

Based on staff's recommendation, Council referred the project back to staff to work with the applicant to develop a more urban and pedestrian-friendly development, which better responded to the location of the Guildford Town Centre Mall as the heart of the Guildford Town Centre, in accordance with the design strategies listed in the report.

On December 18, 2006, a second Planning Report related to the subject project was submitted to Council for consideration, which in addition to a partial discharge of Land Use Contract No. 365 and a Development Permit included a Development Variance Permit to vary the building setback along the west property line (150 Street) from 7.5 metres (25 feet) to 7.0 metres (23 feet). The applicant had modified their proposal to include the following:

- The redevelopment of the Guildford Town Centre Mall based on a general development concept; and
- The construction of a new 18,808-square metre (202,451 square foot) Wal-Mart store with a grocery component and the associated parking facilities forming part of proposed Phase I.

The project included a two-level mall expansion due south of the existing mall, the addition of a new food court to the shopping centre, a "nature walk" and "terrace steps" as part of an improved pedestrian connection through and within the site, 2 two-level parkades with one level constructed underground, improved loading areas, additional retail opportunities along 152 Street, a facelift to the existing exterior facades and the renovation and updating of the interior spaces of the existing mall to match the new addition. Council approved the application to proceed to a Public Hearing related to the LUC discharge.

On January 15, 2007 the related Public Hearing was held and subsequent to that Hearing, Council granted Third Reading to By-law No. 16221, to allow for the partial discharge of Land Use Contract No. 365 and approved the Development Variance Permit for the variance of the building setback along 150 Street.

Due to the economy the applicant did not proceed with the project at that time. The applicant has now decided to reactivate the project but with some modifications.

DISCUSSION

Site Boundary

The site that was the subject of the application presented to Council in 2006 encompasses 14 lots of the Guildford Town Centre Mall site bounded by 105 Avenue, 101 Avenue, 150 Street and 152 Street.

Three lots at the southerly end of the site, near 101 Avenue, addressed as 10160-150 Street and 10191 and 10181-152 Street are currently regulated by Land Use Contract No. 365. Land Use Contract No. 365 is being discharged from these lots as part of the application and as a result the underlying C-8 Zone will come into effect, which is the same zone as the remaining 11 lots within the subject site.

Current Application

On September 16, 2009, a revised application was submitted, which changed the scope of the original application. The construction of a new Wal-Mart is still proposed and includes a grocery supermarket and a garden centre.

The key differences between the original proposal which proceeded to Council in 2006 and the current proposal are as follows:

- The most significant change is a decrease in floor area of the Wal-Mart store by over 5,000 square metres (54,000 square feet) to a proposed floor area of 15,329 square metres (165,000 square feet).
- The Wal-Mart store has been shifted further south and the vehicular access from 150 Street has been eliminated in response to traffic flow studies conducted by the developer's consultant. These studies supported the elimination of the below grade vehicular access south of Wal-Mart which previously resulted in CPTED issues;
- The floor elevation of the Wal-Mart store has been raised to match the existing floor elevation of the existing lower mall level to eliminate grading difficulties and to improve accessibility;
- The access point to the parking deck has been moved further to the west. The associated new on grade ramp creates a new landscape berm into the site and connects to the landscape buffer along 150 Street;
- There is only one entrance into the Wal-Mart store;
- The grocery store component has been relocated to the south side of the Wal-Mart component and the mechanical equipment has been shifted in location; and
- One less loading bay is proposed at the rear of the store, which allows for a north orientation of all bays and the access point for the truck loading ramp has been shifted further to the north.

The revised development application was forwarded to the Advisory Design Panel (ADP) on October 15, 2009 and received general support, subject to some design revisions.

The applicant has further modified the project as follows (see the attached Appendix I):

- The Phase I boundary has been changed to exclude a parking deck west of the Phase II retail expansion;
- The sidewalk along the north side of the lower level has been widened by slightly shifting the parking grid to the south;
- The Wal-Mart pavilion has been redesigned using a pyramid profile to reflect a better relationship to the existing building;
- Light wells have been introduced on the south side of the parking to allow day light penetration, as recommended by the ADP;
- The sidewalks on each side of the main vehicular entrance off of 152 Street will be enhanced. The ADP had recommended that the sidewalks be of equal dimensions;
- Further design has been incorporated along the east/west pedestrian connection along the south elevation of Wal-Mart and along the south elevation of the parking deck from 150 Street to 152 Street;
- The treatment along the west facade of the mall, along 150 Street, has been revised to provide vertical landscape elements and tree textured motifs within the concrete walls; and

- The upper deck is to be regraded at the west entrance and along the sidewalk to the east of the entrance to satisfy drainage issues and to have a better relationship with the upper level of the retail floor.

A separate Development Permit application for Phase II, Application No. 7909-0174-00, has been submitted by the applicant, to allow for the following:

- Expansion of the upper and lower retail components on the southerly portion of the site along 152 Street;
- Facade renovations to the existing building, except for the Bay and London Drugs;
- Modifications to the existing entries to the mall;
- Existing free-standing canopies will be demolished and will be replaced with new entry canopies;
- Surface parking will be restriped to maximize parking layout and landscaped to include surface lighting; and
- Demolition of the building west of Sears.

Phase II will also include the reorientation of parking, which is currently within road dedication areas but which will remain as part of the Phase I project to support existing tenant agreements. The developer proposes to have the new parking in place at the end of Phase II, when all improvements have been completed to the mall. A separate Planning report will be forwarded to Council when the applicant is ready to proceed with the Phase II application.

Lot Consolidation and Road Dedication

Under the current Phase I application eight of the 14 lots within the overall site will be consolidated (Appendix II). The three lots at the southeast corner of 150 Street and 104 Avenue will remain as separate lots, along with the three lots north of 104 Avenue.

The lot consolidation will facilitate road dedication along 150 Street, 104 Avenue and 152 Street. The remaining parcels not being consolidated will be subject to the registration of rights-of-way to secure future road widening.

Since a revised application has been submitted, new Engineering requirements were generated for the revised proposal (Appendix III).

Engineering staff has confirmed with TransLink that a possible 39.0-metre (128 foot) wide road right-of-way will be required along 104 Avenue and along 152 Street to facilitate the implementation of Light Rapid Transit. This requires a 3.952-metre (13 foot) road dedication along the 104 Avenue and 152 Street frontages of the site that is the subject of the current application, which is an increase from 1.455 metres (4.5 feet) previously stipulated along the south side of 104 Avenue and the west side of 152 Street under the earlier development proposal.

Circulation, Access and Parking

The main vehicular access points to the mall have not changed significantly. There are currently two access points from 150 Street, one from 101 Avenue, one from 104 Avenue and three access points from 152 Street. The main access point at 152 Street and 102 Avenue will be

enhanced with landscaping and a median to provide a more formal entrance to the mall. The northerly access points from 150 Street and 152 Street are to be shifted further north; however, this will not occur until the Phase II component of the project proceeds. The current most northerly vehicular access point along 150 Street must be shifted further north to accommodate two loading bays for the development. These new loading bays are to be screened with mesh panels and climbing vines and hedging.

Median along 150 Street

An existing vehicular access off 150 Street is being relocated further north to accommodate the new location of a loading bay for Wal-Mart. As a result, Engineering staff has requested that a median be constructed along 150 Street to restrict access to right in/right out at this driveway. This median will also restrict traffic movements to right in/right out on 150 Street for those commercial sites located along the west side of 150 Street, south of 104 Avenue.

The developer held a meeting at Guildford Town Centre Mall on April 21, 2010, with Surrey Planning and Development Department and Engineering Department staff in attendance, to present information about this proposed median to the owners of the businesses on the west side of 150 Street that will be affected by its installation. Only three business owners attended the meeting. Generally, the sentiment was that the mall renovations were supportable, welcomed and much-needed for the community. The property owners understood that the median construction would only formalize the existing traffic movement currently permitted along 150 Street, south of 104 Avenue.

Multi-use Walkway

The applicant has agreed to provide a 2.5-metre (8 foot) wide multi-use walkway along 150 Street, which will allow for a pedestrian connection from Guildford Town Centre Mall to the Guildford Recreation Centre and Library at 150 Street and 105 Avenue.

Inclusion of Future Transit Exchange

Ultimately, a Transit Exchange will be required in the Guildford Town Centre area, north of 104 Avenue, which will be located within the area covered by Phase II component of the project.

TransLink has not provided any details at this time regarding the design of such a facility. City Engineering staff are involved in continuing discussions with TransLink.

TownShift Competition

On November 2, 2009, the City of Surrey announced a competition to generate a future vision and design ideas for the five Town Centre areas within Surrey. The site selected as the focus of the competition in Guildford consisted of lands on the west side of 152 Street at 104 Avenue, which are in part included in the Phase II component of the redevelopment of the mall. The overall winning submission for the Town Shift competition was an entry by Mr. Renate Solivar, MAIBC, related to the Guildford site. Coincidentally, Mr. Solivar is an architect with Musson Cattell Mackey Partnership, the architectural firm for the subject application.

