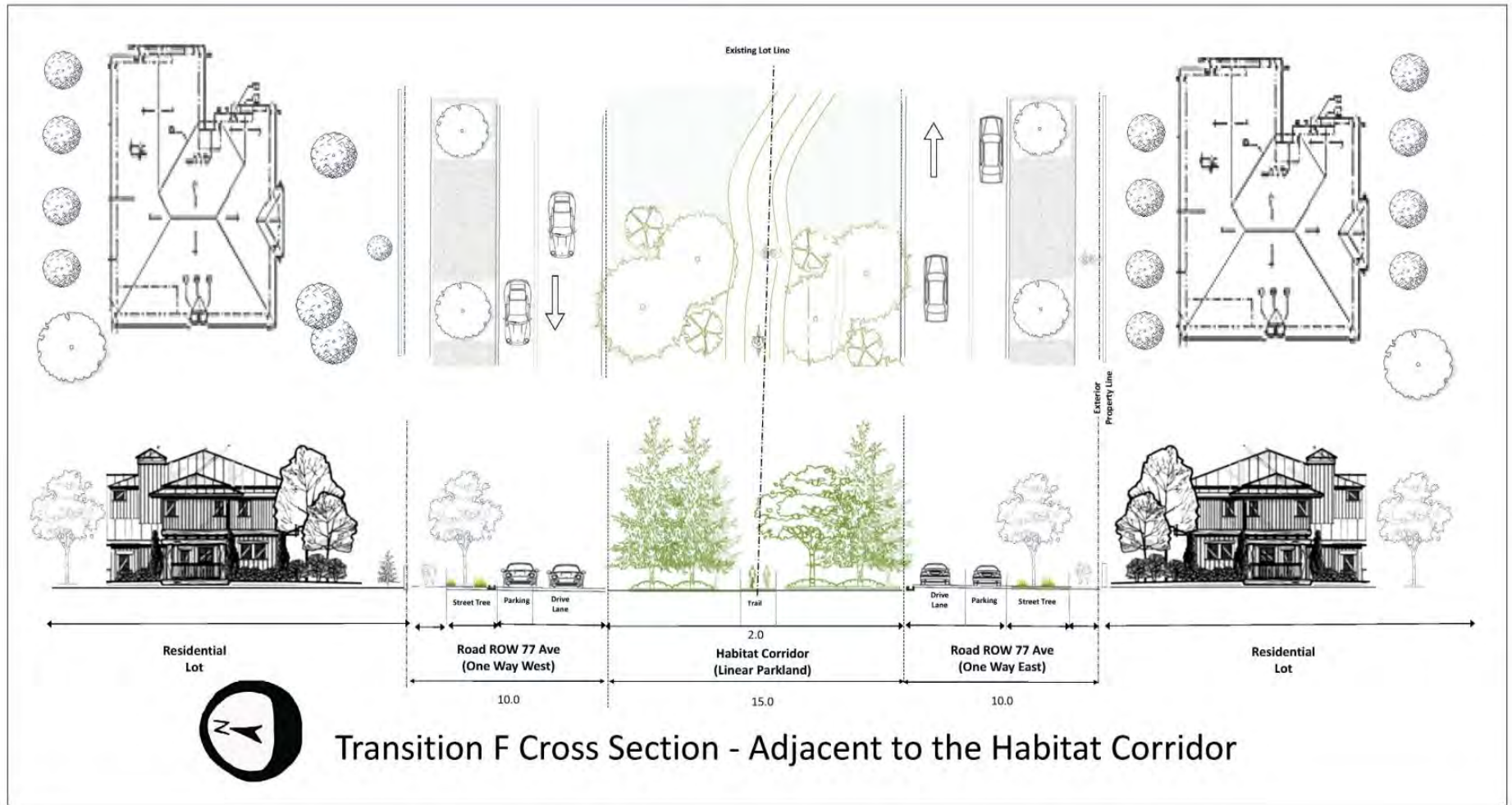
Transition E Cross Section – 156 Street Greenway

Cross Section 3 - Transition E -156 Street Greenway

2.6.5.5 Transition F: Adjacent the Habitat Corridor

A 15 meter East- West Habitat Corridor will be located within the centre Boulevard of 77 Ave. Given the high ecological significance and purpose of the Habitat Corridor, special care must be taken in the design of 77 Ave adjacent to the Habitat Corridor as well as the park pathway within the Habitat corridor. Access and tree protection within the natural areas should be carefully managed to avoid impacts to soils and retained trees. A 2 meter gravel pathway will meander within the Habitat Corridor providing access to Fleetwood Park and 156 Street greenway.

The majority of houses will contain side yards which will flank 77 Ave except for south west of 155 Street, where housing will front onto the south side of 77 Ave.



Cross Section 4 – Transition F Habitat Corridor Interface

2.6.5.6 Transition G: Adjacent to Golf Course Lands

The transition between the Golf Course and residential will encourage clustering housing away from the edge to preserve green space and an open, integrated public interface which faces the green space, while respecting the transition between the two private realms.

A small 15 meter wide local road right-of-way “Neo Traditional Green Street” is proposed 25 meters east of the North Western Boundary of the Fleetwood Enclave with no parking or sidewalk along the western edge of the roadway. Low, open fencing and landscaping will be consistent with the City’s CPTED principles and tree planting will be provided on both sides of the road. Future public access to nature trail pathway along the western side of 154 St within the greenway should be carefully managed to avoid impacts to sensitive habitat. Specific architectural guidelines for the single family lots will be developed during the rezoning process, that will specify low, open but continuous fencing, reduced front yard setbacks to a minimum 3.5 meters, and an encouragement for balconies overlooking parks. Finished floor elevations will be established 0.6 to 0.8 m above the park elevation where possible. The architectural guidelines will be implemented in the form of a building scheme enforced by covenant registered on title.

Along the South Western Boundary of the plan housing will back onto open space buffer area near the ALR Edge. Minimum Depth of the buffer area is 7.5 m and provide a continuous greenway belt along nearly the entire western edge of the plan. Housing will front the angled 154 St. **See Cross Section G – Adjacent to Golf Course Lands.**

2.6.6 Ecosystem Management Opportunities

Approximately 15 Acres (24%) of the Plan Area is within the Fleetwood Park Hub (#23) , and 22 Acres (35%) of the Plan Area contains EMS Sites as identified in the Surrey Ecosystem Management Study (EMS). The EMS area portions of the plan area are made up of mainly-natural (42%) vegetation cover comprised of closed-canopy Deciduous and Evergreens trees. The total overall size of the (Fleetwood Park) Terrestrial Hub is 120 Acres, and has been classified with an ecological significance score of 60/100, ranked #31 out of a possible 88 total Terrestrial Hubs in the City, with an important habitat 'ecosystem' corridor opportunity connection to Surrey Lake Park Hub (#20) to the south in the ALR.

See **Figure 6 – Ecosystem Management Study Green Infrastructure Network Areas Map.**

The Infill Area plan (**Figure 4**) conserves approximately (9 Acres) of the subject sites EMS Hub, site area as in the form of a Habitat Corridor (1.8 Acres), Open Space (5.6 Acres), Park Buffer (4.2 Acres), Landscape Buffers (1.2 Acres) and Multiuse Greenways (0.6 Acres); which is 20 % of the total plan area, or 23 % of the Existing EMS areas.

See **Figure 7 - Proposed Green Infrastructure Areas Map.**

Section 4 of the Surrey EMS describes these Terrestrial Habitats, serving highly important ecological functions, ranging from water purification and erosion protection, to habitat for birds and small terrestrial wildlife which represent good candidate areas for ecological protection and restoration as part of development, as some areas remain degraded from past land clearing and development activities. Trails, utilities and other access to the Fleetwood Park Hubs should be planned in ways that limit disturbance to the species resident in the hub. Trails may be restricted to adjacent lands or outer limits, so that core is undisturbed.

2.6.7 Habitat Corridors

The Fleetwood Enclave plan includes a local habitat corridor connection between Fleetwood Park and Golf Course Lands with connection to the Surrey Lake terrestrial Hub. An additional secondary corridor connection along the ALR Boundary also helps reduce risks to biodiversity, and wildlife movement. Preference to the East West Corridor location was given to protecting or restoring continuity of the corridor with higher ecological significance shown in **Figure 9 of Ecosystem Management Study, and Figure 10 – Fleetwood Enclave Habitat Corridors Assessment**, and in the Diamond Head Environmental Study Wildlife Corridor map. The Habitat Corridor is along existing wooded sites. The 'minimum' width of the habitat corridor is 15 meters was determined based on detailed biological studies of the species that are or could use the habitat provided by the lined hubs and corridors.

See **Cross Section 5** for details on Habitat Corridor Interface.