The TownShift design brief for Guildford presented a challenge to temper and humanize the busy corner of 104 Avenue and 152 Street, to introduce place-making and to provide a sense of arrival to Guildford. Most of the entries that were received focused on enlivening 152 Street as the "Main Street" of Guildford. The winning submission as referenced above developed a building that engaged the street while addressing the mall, with a sophisticated scheme that included "permeable" retail at the ground floor along 152 Street and a mix of uses above including residential all housed in a bold building and colourful forms.

Given that the Guildford TownShift site is situated partially within the boundary of the Phase II component of the Guildford Town Centre redevelopment, Mr. Solivar has presented his winning submission to the developer. The master plan for the mall site, including Phase II, shows the addition of three retail buildings located along 152 Street on both sides of 104 Avenue. This gives a sense of street engagement. As well, a wider sidewalk and public realm treatment will be provided along 152 Street in Phase II. Beyond that, the mall owner has stated that any additional buildings in front of the mall's major tenant (The Bay) cannot be considered due to leasing constraints. The loss of surface parking would also be a concern. Staff would encourage the mall owner to take into consideration the ideas from the TownShift competition as part of the planning and design when they proceed with the Phase II component of the mall redevelopment (Application No. 7909-0174-00). This could include an enhanced public realm involving art sculptures and greening solutions. The three retail buildings should engage the street rather than turning their backs on the street. These considerations will be reviewed in Phase II.

Application Status

The developer is completing all outstanding requirements for finalization of the development application. It is anticipated that this application will be proceeding to Council in May 2010 for consideration of final adoption.

CONCLUSION

Based on the above discussion, it is recommended that Council approve the revised design for Phase I of the Guildford Town Centre Mall redevelopment, as described in this report, including all necessary adjustments to the Development Permit related to Development Application No. 7906-0413-00.

Original signed by
Jean Lamontagne
General Manager,
Planning and Development

PL/kms/saw

Appendices:

Appendix I	Revised Design Drawings Reflecting Current Development Proposal
Appendix II	Lot Consolidation Map
Appendix III	Revised Engineering Comments, dated November 6, 2009



PROJECT INFORMATION

GUILD FORD
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PRIME CONSULTANT
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CALLISON ARCHITECTURE
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LANDSCAPE
PWL PARTNERSHIP LANDSCAPE
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PROJECT DATA

PROJECT DESCRIPTION:
EXPANSION TO RETAIL AND PARKING
SURREY BC V8R 7C1

PROJECT ADDRESS:
2656 GUILD FORD TOWN CENTRE
SURREY BC V8R 7C1

ZONING:
C8

LEGAL DESCRIPTION:
REFER TO TABLE ON DPI-A001

ARCHITECTURAL

DRAWING DESCRIPTION

PHASE 1 COVER SHEET, CONCEPTS & DRAWINGS LIST
PHASE 1 POSSIBILITIES FOR FUTURE MODULAR DEVELOPMENT
SURVEY
EXISTING LOWER LEVEL SITE PLAN
EXISTING UPPER LEVEL SITE PLAN
EXISTING ROOF LEVEL SITE PLAN
REDESIGNED COMPLETE LOWER LEVEL SITE PLAN
REDESIGNED COMPLETE UPPER LEVEL SITE PLAN
PHASE 1 UPPER LEVEL SITE PLAN
PHASE 1 LOWER LEVEL PLAN
PHASE 1 UPPER LEVEL PLAN
PHASE 1 ROOF LEVEL PLAN

ARCHITECTURAL

DRAWING DESCRIPTION

PHASE 1 ELEVATIONS 1500 STREET
PHASE 1 SECTIONS / ELEVATIONS
PHASE 1 ELEVATIONS 1500 AND 1600 AVENUE
PHASE 1 SECTIONS X AND Y
PHASE 1 SECTIONS Z AND W
PHASE 1 BASIS FOR PLANS, SECTIONS AND ELEVATIONS
PHASE 1 EXIST STUDY PLANS, SECTIONS AND ELEVATIONS
PHASE 1 LOWER LEVEL, RCP

LANDSCAPE

DRAWING DESCRIPTION

LANDSCAPE SET PLAN
TREE MANAGEMENT PLAN
PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 PLANTING AND MATERIALS PLAN
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LANDSCAPE DETAILS
LANDSCAPE DETAILS

ARCHITECTURAL

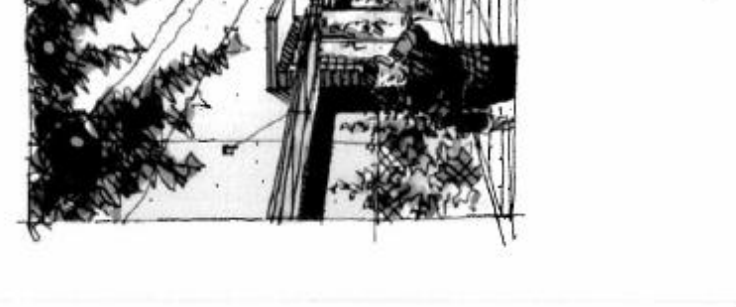
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PHASE 1 ELEVATIONS 1500 STREET
PHASE 1 SECTIONS / ELEVATIONS
PHASE 1 ELEVATIONS 1500 AND 1600 AVENUE
PHASE 1 SECTIONS X AND Y
PHASE 1 SECTIONS Z AND W
PHASE 1 BASIS FOR PLANS, SECTIONS AND ELEVATIONS
PHASE 1 EXIST STUDY PLANS, SECTIONS AND ELEVATIONS
PHASE 1 LOWER LEVEL, RCP

LANDSCAPE

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PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 PLANTING AND MATERIALS PLAN
LANDSCAPE DETAILS
LANDSCAPE DETAILS



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GUILD FORD TOWN CENTRE EXPANSION PH1
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SURREY BC V8R 7C1

PHASE 1 COVER SHEET CONSULTANTS & DRAWINGS LIST

DATE: 2018.04.11

SCALE: 1:100

DPI-A00

ARCHITECTURAL	LANDSCAPE
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DPI-A06	L3.06
DPI-A07	L3.07
DPI-A08	L3.08
DPI-A09	L3.09
DPI-A10	L3.10
DPI-A11	L3.11
DPI-A12	L3.12

ARCHITECTURAL	LANDSCAPE
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1/4" = 1'-0"	1/4" = 1'-0"
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1" = 1'-0"	1" = 1'-0"
AS NOTED	AS NOTED

ARCHITECTURAL	LANDSCAPE
DRAWING DESCRIPTION	DRAWING DESCRIPTION
PHASE 1 ELEVATIONS 1500 STREET	LANDSCAPE SET PLAN
PHASE 1 SECTIONS / ELEVATIONS	TREE MANAGEMENT PLAN
PHASE 1 ELEVATIONS 1500 AND 1600 AVENUE	PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 SECTIONS X AND Y	PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 SECTIONS Z AND W	PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 BASIS FOR PLANS, SECTIONS AND ELEVATIONS	PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 EXIST STUDY PLANS, SECTIONS AND ELEVATIONS	PHASE 1 PLANTING AND MATERIALS PLAN
PHASE 1 LOWER LEVEL, RCP	LANDSCAPE DETAILS