Where roads or other barriers are being constructed or rebuilt, provisions for ease of passage wildlife species that the corridor serves should be provided, such as restricting road

access through the corridor where possible, and re-vegetation (naturalization) of utility corridors to enhance important linear connections between hubs and sites. For example, where the east west wildlife corridor crosses 155 and 156 Street, efforts should be made to include a narrowed paved surface that may include roadway bulges.

These rights-of-way may be developed for wildlife and other ecological values; for example, the corridors with overhead power lines are ideal location to manage for old field habitat, rather than re-establishing tall tree cover.

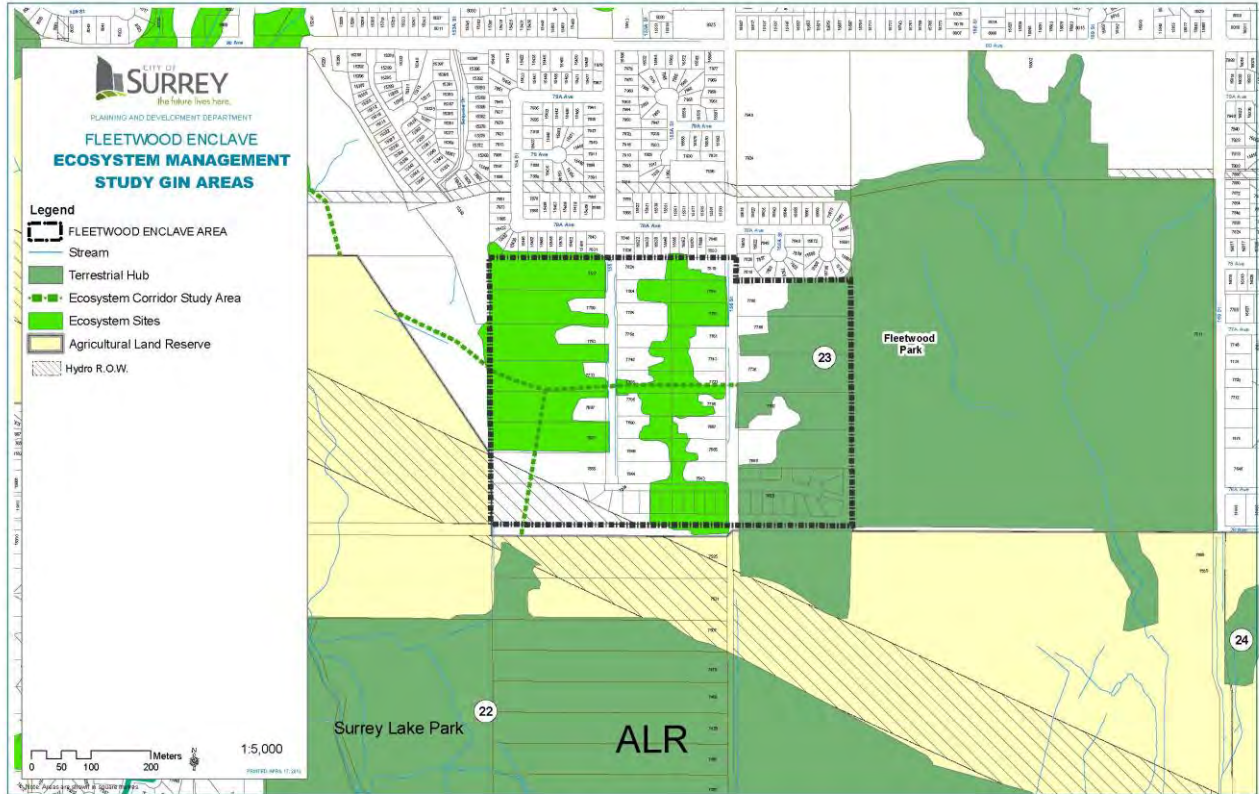


Figure 10 - Ecosystem Management Study Green Infrastructure Network Areas



Figure 9 - Fleetwood Enclave Habitat Corridors Assessment

2.6.8 Parks, Recreation, Trails and Open Space

The Fleetwood Enclave Plan establishes an open space and trail network that provides passive and natural area parkland, as well as active connections between the plan area and adjacent community amenities. The network takes advantage of the Surrey Lake Greenway's alignment, as well as the plan area's adjacency to Fleetwood Park, the Eaglequest Golf Course and the Agricultural Land Reserve.

Along the eastern edge of the plan area a 25m parkland buffer will provide a natural areas addition to Fleetwood Park. A new park entrance at the eastern end of 77 Ave will include a small open space and picnic area, as well as a 3.0m gravel pathway connection east to existing amenities within Fleetwood Park. A secondary park entrance at the eastern end of 76A Ave will provide a 2.0m pathway that will connect north-east to the primary pathway within the Park. A 2.0m gravel pathway will also run along the eastern edge of 157 Street in lieu of a sidewalk. The interface with Fleetwood Park will remain primarily natural to retain and take advantage of existing forest canopy.

A 15m planted median within 77 Ave will address an important east-west habitat corridor and linear green space. A 2.0m pathway will provide an extension to the Fleetwood Park connection through the median. The pathway will meander to enable maximum tree retention. At 156 Street, where the corridor intersects the Surrey Lake Greenway, a small amenity node will provide seating and wayfinding. Along the western edge of the plan area a parkland buffer, averaging 25m in width, will provide a natural area interface with the Eaglequest Golf Course. This buffer will form the western end of the east-west habitat corridor and provide a habitat link to Bear Creek. The buffer will also ensure the retention of an important south-west view corridor over the golf course, Bear Creek and the Agricultural Land Reserve.

A 2.0m pathway along the western edge of 154 Street will provide local circulation and access to passive park space and picnic areas that take advantage of the views. Along the southern extent of the plan area natural area parkland will provide a buffer between development and the Agricultural Land Reserve.

Table 9 indicates the areas allocated, and **Figure 11** shows the location of various components of the Public, Park, Trails and Natural Areas network. The network serves both ecological and social purposes by supporting the surface drainage system, providing sufficient avian and terrestrial wildlife habitat, maintaining surface flows, and providing areas for both passive recreation.

Public and Natural Areas (Land Use Type)	Hectares	Acres	% of Total Plan Area
Passive Parkland Buffer along ALR Edge (Existing Dedicated Parkland)	2.08	5.14	7.9%
Fleetwood Park Extension (Treed Linear Open Space - Parkland)	0.77	1.90	2.9%
Western Edge Parkland (Treed Linear Open Space - Parkland)	1.03	2.56	3.9%
East-West Habitat Corridor (Linear Parkland)	0.71	1.75	2.7%
156 Street Greenway (4.0 m Multiuse Pathway)	0.12	0.31	0.4%
TOTAL	4.72	11.66	17.9%

Table 10 - Public, Park and Natural Areas



Figure 11 - Parks, Recreation, Trails and Open Space

2.6.9 Pedestrian Circulation

The proposed circulation system is shown on **Figure 12: Pedestrian Circulation Plan**. It is a fine-grained combination of sidewalk and walkway connections through the neighbourhood, connecting to adjacent multi-use paths along 156 Street and the East West Wildlife Corridor trail, as well as direct access to adjacent parkland and the existing urban neighbourhood and schools to the north. In addition, there are paths through the local parks as part of the local circulation system.

- The multiuse pathway connection along 156 Street is proposed to be 4.0meter wide partially within 2.0 meter wide Statutory Right-of-way along the eastern fronting lots.
- A 2.0 meters wide gravel pathway will be provided within the median of the East West Habitat Corridor.
- Bicycles will be accommodated within the neighbourhood primarily on local traffic calmed roadways and through connections to the overall Surrey greenway system.
- Pavement treatments will be provided where the pedestrian circulation network crosses streets.
- Walkway locations are conceptual and the Planning & Development, Engineering, and Parks, Recreation & Culture Departments will determine final design jointly.
- Driveways will be paired to limit potential conflict with pedestrians on sidewalks.