ARCHITECTURAL	LANDSCAPE
CONCEPT / MEMORANDUM	

ARCHITECTURAL	LANDSCAPE
CONCEPT / MEMORANDUM	

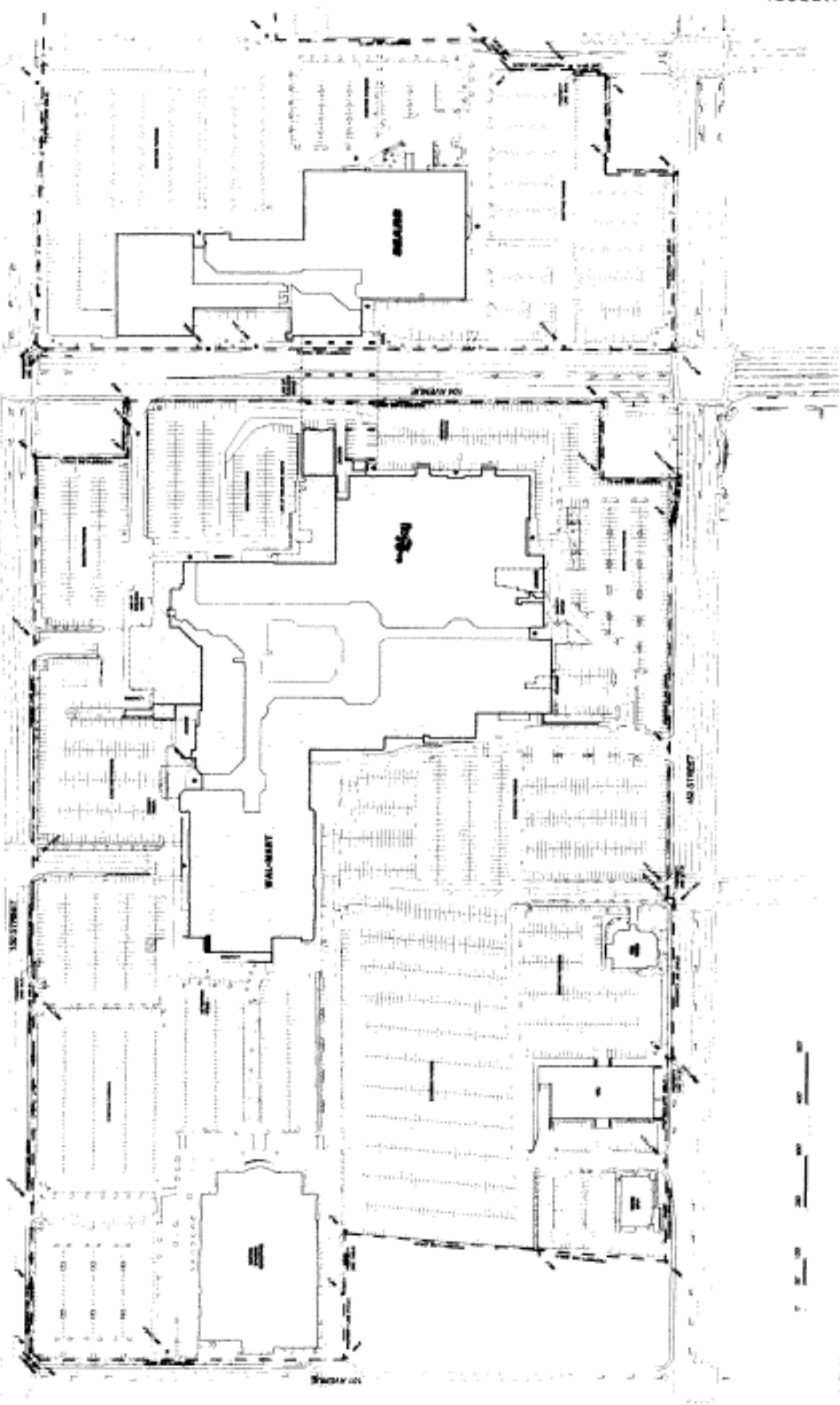
ARCHITECTURAL	LANDSCAPE
CONCEPT / MEMORANDUM	

ARCHITECTURAL	LANDSCAPE
CONCEPT / MEMORANDUM	

ARCHITECTURAL	LANDSCAPE
CONCEPT / MEMORANDUM	



CALLISON



EXISTING LOWER LEVEL SITE PLAN

RUSSON
CATELL
PARTNERSHIP

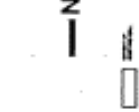
GUILDFORD
COMMUNITY
DEVELOPMENT
DEPARTMENT

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DRAWN: J. WELLS
CHECKED: J. WELLS
DPI-A004



CALLISON



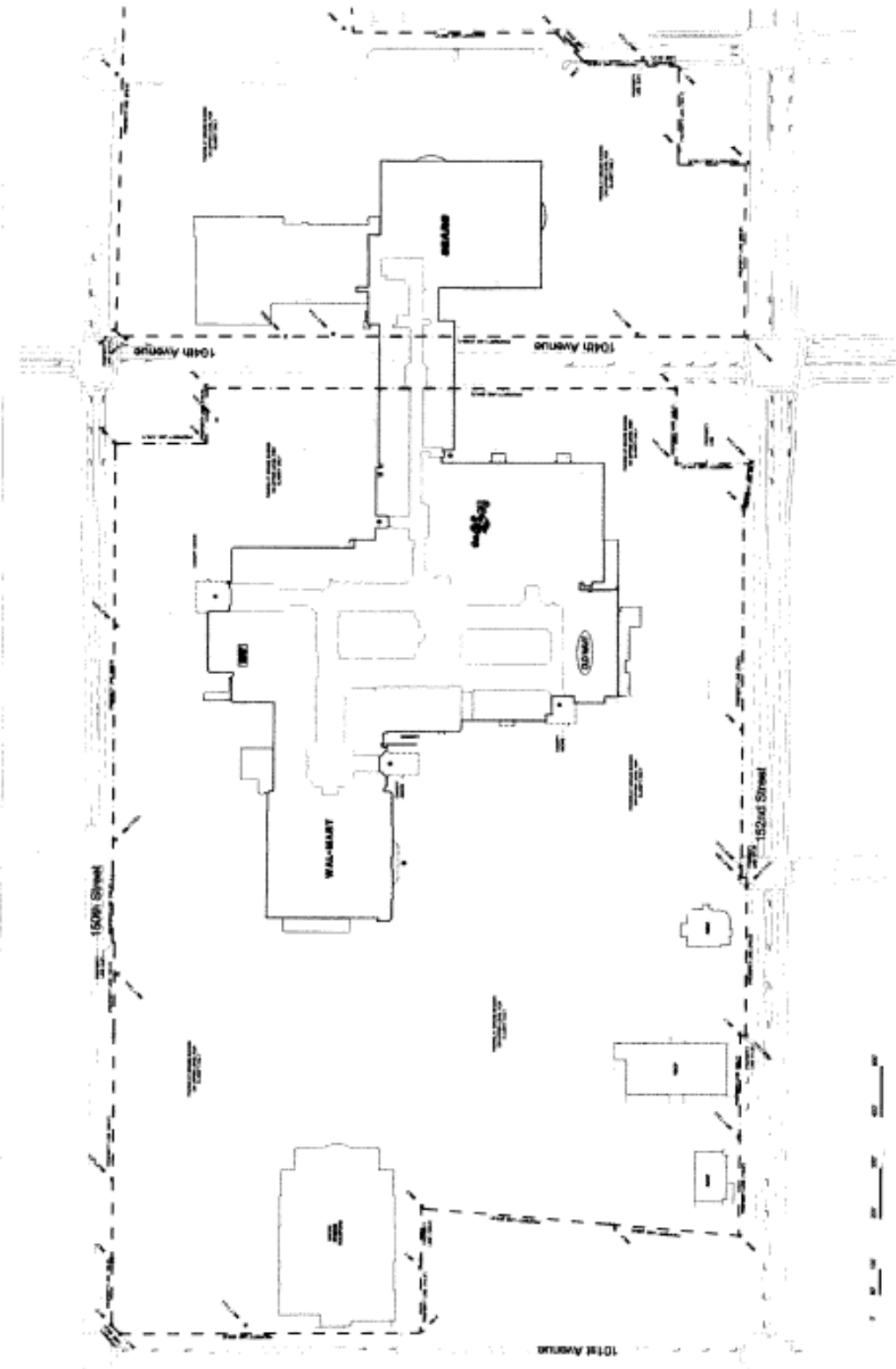
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MUSSON
CASTELL
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ARCHITECTS
1000 10th Avenue
Denver, CO 80202
TEL: 303.733.1100
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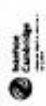
GOULD FORD
TOWN CENTER
EXPANSION (PH)
1000 10th Avenue
Denver, CO 80202