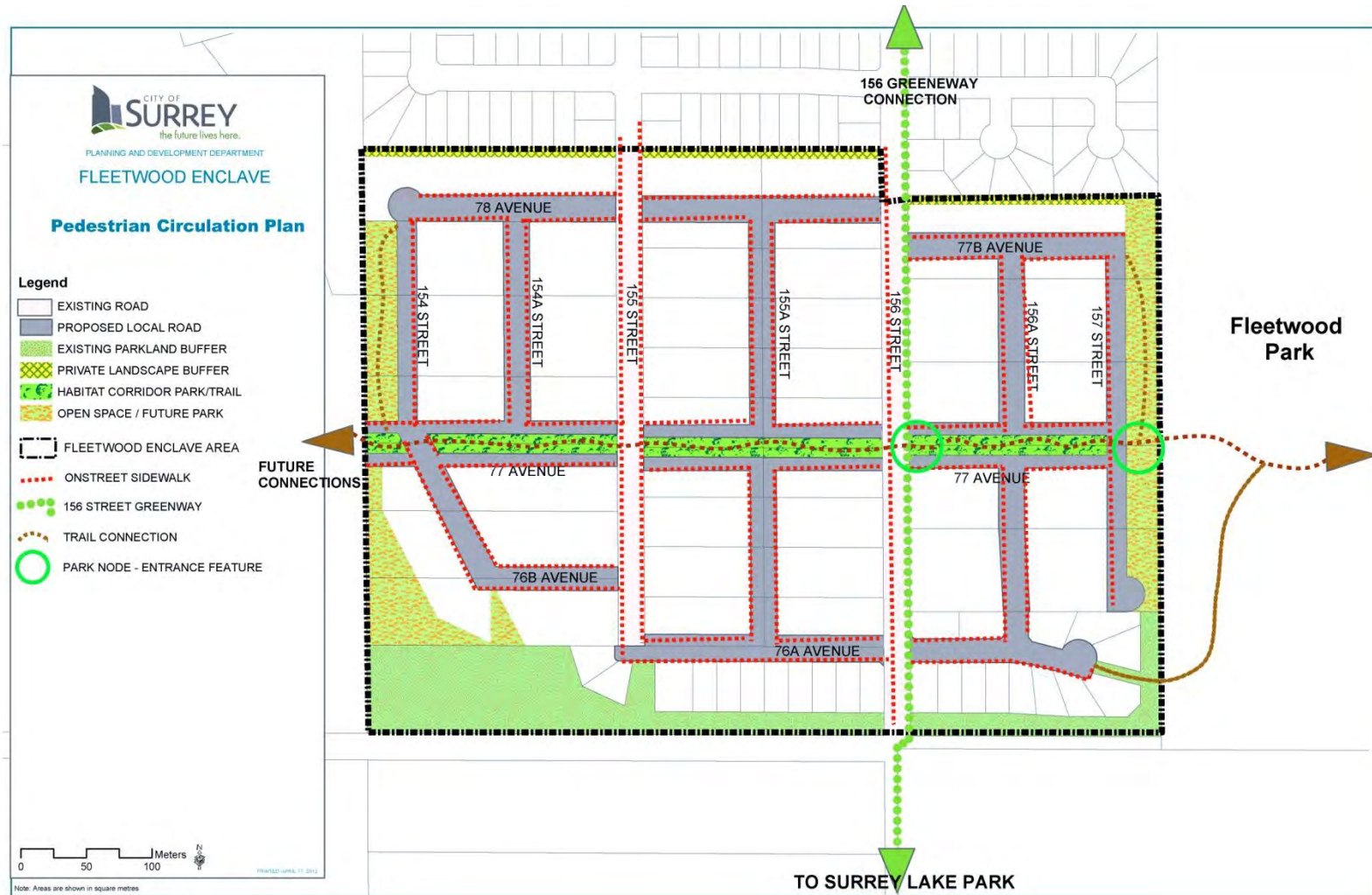


Figure 12 -Pedestrian Circulation Plan



Figure 13 - Fleetwood Enclave Block Plan Dimensions

2.7 LAND USE AND POPULATION STATISTICS

A statistical summary of each of the land use components of the Land Use Concept for the Infill area –area, dwelling units, and population projected is shown below.

Table 11 – Statistical Summary and Projected Build out Scenario for the Fleetwood Enclave Plan Area

Fleetwood Enclave Build out Scenario							
Land Use	Acres	% of Area	Principal Units	*Principal Unit Population	Secondary Suites	*Secondary Suite Population	Total Population
EXISTING ROADS	3.9	6%	0	0	0	0	0
PROPOSED ROADS	12.7	20%	0	0	0	0	0
PROPOSED PARKLAND BUFFER	5.1	8%	0	0	0	0	0
HABITAT CORRIDOR	1.8	3%	0	0	0	0	0
CLUSTER OPEN SPACE/FUTURE PARK	4.4	7%	0	0	0	0	0
LANDSCAPE BUFFER (RC)	1.0	2%	0	0	0	0	0
EXISTING SUBURBAN CLUSTER 2 U.P.A.	4.5	7%	24	77	24	44	121
1/4 ACRE GROSS DENSITY 4 U.P.A.	13.9	22%	79	254	79	95	262
LOW DENSITY URBAN CLUSTER 4 U.P.A	7.6	12%	52	168	52	95	646
LOW-MEDIUM DENSITY CLUSTER 5 U.P.A	9.7	15%	85	263	85	155	418
TOTAL	64.7	100.0%	240	762	232	437	1,199

* Population of Principal Unit is based on 3.229 People per Dwelling

** Population of potential Secondary Suites based on 1.82 People per Secondary Suite Dwelling

2.8 LAND CONSOLIDATION AREAS

Land consolidation areas have been identified to advise future developers of consolidation requirements and to ensure feasible development areas which achieve an equitable distribution of road dedication, land development, and construction costs.

Land consolidation may be required for:

- small acreage parcels;
- encumbered properties with little development potential;
- cluster designation areas;
- sharing road construction costs.

Within the plan area there are a number of parcels where lot consolidation is required in order to develop. These land consolidation opportunities can often be determined on a case by-case basis at development application stage. In some cases, however, pre-determined consolidation requirements are identified in the Land Use Plan in order to avoid creating remnant pieces that could not develop on their own. These particular properties should be developed together through consolidation and land assemblies or through coordinated development.

If land consolidation is not possible during the development process, the developer must:

- Demonstrate that the development potential of the excluded property is not compromised; and
- Share road construction costs amongst properties shown in the land consolidation area.

Consolidation areas are identified in **Figure 6** and a **Table 10** describes the build out summary of proposed land use and estimated unit yields.