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SHEET: DP1-A005



EXISTING UPPER LEVEL SITE PLAN



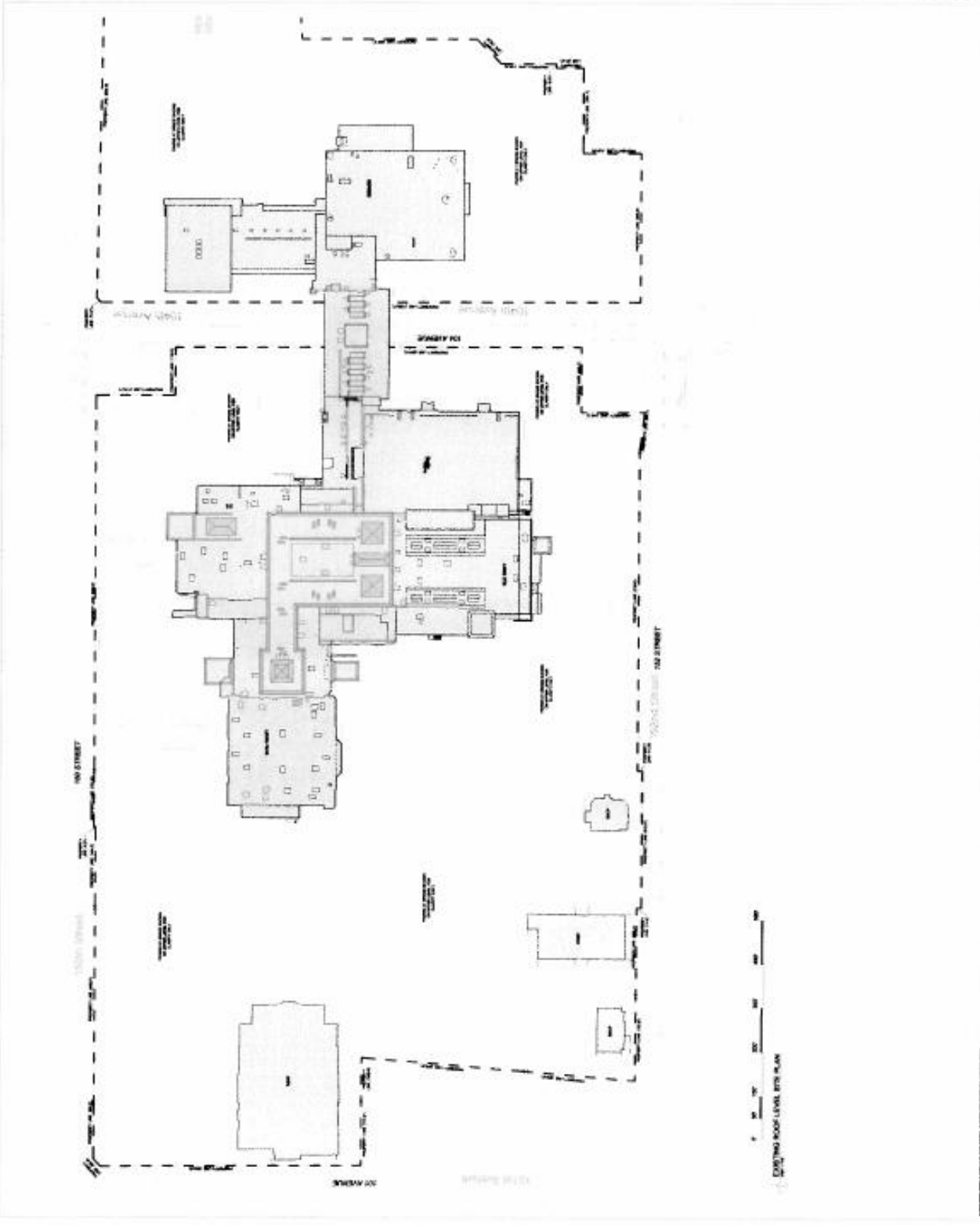
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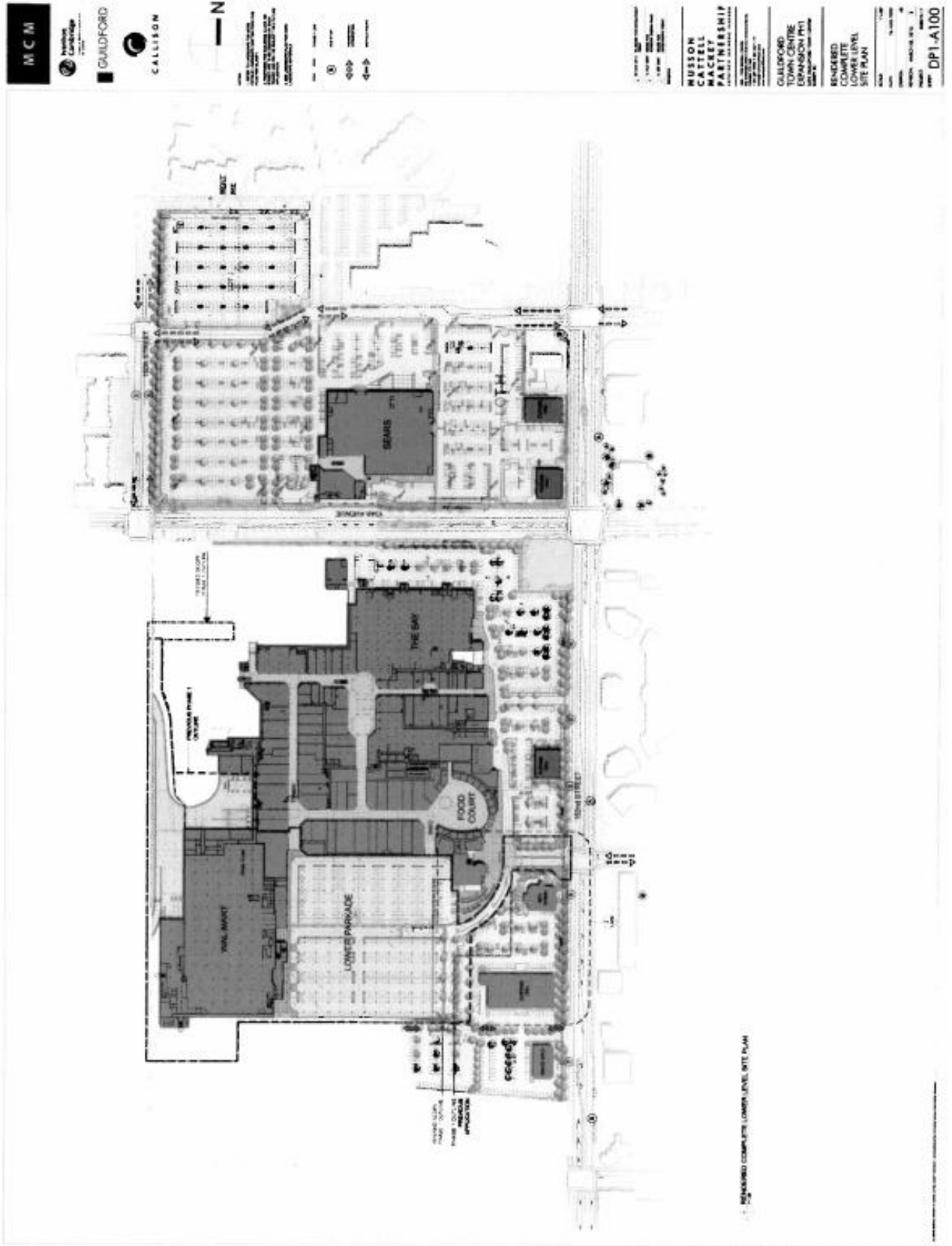
**MUSSON
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GUILD FORD
 TOWN CENTRE
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**EXISTING ROOF
 LEVEL SITE PLAN**

DATE: 08/11/10
 DRAWN: J. WAKELI
 CHECKED: M. WAKELI
 PROJECT: TOWN CENTRE
 SHEET: DP1-A006





MCM
McCarthy Construction Management
2020
2021

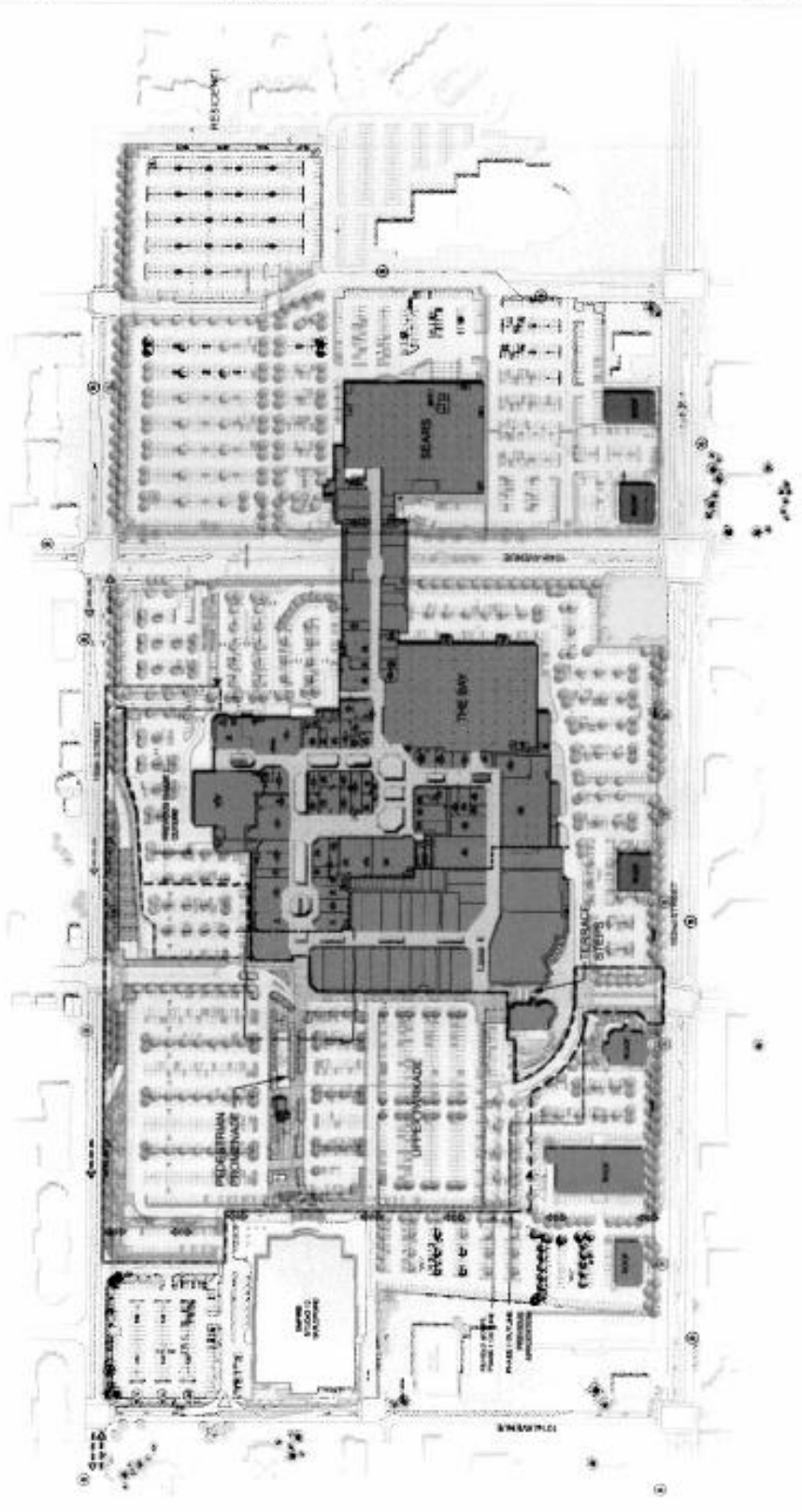
GUILDFORD

CALLISON

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100'-0" DIMENSION LINE
20'-0" DIMENSION LINE
40'-0" DIMENSION LINE
80'-0" DIMENSION LINE