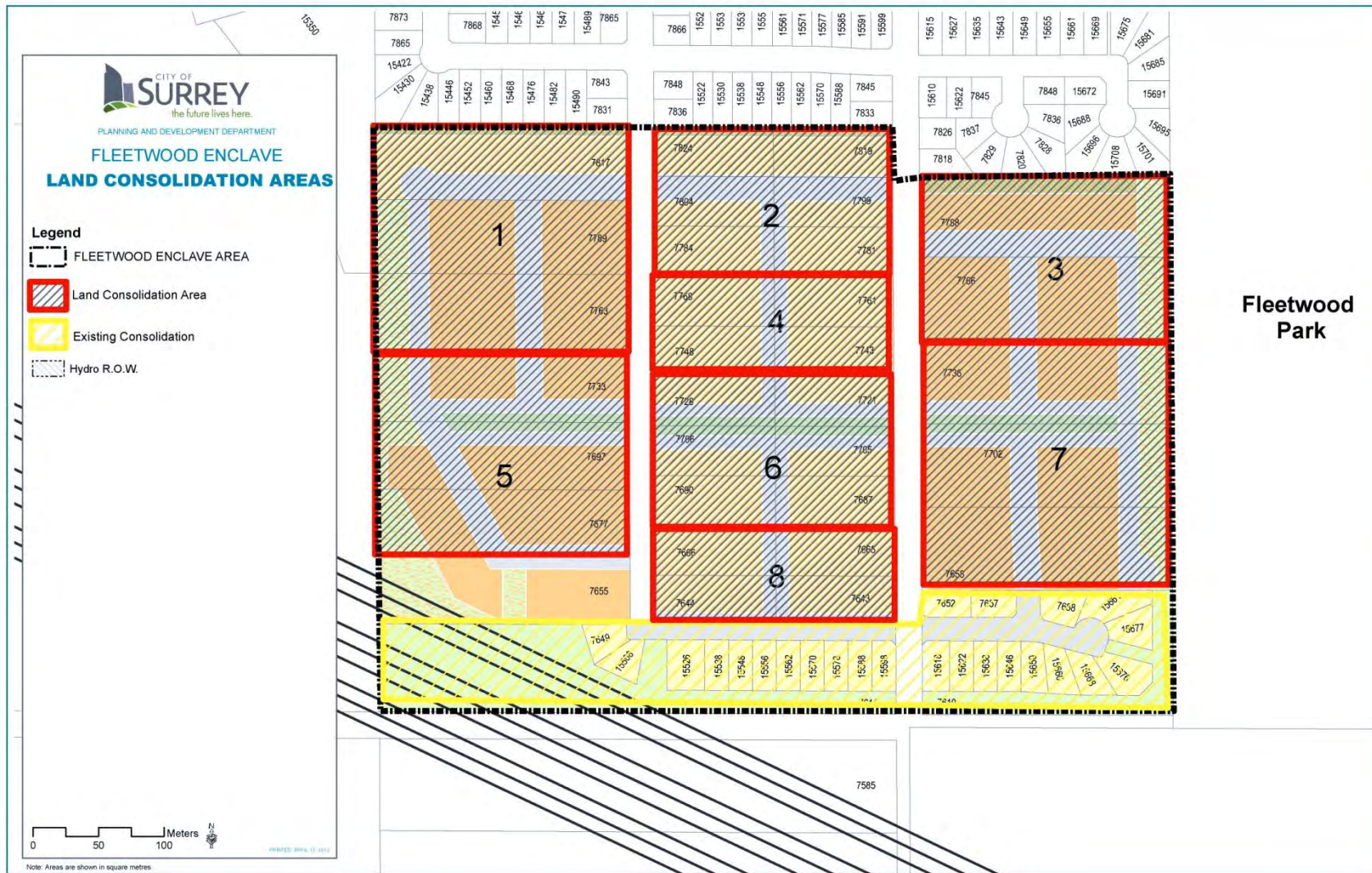


Figure 14 - Land Consolidation Area

2.9 ZONING BYLAW VARIATIONS

The implementation of the Southwest Fleetwood Enclave infill development concept strategy will require variations of some existing Zoning Bylaw regulations (probably through the use of comprehensive development zones), based on the Residential Cluster (RC), Residential Single Family Gross Density (RF-G) and/or Residential Half Acre Gross Density (RH-G) Zones, including variations to:

- front and rear yard setback dimensions;
- density limits and floor space ratios;
- site coverage maximums;
- minimum lot size, width, and depth requirements; and
- minimum open 'Green Space' dedication percentages.

2.9.1 Cluster / Gross Density Zones

Gross density cluster developments, consisting of single dwellings, and in some instances duplexes, are permitted in Urban and Suburban designations as a means of preserving significant open space, subject to overall density restrictions as per OCP Policy 3.5.2.

Surrey's Zoning Bylaw by means of the Residential Cluster (RC) Zone, Single Family Gross Density (RF-G) Zone, and Residential Half Acre Gross Density (RH-G) Zone, permits lot area averaging within a subdivision. This provision provides that parcels within an area below the specified minimums may be created as long as the average area of all parcels in the subdivision is greater than or equal to the minimum. Such provision or related provisions within an associated Comprehensive Development (CD) Zone can be used to permit the clustering of the development on larger parcels, which can be used to set aside environmentally sensitive areas and/or woodlands, with defined green 'open space' areas from development, as identified in the Fleetwood Enclave Infill Area Land Use Plan.

Cluster and Gross Density areas include the possibility of provisions to include a minimum average lot area for any park, open 'green space', road, and wildlife corridor areas within the parent parcel within the new area plan as an incentive to use lot area averaging cluster designation.

Part 3: Design Guidelines

3.1 OVERALL DESIGN PRINCIPLES

- 3.1.1 To create an identifiable and sustainable community of high visual quality, lasting value, and character through architectural coordination, details and material selection.
- 3.1.2 To develop a distinctive public realm with a sense of place that is not just conventional subdivision park or green space, but a carefully designed walking network of interconnected local roads which face onto green space to create an interactive, pedestrian friendly and safe community environment.
- 3.1.3 To develop a distinctive private realm of defined public, private and semi-private yards, terraces, porches and residential buildings that will complete the street enclosures and vistas to create a strong street edge.
- 3.1.4 To have a diversity of physical environment: private and public spaces, integration of natural topography, trees and habitats, connected city park system and a differentiated arrangement of street and character areas.
- 3.1.5 To encourage a mix of compatible housing designs that accommodates a diversity housing styles.
- 3.1.6 To use residential building design that provides an enjoyable pedestrian scale and character.
- 3.1.7 To encourage a pedestrian friendly environment with convenient access to park areas, transit areas along 152 Street and 80 Avenue, Schools and other destinations.
- 3.1.8 To incorporate natural on and off site drainage and rainwater management techniques.
- 3.1.9 To preserve existing significant tree habitat and canopy coverage, where practical.
- 3.1.10 To provide traffic calming through the use of minimum road widths at all intersections and circular landscaped islands on through roads.
- 3.1.11 To develop a distinct but connected residential neighbourhood that is comprehensively designed for an aesthetically pleasing place to live, live and work and play.
- 3.1.12 To ensure improved access to both private outdoor living spaces and public open spaces in adjacent Fleetwood and Surrey Lake Parks.

3.1.13 To develop streets that will encourage walking and pleasant views with trees and street furniture where appropriate for rest and comfort.

3.1.14 To develop the general residential character by means of a variety of building setbacks, private outdoor space created by low front property line boundary landscape buffers, and road design.

3.1.15 To develop street specific built character to coordinate such elements as siting, massing for site specific lots, exterior finishing materials and colors for single family housing.

3.1.16 To provide an architectural control and review process to ensure diversity of built form and material composition detailing as set out in the building scheme at time of rezoning.

3.2 DESIGN GUIDELINES FOR STREETS

3.2.1 Roads should be throated down (with curb extensions) at intersections where possible to calm traffic.

3.2.2 Any Roundabouts and/or traffic circles in the through local roadway will define key character intersections and contribute to one's sense of orientation a progression within the neighbourhood.

3.2.3 The size and configuration of the roundabout, traffic circles, traffic calming throats, sidewalks and location of parking will be determined at the detailed design stage when the layouts of lots and driveways are finalized.

3.2.4 Street lighting will be installed similar in architectural character to the residential development and will be to Engineering Department standards.

3.2.5 On street parking will be in parking bays where possible.

3.3 DESIGN GUIDELINES FOR STREET TREES

Objective

3.3.1 Trees are a major landscape element that makes the development special, unified and softens the built form.

Street Tree Design Strategies

3.3.2 There will be a specific order as to type and ultimate height and scale of trees to correspond to specific street boulevard functions and abutting residential housing scale.

- 3.3.3 Boulevard tree planting will be developed through consultation with the Parks and Recreation Department in terms of species selection and ultimate height, caliper installation sizing, color and flowering, planting patterns and spacing, installation detailing and maintenance requirements, all to meet boulevard tree planting standards.
- 3.3.4 Trees will not be planted within 0.6 m of curbs.
- 3.3.5 Tree planting on front yards will be coordinated with boulevard tree locations to avoid conflicts and to provide suitable front door viewing.
- 3.3.6 One front lawn tree will be planted on single-family lots with a frontage less than 20.0 m and will be included within the tree replacement plan required for the Fleetwood Enclave.

3.4 DESIGN GUIDELINES FOR SINGLE FAMILY DETACHED HOUSING

Objective

- 3.4.1 While the public realm of streets will begin to establish Southwest Fleetwood's urban character, it is the private realm of defined yards, terraces, porches and residential buildings of various types that will complete the street enclosures and vistas and create a strong street edge. Housing will be street oriented and porches will be encouraged.

Single Family Detached Housing Strategies

- 3.4.2 While the developer will establish design guidelines for the various housing types and locations, the theme will be a variety of architecturally coordinated yet differentiated buildings.
- 3.4.3 Special consideration will be given to homes adjoining intersections, roundabout and traffic circles, open space areas and transitions from lower density single family homes urban density single family homes, and the central wildlife corridor.
- 3.4.4 Architectural detailing referring to material selection, application of accenting elements, wall face applied versus set in windows, etc., will be considered as part of the builder's guidelines for the purpose of establishing massing, shadows, depths and most importantly building value.
- 3.4.5 Considering the material development of the boulevards, the low bordering landscape fence and contributing landscaping, the foremost element of the single family home will be the front door element for scale and expression of style.
- 3.4.6 The second element in the evolution of the Southwest Fleetwood Enclave home will be the lowering of the second floor roof to integrate with entry porches an arbour for a more human scale versus a conventional two-storey wall with buildouts.

- 3.4.7 The residential built character will demonstrate a ‘hand crafted’ approach featuring such elements as entrance doors, windows, and corners and employing smaller scale elements as bricks and shingles.
- 3.4.8 The design, composition and materials for the front entry are to be consistent with the overall style of the home. Limited use of cultured stone as an accent material may be permitted.
- 3.4.9 Garages with doors in a compatible color and style to the house are required.
- 3.4.10 Garages must be set back from the front face of the dwelling and may be lowered or otherwise subordinated to the front entry.
- 3.4.11 In order to mitigate the impact of the garage door to the street, the garage should not exceed 50% of the width of the house front elevation. On corner lots, the garage driveway should be provided from the secondary road where practical.
- 3.4.12 Walls may be clad in a variety of materials including vinyl subject to individual dwelling approval, location, color choice and detailing. Specific provisions to prevent unbroken two-storey sheer walls, overuse of vinyl and inappropriate color choices will be provided.
- 3.4.13 Colors of wall materials and trim must be selected from a color and repetition palette supplied by the developer or his agent. Basic white will not be permitted and off-white as a body color will be permitted on a limited basis.
- 3.4.14 Rooflines, materials and colors, soffit details, gutters and fascia trim will be controlled by the building scheme to ensure continuity.
- 3.4.15 Front yards will be fully landscaped at the time of building completion and prior to occupancy to ensure an appropriately finished street appearance and for street cleaning purposes.
- 3.4.16 Driveways will be paired wherever possible with landscape finishing between driveways and continuity of sidewalks will be maintained.

Fences

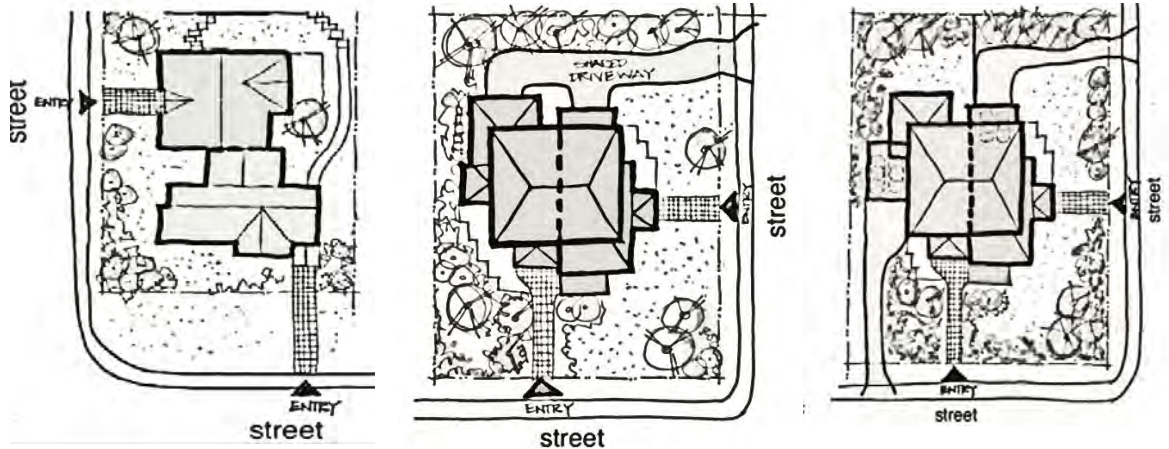
- 3.4.17 Solid wood materials are required in a style and stain finish for overall style consistency.
- 3.4.18 Low, landscape style open or transparent fencing only is permitted within the projected side yard of front yard areas as an alternative to hedging or on the soft landscaping material to a maximum height of 0.60 m.
- 3.4.19 Traditional solid fencing (to Zoning Bylaw standards) will be required on the flanking side of corner lots for rear yard privacy and streetscape control. In any event, such fencing may only extend to cover the rear portion of the flanking

street and must be augmented with landscaping along road and fence. Flanking fences should be setback a minimum of 0.6 m to allow for additional landscaping.

3.4.20

3.5 DESIGN GUIDELINES FOR SINGLE FAMILY DUPLEX HOUSING

3.5.1 Duplex Dwellings on corner lots should be designed to “face” both streets, i.e. provide an entrance to one unit from the primary street, with the entrance to the second unit from the flanking street.



Drawing 1 - Corner Lot with entrances facing both streets

3.6 DESIGN GUIDELINES FOR DEVELOPMENT ABUTTING PARKS & OPEN SPACE

Objectives

3.6.1 The objective is to encourage an open, integrated public interface while respecting the transition between public and private realms.

Design Strategies

3.6.2 Specific architectural guidelines for the single family lots will be developed during the rezoning process, that will specify open space preservation and single loaded fronting lots facing the green open space areas and parkland, and encouraging balconies overlooking parks. Finished floor elevations will be established 0.6 to 0.8 m above the park elevation where possible.

3.7 DESIGN GUIDELINES FOR BUILDINGS

3.7.1 The site layout and design of the building and open space should be based upon the principles of defensible space and provide ample opportunities for casual surveillance of public spaces (CPTED).

3.7.2 Site planning and building design should be responsive to the contours, vegetation and natural features of the site, and the specific conditions of the site (i.e., surrounding uses, views, noise, slopes, etc.)

- 3.7.3 The building materials and colours will be determined at time of development; however, they should be high quality and represent aesthetics of the area.
- 3.7.4 The entrance to the building should consider the use of architectural or landscaping elements that identify the threshold between public and private property. Any minor structure used for this purpose should be located at the dominant front yard setback line.
- 3.7.5 Consideration should be given to the siting and massing of the buildings in such a way that they ‘anchor’ the intersection, site, corners, edges and act as a gateway into the neighbourhood.
- 3.7.6 The articulation of the roofs should reflect and be similar to the adjacent residential buildings.

3.8 DESIGN GUIDELINES FOR PARK AREAS

Park and Open Space Design Policy

- 3.8.1 The City of Surrey Parks, Recreation and Culture Department will be responsible for all design and development within open space and buffer area designated to parks. Details will be finalized at the time of development.

Park Design Strategies

- 3.8.2 The nature park open ‘Green Space’ areas along Fleetwood Park and Golf course Lands will be maintained as a natural woodland habitat with opportunities for low impact recreational pathway component along the eastern edge of 157 and 154 street.
- 3.8.3 The Habitat Corridor spanning the Infill Area will be dedicated and/or purchased when exceeding the parkland dedication requirements with infill areas parkland cash in lieu payments, and maintained as linear parkland with significant tree and vegetation species maintained, or planting enhancement will be provided as determined by Surrey Parks, Recreation and Culture Department.
- 3.8.4 Engineered storm water retention or bio-filtration areas will not be allowed in the treed open space or wildlife Corridor areas, but may be permitted in curb buldges.
- 3.8.5 A 2.0m pathway will meander through the Habitat Corridor area to minimize impact on existing natural environment and on the adjacent residential lots.
- 3.8.6 Permanent hard fencing will be erected at the private property lines along parks by the developer as soon as possible in the development process.
- 3.8.7 Existing tree stands in all park areas will be retained where appropriate.
- 3.8.8 Special park amenities and/or structures may be determined by Surrey consultation with the community.

Part 4: Implementation

4.1 OFFICIAL COMMUNITY PLAN AMENDMENTS

The entire area covered by the Fleetwood Enclave Infill Area Plan is currently designated Suburban in the OCP. In Suburban Designated Areas with full services and where a Neighbourhood Plan is in place, the Surrey OCP indicates, in Section 3.6, that a minimum ½ acre lot size, including gross density, where applicable is permitted. This may be increased to 10 units per hectare (4 units per acre) where development with this density is a minimum of 200 metres from the edge of the Agricultural Land Reserve.

Amendments to the OCP will therefore be required for lots within 200 meters of the ALR, where densities of 4 units per acre or greater are proposed and all lots outside 200 m where densities of 5 units per acre are proposed. Amendments will likely take place in phases as owners wish to proceed to re-designate lands for the appropriate “Urban” Residential densities and uses.

4.2 REZONING

The residential lands will need to be rezoned before development can proceed. Rezoning will be completed in a logical staged manner. Areas suitable for development may be rezoned when owners make application consistent with this Infill Area plan.

4.3 SUBDIVISION

Future subdivision will be consistent with both the Infill Area plan and the ultimate zoning. As noted in the section on phasing, subdivision will be dependent upon market conditions and at a pace determined by the landowners. Detailed subdivision patterns will be determined at the subdivision application stage.

4.3.1 Dedication of Parkland

The *Local Government Act (LGA)* requires the dedication to the City of Surrey without compensation of up to five per cent of land being subdivided as park land, unless the subdivision creates fewer than three additional lots (except if lot being subdivided was created within the last five years), creates lots each of which is larger than two hectares, or is merely consolidating existing parcels.

The City of Surrey Parks, Recreation and Culture Department will determine the location and amount of land being provided under s. 941 of the *LGA*, under guidance of this Infill area Plan.

Lots with special characteristics, such as mature vegetation, watercourses, or other features worthy of preservation and/as indicated in this Infill Plan that are designated for Cluster Housing, may require substantial public open space be set

aside within the subdivision to achieve gross densities calculated on the basis of the entire lot (Including undevelopable areas).

4.3.2 Money in Lieu of Parkland Dedication

In some cases, the *Local Government Act* permits the payment of money in lieu of dedicating park land, at the owners option, except where the City of Surrey OCP and/or other Plans and policies, (such as the Infill Area Plan) and designations respecting the location and type of future parks, in which case Surrey Parks, Recreation and Culture may dictate whether land is dedicated or money paid, as per the Fleetwood Enclave Infill Plan park dedication areas.