1'-0" DIMENSION LINE
2'-0" DIMENSION LINE
4'-0" DIMENSION LINE
8'-0" DIMENSION LINE
16'-0" DIMENSION LINE



RENDERED COMPLETE UPPER LEVEL SITE PLAN

MUSSON
CATELL
MACKAY
PARTNERSHIP

GUILDFORD
TOWN CENTRE
EXPANSION (PH)

RENDERED
COMPLETE
UPPER LEVEL
SITE PLAN

DATE: 10/26/2021
PROJECT: GUILDFORD TOWN CENTRE EXPANSION (PH)
DRAWN BY: MCM
CHECKED BY: MCM
SCALE: AS SHOWN

DP1-A101

MCM
MASSACHUSETTS COLLEGE OF MANAGEMENT
GUILDFORD
CALLISON

PHASE 1 LOWER LEVEL SITE PLAN

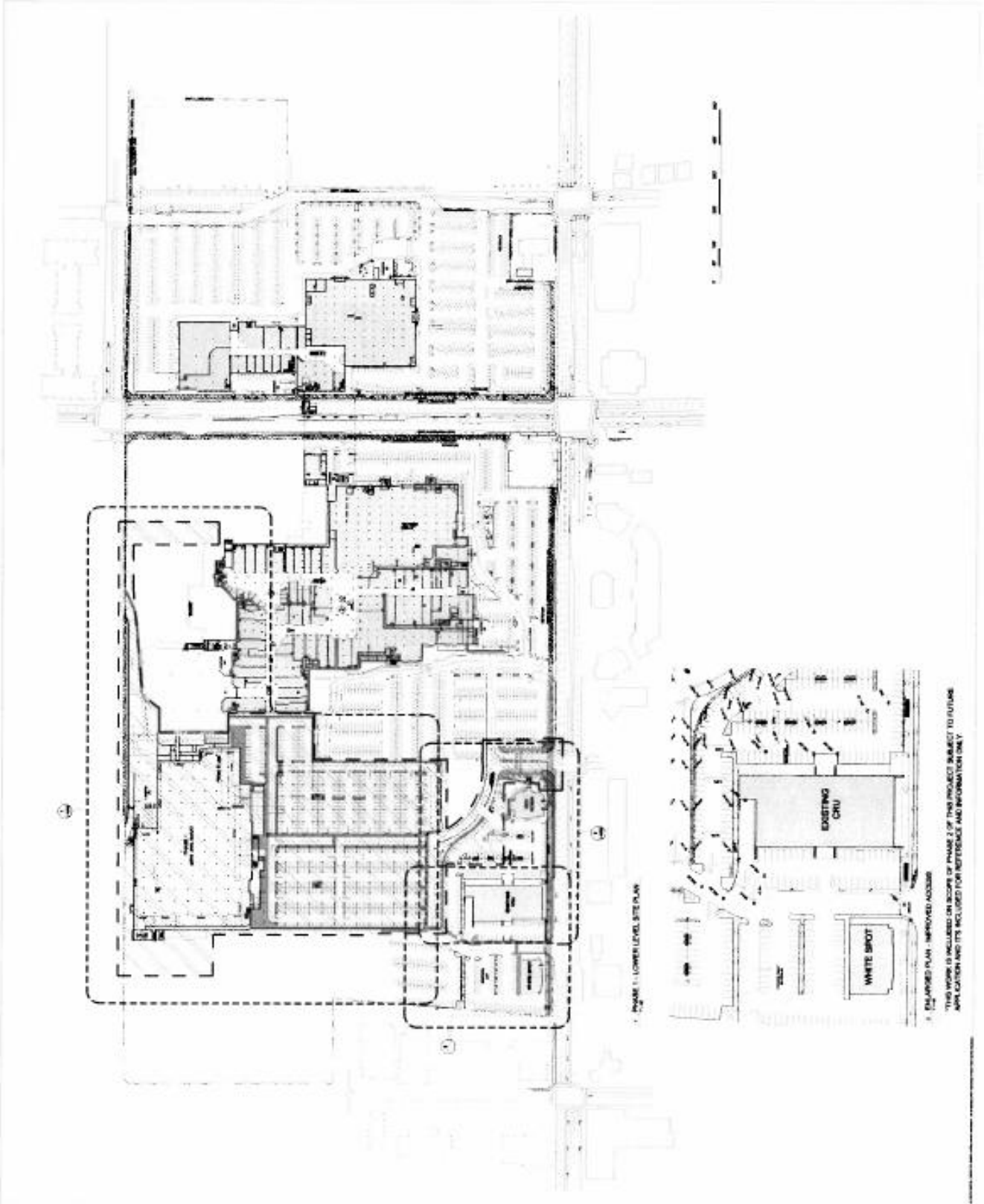
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PHASE 1 LOWER LEVEL SITE PLAN

DATE: 11/14/11
 DRAWN: J. R. BROWN
 CHECKED: J. R. BROWN
 APPR: J. R. BROWN
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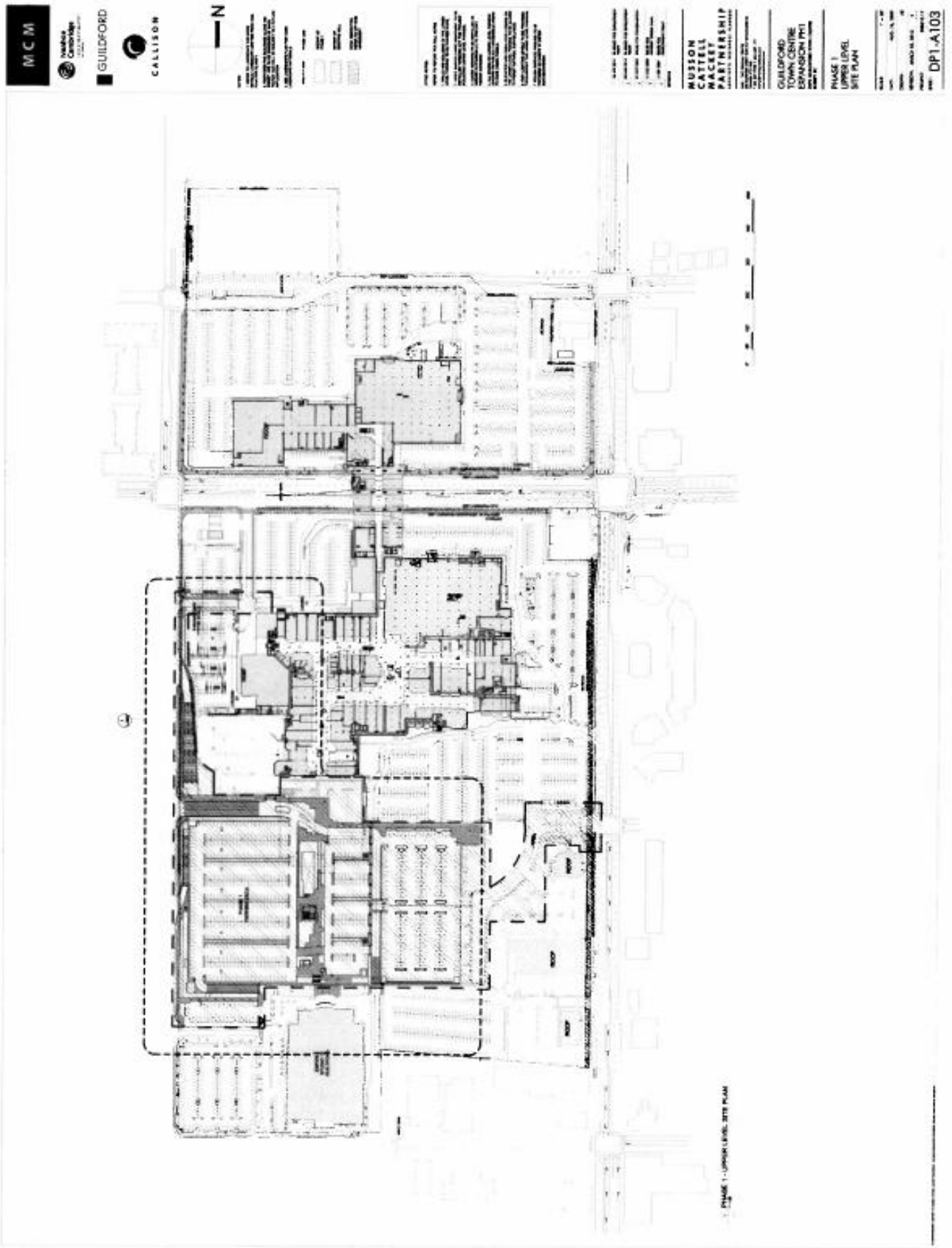
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PHASE 1 LOWER LEVEL SITE PLAN

PHASE 1 LOWER LEVEL SITE PLAN

THIS WORK IS INCLUDED ON SCOPE OF PHASE 2 OF THIS PROJECT SUBJECT TO FUTURE APPLICATION AND ITS INCLUSION FOR REFERENCE AND INFORMATION ONLY.