4.3.3 Parkland Allocation Process

Parkland must be dedicated or the money paid in lieu of before the subdivision is approved, unless a park land agreement is entered into between the City of Surrey and the subdivision applicant. Such an agreement must require the dedication of the land or the payment of the money by a specific date, and notice of the agreement must be filed in the Land Title Office in the same manner as a permit under Part 26 of the *LGA*. The agreement is binding on the applicant's successors in title.

4.4 DESIGN GUIDELINES

The Infill Area Plan contains design guidelines for land uses that are intended to provide general direction to achieve the desired neighbourhood character, preserve and enhance natural space, encourage pedestrian access to destination areas, and achieve the overall development objectives defined in the final Infill Area Development Concept Plan.

The design guidelines make recommendations regarding the interface between residential areas and the public street, residential areas and agricultural lands, viewsapes, green space management areas, transition areas, stormwater corridors and on-site drainage works, as well as architectural elements appropriate for residential buildings.

These guidelines will be used by City staff to guide the developers in coordinating the design among individual rezoning/development applications and to ensure that the desired neighbourhood character is achieved in the Fleetwood Enclave. The Design Guidelines will be implemented through Building Schemes for single family developments, which will be registered on the lots and administered by design consultants hired by the developers and approved by the City

4.5 COMMUNITY SERVICES AND AMENITY CONTRIBUTIONS

To address the amenity needs of the proposed new development in the Fleetwood Enclave, all development proposals at the time of rezoning or building permit issuance will be required to make a monetary contribution toward the provision of new police,

fire protection and library services and toward the development of the parks, open spaces and pathways.

The monetary contributions toward police, fire and library materials will offset the capital costs of providing these services to the new development and are applied on a standardized basis in all of Surrey's Neighbourhood Concept Plan or Infill Areas. The monetary contributions toward parks, open spaces and pathway development are based upon an estimate of the capital costs of these improvements for this particular Infill area. The total cost is divided by the anticipated number of dwelling units and acreages in the case of non-residential development to ensure an equitable contribution arrangement.

4.5.1 Parkland Development

The Fleetwood Enclave community will contain no new active park sites, but it will contain several linear parks, and addition to Fleetwood Park as natural area, multiuse pathways, trails and an east west habitat corridor. The new Natural areas will include the areas adjacent to the Agricultural Land Reserve, Fleetwood Park and the Eaglequest golf course, as well as lands within the Hydro Corridor.

The estimated cost of developing parks trails and related park amenities in the future Enclave is approximately \$258,326. This results in a contribution of \$1, 1195.95 (in 2013 dollars) per dwelling unit.

4.5.2 Library and Library Material

A study of library requirements in Surrey's new neighbourhoods has established that a contribution of \$ 142.98 (in 2013 dollars) per dwelling unit is necessary to cover the capital costs for library materials and services, which is sensitive to population growth. Consequently, a total of approximately \$30,883.68 will be collected from Fleetwood Enclave towards materials such as books, computers, DVDs and CDs.

4.5.3 Fire and Police Protection

Future development in this neighbourhood will drive the need to upgrade existing fire and police protection facilities. A study of fire protection requirements in Surrey's new neighbourhoods has established that a contribution of \$ 274.54 per dwelling (in 2013 dollars) will cover the capital costs for fire protection. A contribution of \$ 63.56 per dwelling unit (in 2013 dollars) will cover the capital costs for police protection. This will result in a total capital contribution from the Fleetwood Enclave of approximately \$59,300.64 toward fire protection and \$13,728.96 toward police protection.

4.5.4 Summary of Amenity Funding Arrangements

A summary of the applicable amenity contributions (per dwelling unit) and the estimated revenue the City can expect to receive from the Fleetwood Infill Area is documented in **Table 12**.

The per unit amenity contributions are derived from estimated base densities in the residential designations and the number of dwelling units (excluding any secondary suites) anticipated.

The estimated costs of the various amenities are distributed evenly to each dwelling unit. Therefore, if the number of dwelling units in a proposed development is lower than that anticipated by the Infill Area Plan, the applicant will be expected to "top up" the amenity fees based on the number of the dwelling units used to calculate the amenity charge to ensure that there is no shortfall in the funding for the proposed amenity.

FLEETWOOD ENCLAVE INFILL AREA PLAN		
AMENITY CONTRIBUTIONS		
Amenity Type	Per Dwelling Unit Contribution (Approx. 216 DUs)	Anticipated Revenue
Police Protection	\$63.56 per dwelling	\$13,728.96
Fire Protection	\$274.54 per dwelling	\$59,300.64
Development of Park/Pathways and Place making Features	\$1,1195.95 per dwelling	\$258,326.00
Library Materials	\$ 142.98 per dwelling	\$30,488.40
Total Contribution (per unit)	\$1,612.90 per dwelling	
Total Anticipated Revenue		\$348,386.40

Table 12 - Amenity Contribution Summary

4.5.5 Zoning Bylaw Amendment (Amenity Fees – Schedule G)

To enact the amenity contribution requirements, the Zoning By-law requires an amendment to add the Fleetwood Enclave to the list of Infill Areas within which monetary contributions are required. The proposed amendments to *Schedule F and G of the Zoning By-law*, to incorporate the amenity fees for Fleetwood Enclave, are proposed concurrently with Council approval of this Infill Area Plan.

4.6 COST RECOVERY OF INFILL AREA PREPARATION

An Environmental, Engineering Servicing and Transportation Consultant were retained to assist with the preparation of the Fleetwood Enclave Infill Area Plan. The cost of the consultant services to the City was:

Environmental Study	\$15,680.00
Drainage Study	\$37,649.22
<u>Transportation/ Traffic Study</u>	<u>\$32,666.70</u>
TOTAL:	\$86,243.62

In order to recover the Infill Plan preparation costs through the payment of application surcharge fees, the Fee Imposition By-law will be amended concurrently with Council approval of this Infill Area Plan.

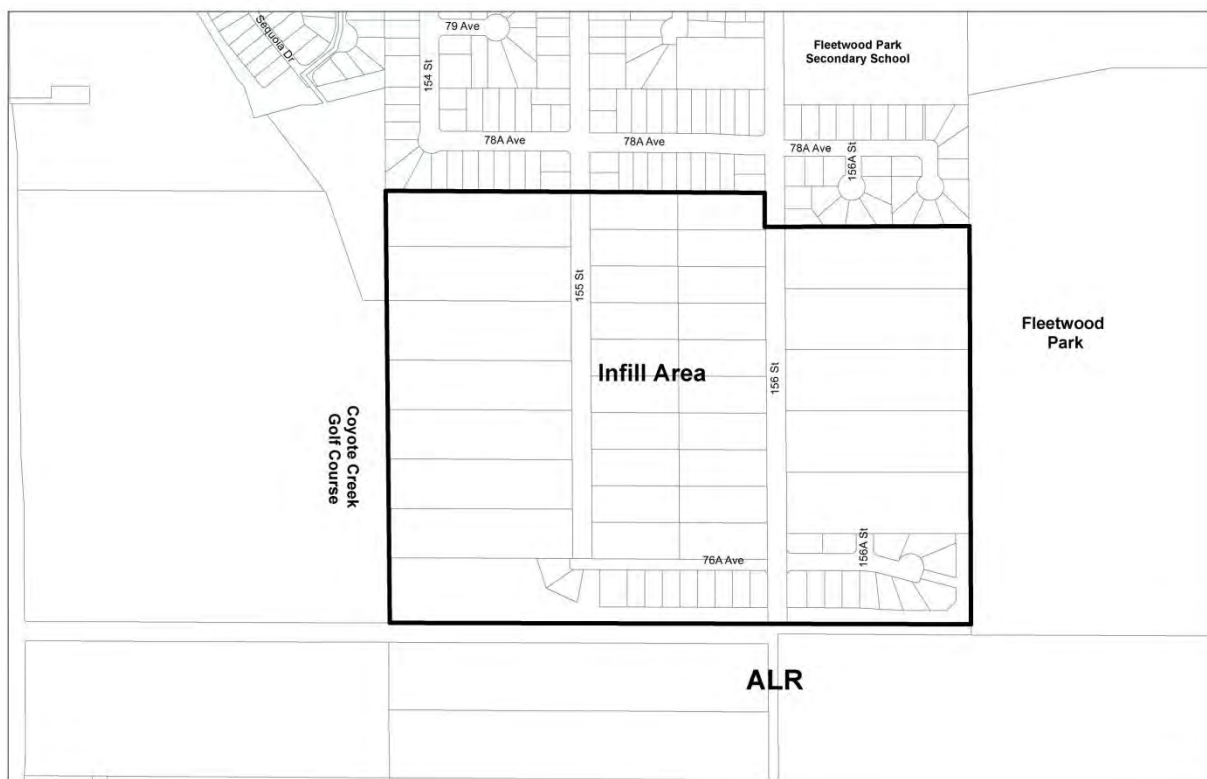
The surcharge fee per unit is based on the anticipated 216 units, and would result in a per unit fee of \$399.27. Should the actual number of proposed units fall below the number anticipated on site; the applicant will be required to make up the shortfall in the surcharge fee to ensure the Infill Plan costs are fully recovered.

Proposed Amendment to Schedule F and G of the Zoning By-law

**Proposed Amendments to
Surrey Zoning By-law, 1993, No. 12000, as amended**

The following amendments are proposed to Surrey Zoning By-law, 1993, No. 12000, as amended:

- Schedule F – Map of Neighbourhood Concept Plan and Infill Areas is amended by inserting Map 28 - Area XXVIII for Fleetwood Enclave, as follows:



- Schedule G - Amenity Requirements in Neighbourhood Concept Plan (NCP) and Infill Areas is amended by inserting a new Item 28 after Item 27, as follows:

NCP and Infill Areas		Amenity	Contributions Per Dwelling Unit	Contributions For All Other Land Uses
28.	Area XXVIII on Schedule F of this By-law	Parks and Pathways Development	\$1,195.95	N/A
		Library materials	\$142.98	N/A
		Fire Protection	\$274.54	N/A
		Police Protection	\$63.56	N/A
		<i>Total Amenity Contributions</i>	\$1,677.03	N/A

**Proposed Amendments to
Surrey Land Use and Development Application Fees Imposition By-law, 1993, No. 11631**

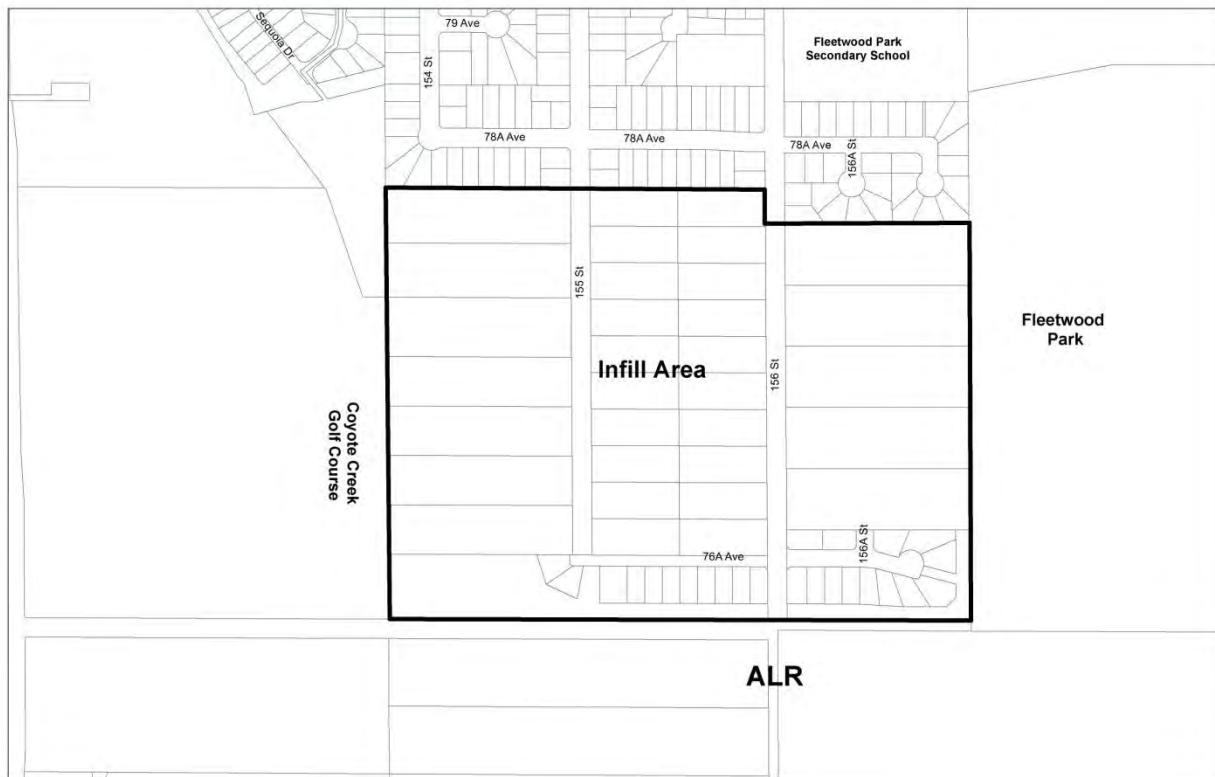
The following amendments are proposed to Surrey Land Use and Development Application Fees Imposition By-law, 1993, No. 11631:

1. Amend Schedule Eight (8) - Application Surcharge Fees within Neighbourhood Concept Plan Areas and Infill Areas, as follows:

- (a) Insert the following in the table, after item number 12:

(13) Fleetwood Enclave Infill Area Plan as shown on Map 13	\$399.27 per lot or dwelling unit (or equivalent) calculated on the maximum number of lots or dwelling units allowable on the site after the zoning amendment is granted.
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- (b) Insert the following figure as Map No. 13, following Map No. 12 and insert "Map No. 13 Fleetwood Enclave Infill Area Plan Lands Subject to Surcharge" as the title at the bottom of the left-hand corner of Map No. 13.