MCM
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MAYHEW
MAYHEW PARTNERSHIP

GUILD FORD
GUILD FORD PARTNERSHIP

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**PHASE 1
UPPER LEVEL
SITE PLAN**

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DRAWN BY: J. HARRIS
CHECKED BY: J. HARRIS
DATE: 11.14.18
PROJECT NO: DP1-A103



GUILDFORD



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- 4. EXISTING GLAZING
- 5. EXISTING ROOF
- 6. EXISTING FLOOR
- 7. EXISTING CEILING
- 8. EXISTING MASONRY
- 9. EXISTING CONCRETE
- 10. EXISTING METAL
- 11. EXISTING WOOD
- 12. EXISTING GROUND
- 13. EXISTING UTILITIES
- 14. EXISTING LANDSCAPE
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- 19. EXISTING LIGHTING
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- 1. NEW WALL
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- 6. NEW FLOOR
- 7. NEW CEILING
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- 47. NEW AIRWAY TREES
- 48. NEW AIRWAY WALKWAYS
- 49. NEW AIRWAY DRIVEWAYS
- 50. NEW AIRWAY DRIVE
- 51. NEW AIRWAY HIGHWAY

MUSON
CUSTON
MAGNET
PARTNERSHIP

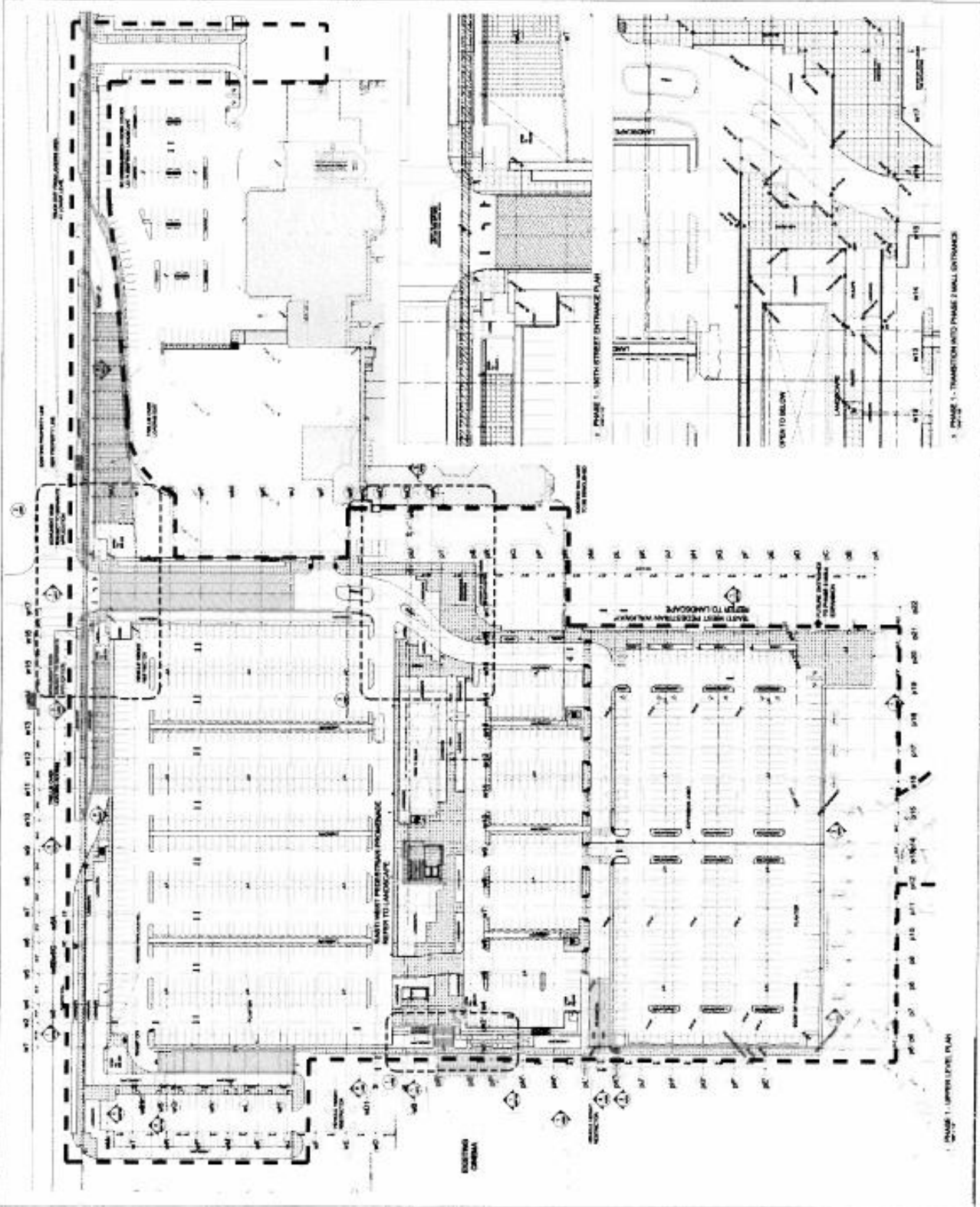
GUILDFORD
TOWN CENTRE
EXPANSION PH1

PHASE 1
UPPER LEVEL
PLAN

DATE: 10.11.17
SCALE: 1:500
DRAWN: J.P.
CHECKED: M.S.
PROJECT: PH1

NO.	REV.
1	ISSUED FOR PERMIT
2	ISSUED FOR TENDER
3	ISSUED FOR CONSTRUCTION

DP1-A201



PHASE 1 - WITH STREET ENTRANCE PLAN

PHASE 1 - TRANSITION INTO PHASE 1 SMALL ENTRANCES

PHASE 1 - UPPER LEVEL PLAN

M C M

GUILDFOORD
CALLISON

CALLISON



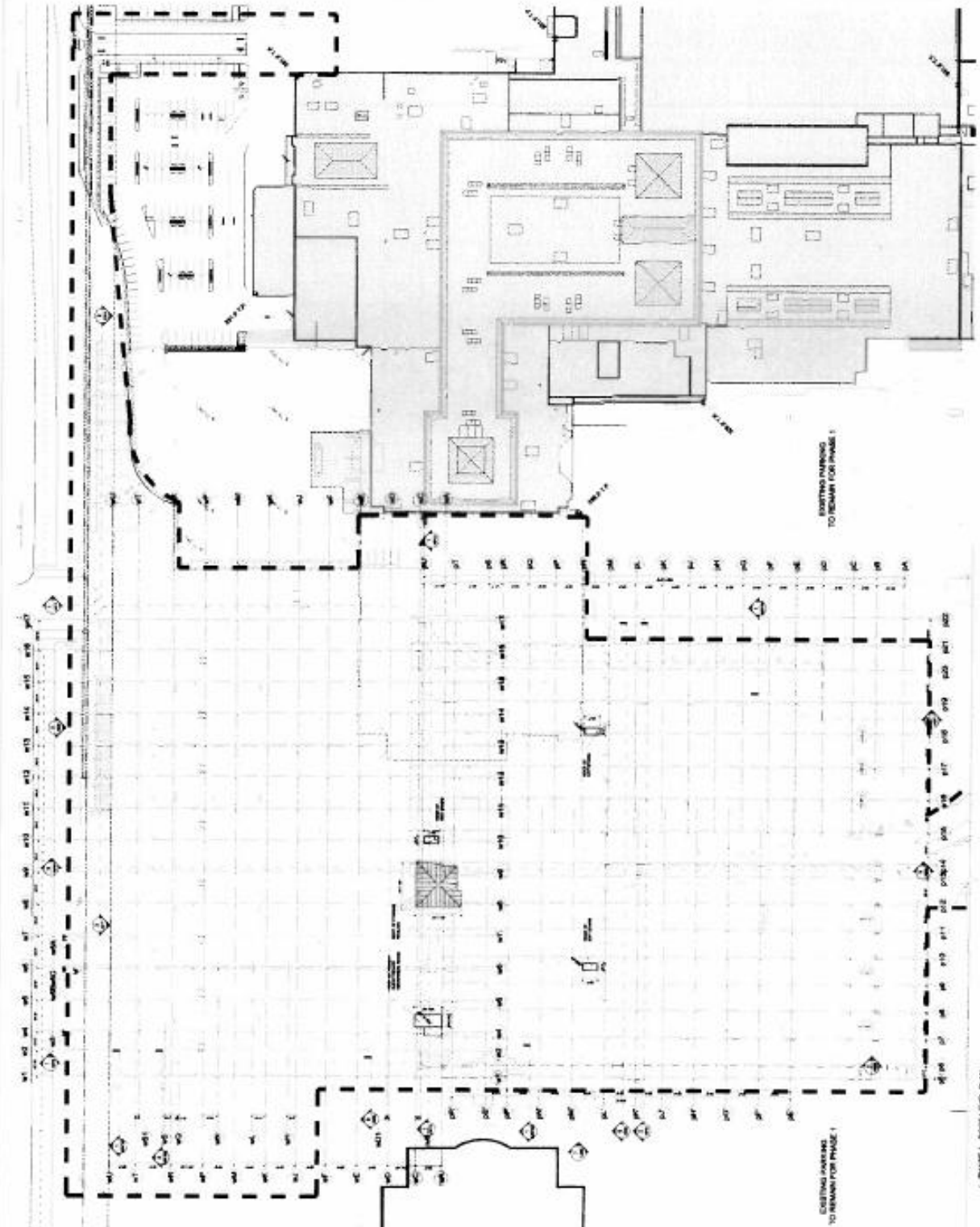
- 1. EXISTING ROOF STRUCTURE
- 2. EXISTING ROOF FINISH
- 3. EXISTING ROOF DECK
- 4. EXISTING ROOF MECHANICAL
- 5. EXISTING ROOF PENETRATIONS
- 6. EXISTING ROOF CURBS
- 7. EXISTING ROOF DRAINAGE
- 8. EXISTING ROOF VENTS
- 9. EXISTING ROOF ACCESS
- 10. EXISTING ROOF ELEVATION
- 11. EXISTING ROOF AREA
- 12. EXISTING ROOF PERIMETER
- 13. EXISTING ROOF INTERIORS
- 14. EXISTING ROOF EXTERIORS
- 15. EXISTING ROOF UTILITIES
- 16. EXISTING ROOF STRUCTURE
- 17. EXISTING ROOF FINISH
- 18. EXISTING ROOF DECK
- 19. EXISTING ROOF MECHANICAL
- 20. EXISTING ROOF PENETRATIONS
- 21. EXISTING ROOF CURBS
- 22. EXISTING ROOF DRAINAGE
- 23. EXISTING ROOF VENTS
- 24. EXISTING ROOF ACCESS
- 25. EXISTING ROOF ELEVATION
- 26. EXISTING ROOF AREA
- 27. EXISTING ROOF PERIMETER
- 28. EXISTING ROOF INTERIORS
- 29. EXISTING ROOF EXTERIORS
- 30. EXISTING ROOF UTILITIES