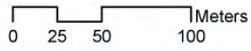


PLANNING AND DEVELOPMENT DEPARTMENT

**FLEETWOOD ENCLAVE
LAND USE CONCEPT PLAN**

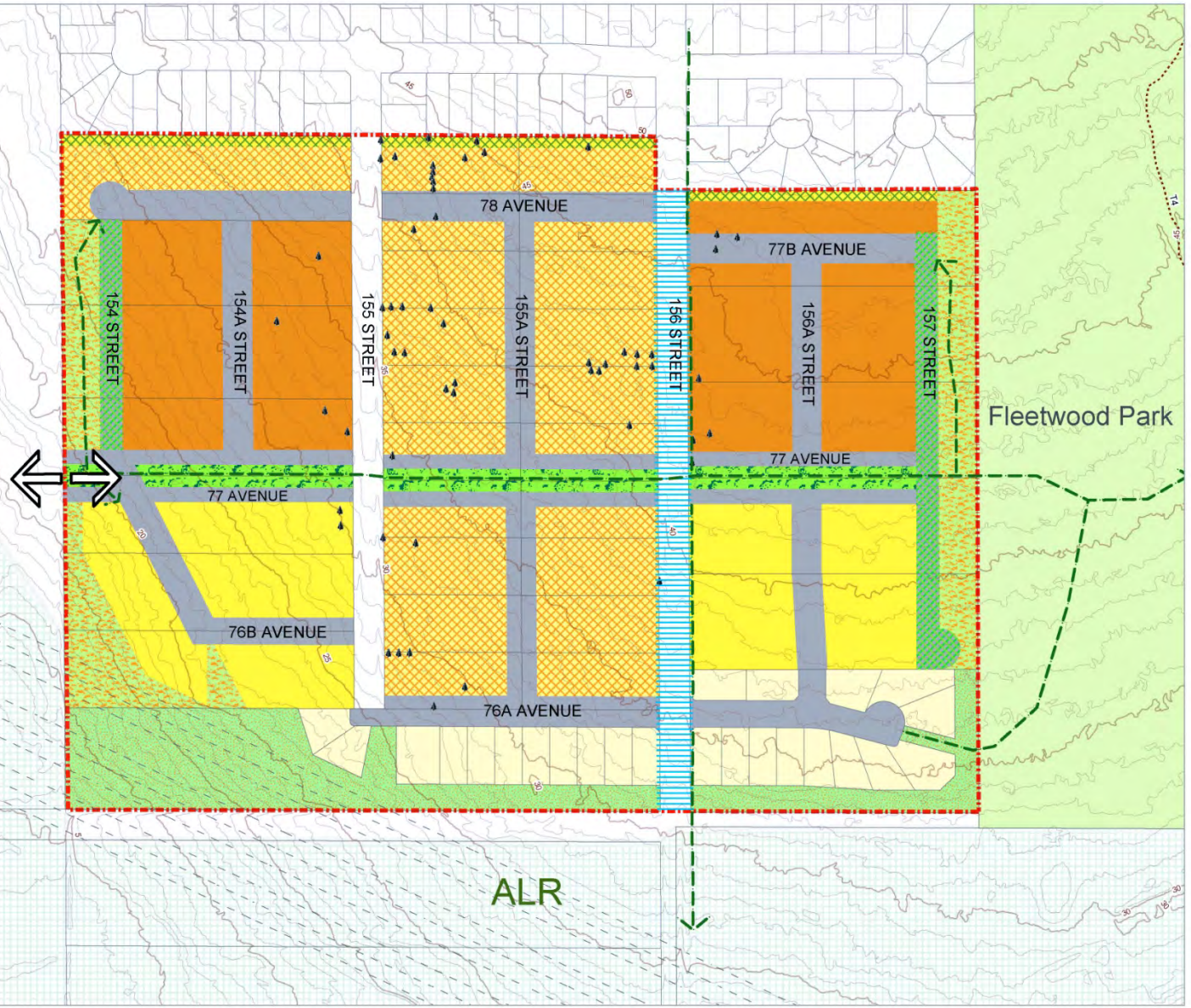
Legend

-  FLEETWOOD ENCLAVE BOUNDARY
-  EXISTING LOTS
-  PROPOSED COLLECTOR ROAD
-  PROPOSED GREEN ROAD (SPECIAL STANDARD)
-  PROPOSED LOCAL ROAD
-  EXISTING SUBURBAN CLUSTER 2 U.P.A.
-  1/4 ACRE GROSS DENSITY 4 U.P.A.
-  LOW DENSITY CLUSTER 4 U.P.A.
-  LOW-MED DENSITY CLUSTER 5 U.P.A.
-  EXISTING PARKLAND BUFFER
-  PRIVATE LANDSCAPE BUFFER
-  HABITAT CORRIDOR PARK/TRAIL
-  OPEN SPACE / FUTURE PARK
-  PROPOSED GREENWAYS/TRAILS
-  FLEETWOOD CITY PARK
-  POTENTIAL SIGNIFICANT TREE
-  Agricultural Land Reserve
-  EXISTING PARK TRAIL
-  HYDRO TRANSMISSION LINES
-  FUTURE CONNECTION (To 152 St and 76 Ave to be required in the future if and when adjacent "Golf Course" area is redeveloped)



1:2,500

Date: 1-31-13



FLEETWOOD ENCLAVE: PUBLIC FEEDBACK/COMMENT SUMMARY

April 30, 2012 Land Owners Meeting

The formal land owners meeting was held prior to the first public open house at Fleetwood Park Secondary School. The presentation and display materials reviewed the Infill Area boundaries, introduce the City's planning team, reviewed the key environmental study findings, and discussed key opportunities and challenges, and how the planning process will take place.

Time was allotted to allow initial comments and suggestions about development in the area to provide input into the preparation of the Development Concept Plan and proposals for the future of the area. The overall Land Owners reaction was positive with some concern regarding density ranges and green space allocation. Approximately Twenty Five (25) Land Owner attended the meeting with a total of six (6) Comment Sheets received by the City, with specific issues identified, including:

- concerns by some residents regarding the lobbying of Council by developers for higher density without (adequate public benefit and community amenities);
- support for leaving a green corridor between Fleetwood and Surrey Lake Park;
- suggestions that developers respect existing significant trees, natural features and wildlife habitat corridors identified in the Environmental Study findings;
- some suggestions in support of 6000 square foot lot minimums to maximize land yields
- discussion regarding maximizing viewscape opportunities in and out of the area;
- some suggestions for lots with Coach houses served by lanes, be available for families;
- some suggestions that RF12 and RF9 style lots would be too dense and that RF lots would be ideal;
- some suggestions that density be similar to those lots approved to the south and not to include large acreage style lots;
- concerns that the character of the Community including proximity and rural ambiance of Parks and Golf Course setting be maintained were feasible.

May 15, 2012 Public Open House

All property owners in the Infill area, as well as land owners north and south of plan area up to 80 Avenue were directly notified by mail, with a Newspaper add advertising the Public Open House published in the Surrey Leader.

Approximately 88 people attended the meeting. In addition to a sign-in sheet, an exit questionnaire was provided to obtain comments on issues to be addressed through the Infill area process. Twenty-one (21) Comment sheets were received by the Planning and Development Department. City of Surrey Staff made a short presentation at 7:00 p.m. which was followed by a question and answer period.

These open house sessions attended by City of Surrey staff from Planning and Development, Parks and Recreation, Land Development, and Transportation Divisions. The overall Land Owners reaction was positive to the initiation of the plan. Specific issues identified included:

- the development of Firm Guidelines on Tree Preservation;
- traffic concerns and road network improvements needed adjacent to Fleetwood Park Secondary School;
- additional restrictions or accounting for illegal and legal Secondary Suites and related traffic and parking issues;
- limit development to Single Family style homes (No Townhouses);
- consider smaller houses on larger lots to preserve green space;
- consider affordability of houses for retirees and young families;
- consider a green belt along Golf Course and Fleetwood Park;
- consider traffic along 156 street and 80 Avenue next to School;
- respect existing urban residents to the North;
- improve traffic flows through 80th Avenue and 156 Street;
- suggestions that the Infill Area incorporate improved bicycle and walking paths in and out of Fleetwood Park and Surrey Lake Park to enhance pedestrian circulation;

January 23, 2013 Public Open House

All property owners in the Infill area, as well as land owners north and south of plan area up to 80 Ave were directly notified by mail, with a Newspaper add advertising the Public Open House published in the Surrey Leader. A notification email was also sent to all residents, land owners or interested parties who wished to be contacted after the first Public open house. Approximately 70 people attended the meeting. In addition to a sign-in sheet, an exit questionnaire was provided to obtain comments on issues to be addressed through the Infill area process.

These open house sessions attended by City of Surrey staff from Planning and Development, Parks and Recreation, Land Development, and Transportation Divisions, as well as a Transportation Consultant from Buntt and Associates Engineering Ltd.

A total of twenty four (24) Comment Sheets received by the City. The overall response to the proposed Land Use concept was fairly positive with some general questions and suggestions for minor changes.

Many landowners would like to see greater densities, fewer roads and less green space percentages for parkland and habitat corridors. **Landowners in the Fleetwood Enclave** comments are summarized below:

- Consider ways to reduce road construction costs;
- There are problems with landscape buffers next to existing residential areas;
- Opportunities to provide equity for land development across plan area;
- Consider ways to include safety into design of the plan;
- Road network does not make sense as it reduces green space, and may increase criminal activity, so cul-de-sacs should be considered;
- NO maximizing lot yields and 13% green space is too high for parkland dedication, while buffering is not need on east and west side of plan area;
- 4 Units per acre is to low and park dedication ratio should be lowered to 5%;
- Smaller lots does make sense for future growth;

- I am long-time owner in Fleetwood and I like the medium size lots and good neighbourhood, no need for fancy open space etc.
- My Concerns are the development costs for the area;
- Smaller lots does make sense for future growth;
- Need Higher density There should be 5 Units per acre on all lots above 77 Avenue for consistency;
- Fleetwood Park and Surrey Lake park are in close proximity, no need for additional park and open space land;
- Proposed Fleetwood park expansion is not fair, as this park is 120 acres already.
- Park land proposed next to golf course is not good, as the golf course will be developed in the future.
- Planning has done a good job, and density is O.K., but we like to see some compromise to make work for land owners;
- Fleetwood Park trail connections is O.K.
- Green road next to Fleetwood park and Golf course is extra cost to service and maintain;
- Everything makes sense except park/trails as very few people are going to use them;
- Grid network proposed should not be used for this area, and should be similar to the cul-de-sac neighbourhood existing to the north;
- Lost my Privacy after 40 Years;
- Consolidation areas should be reconsidered, as my property cannot develop on my own:
- New development should be similar to approved development plan to the south.

Residents from outside the study area are typically concerned with green space preservation, tree protection, landscape buffers, traffic around schools and too many homes. Specific issues identified included:

- Opportunities to preserve diverse wildlife in the area should be taken into account;
- Consideration for housing density and amount of secondary suites should be considered when approving development plans;
- Ensure future trails do not encourage other non-designated trails;
- The buffering and expansion of Fleetwood park is considered essential;
- Trail along 77 Avenue is an excellent idea;
- Smaller lots does make sense for future growth;
- Find ways to reduce road construction costs;
- Cost and landscape buffers next to existing residential areas;
- Opportunities to provide equity for land development across plan area;
- Opportunities to include safety into design of the plan;
- Main concern is the limited access to the area which culminates at 156 street and 80 Avenue next to Fleetwood Park Secondary school;
- Lots of big trees to preserve;
- Ensure quality infrastructure such as sidewalks and drainage facilities;
- Delay paving due to ongoing construction activities.
- Lot sizes of 4000-6000 sq feet are smaller to the north and south, and should be the same size to reduce the amount of houses. Consider reduced densities.