MUSSON
KAY
SHELL
PARTNERSHIP

GUILDFOORD
TOWN CENTRE
EXPANSION PH1

PHASE 1
ROOF LEVEL
PLAN

DATE: 10/11/10
PROJECT: GUILDFOORD TOWN CENTRE EXPANSION PH1
DRAWN: M.C.M.
CHECKED: M.C.M.
SCALE: 1:100
NO: DP1-A202



PHASE 1 - ROOF LEVEL PLAN

MCM

GUILD FORD

CALLISON

- REVISIONS**
1. 01/11/2011
 2. 01/11/2011
 3. 01/11/2011
 4. 01/11/2011
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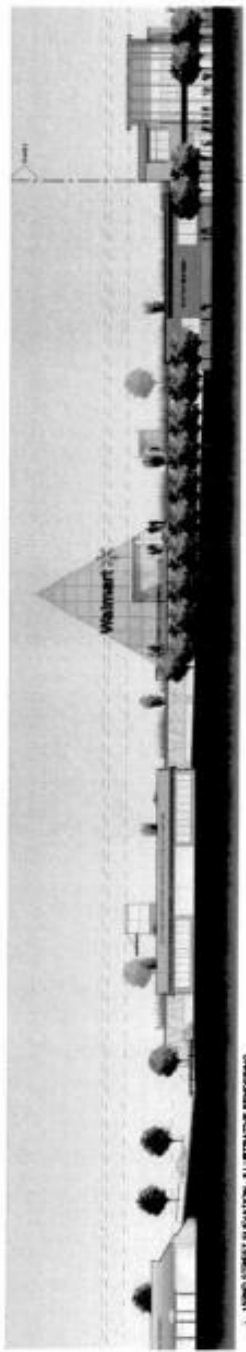
MUSSON CATTELL MACKELLY PARTNERSHIP

GUILD FORD TOWN CENTRE EXPANSION PH1

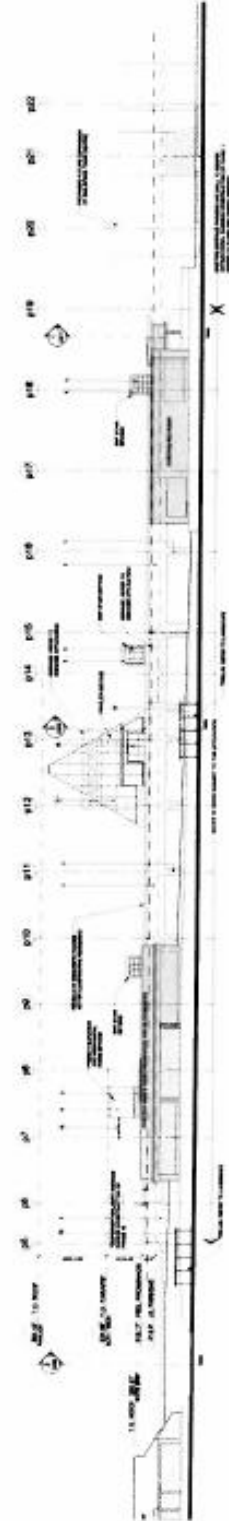
PHASE 1 ELEVATIONS

152ND STREET

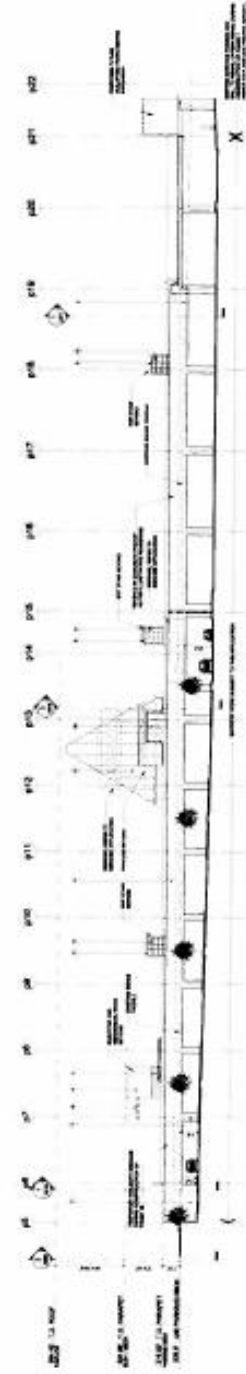
DPI-A300



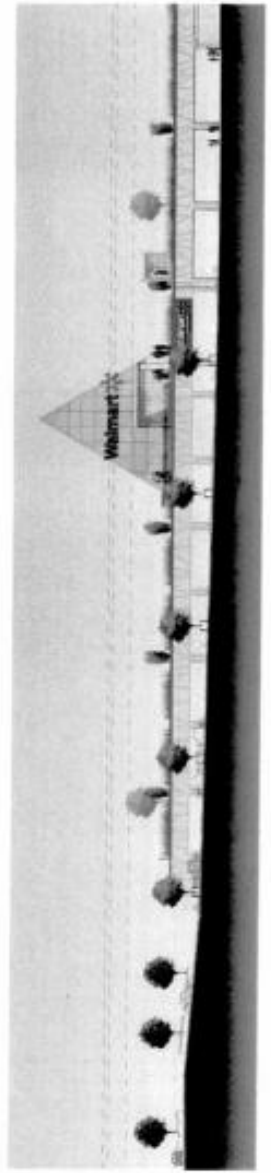
1. 152ND STREET ELEVATION - ILLUSTRATIVE RENDERING



2. 152ND STREET ELEVATION AT STREET



3. 152ND STREET ELEVATION AT PARKADE



4. 152ND STREET ELEVATION AT PARKADE - ILLUSTRATIVE RENDERING



- 1. GENERAL NOTES
- 2. ARCHITECTURAL SYMBOLS
- 3. FINISHES
- 4. MATERIALS
- 5. LIGHTING
- 6. MECHANICAL
- 7. ELECTRICAL
- 8. PLUMBING
- 9. ROOFING
- 10. EXTERIOR WALLS
- 11. EXTERIOR FLOORS
- 12. EXTERIOR STAIRS
- 13. EXTERIOR RAILINGS
- 14. EXTERIOR SIGNAGE
- 15. EXTERIOR LANDSCAPE
- 16. EXTERIOR FURNITURE
- 17. EXTERIOR LIGHTING
- 18. EXTERIOR MECHANICAL
- 19. EXTERIOR ELECTRICAL
- 20. EXTERIOR PLUMBING

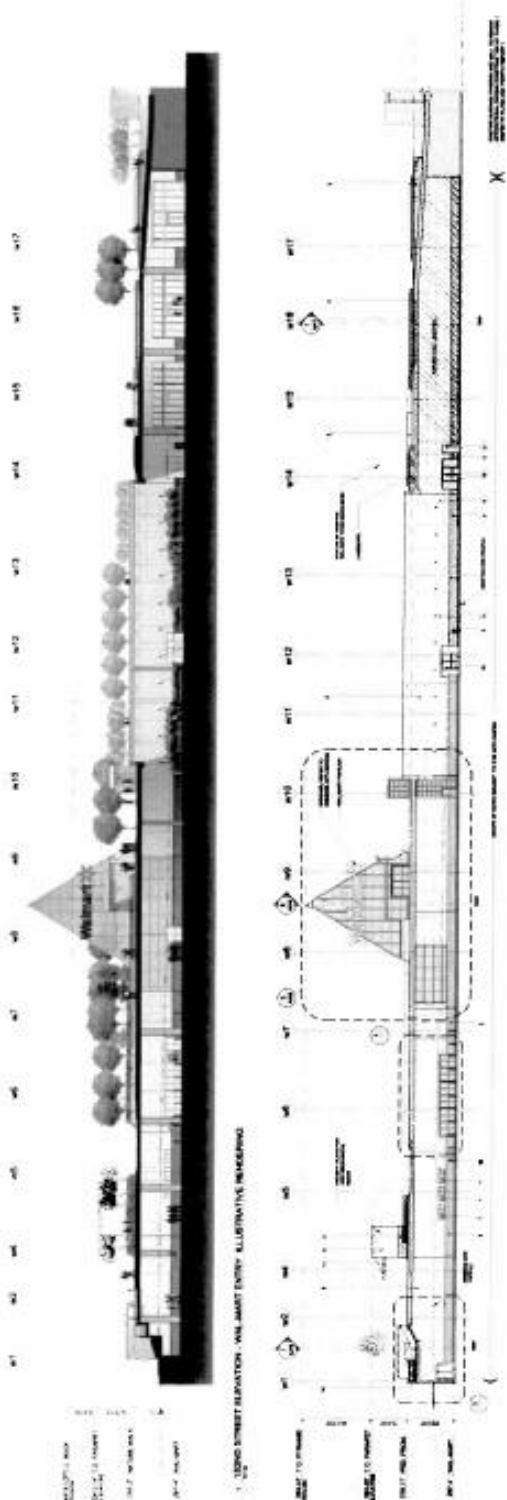
DATE: 10/20/2011
 TIME: 10:00 AM
 USER: JLM
 PROJECT: WALMART TOWN CENTRE

MUSSON
 CATTELL
 MACKAY
 PARTNERSHIP

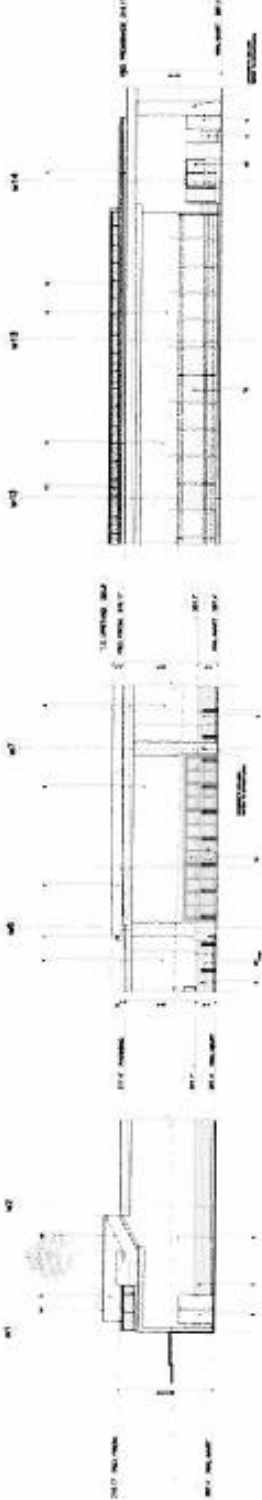
GUILDFORD
 TOWN CENTRE
 WELLS

PHASE 1
 SECTIONS /
 ELEVATIONS

DATE: 10/20/2011
 TIME: 10:00 AM
 USER: JLM
 PROJECT: WALMART TOWN CENTRE
 SHEET: DP-A301



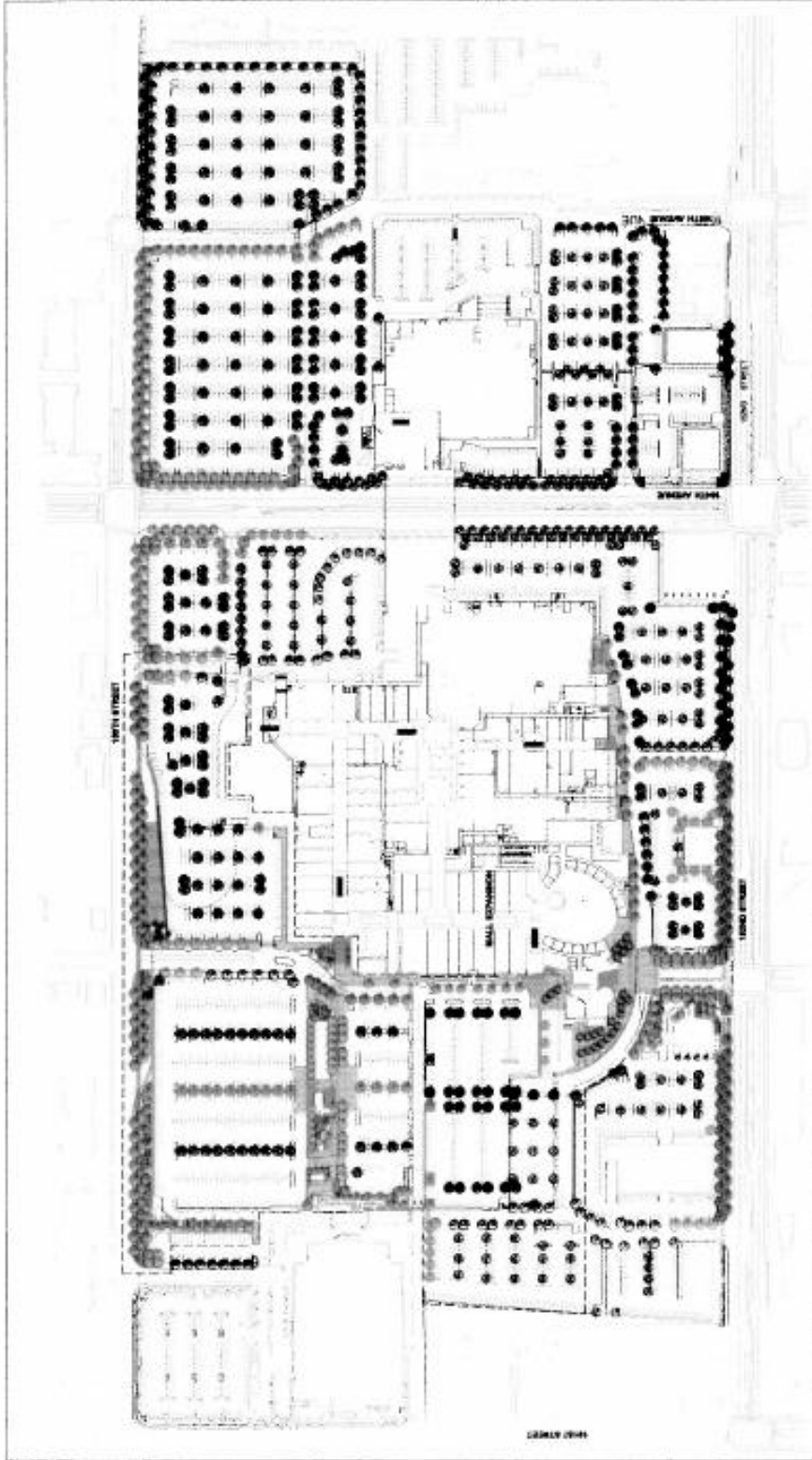
SECTION A - WALMART EAST ELEVATION



SECTION B - WALMART WEST ELEVATION



1. WALMART TOWN CENTRE ELEVATION - RENDERING
 2. WALMART TOWN CENTRE ELEVATION - RENDERING
 3. SOUTH EAST INTERFERENCE WITH FUTURE PHASE 2



MUSSON
CATELL
MACEY
PARTNERSHIP

GUILDFORD
EXPANSION PHASE I

DATE: 08/14/2014

SCALE: 1/8" = 1'-0"

PROJECT: GUILDFORD EXPANSION PHASE I

PREPARED BY: MCM

CHECKED BY: [Name]

APPROVED BY: [Name]

GENERAL TREE PLAN

TOTAL: 157

PLANT: 157

SCALE: 1/8" = 1'-0"

DATE: 08/14/2014

PROJECT: GUILDFORD EXPANSION PHASE I

PREPARED BY: MCM

CHECKED BY: [Name]

APPROVED BY: [Name]

LDP 2.00

GENERAL NOTES:

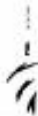
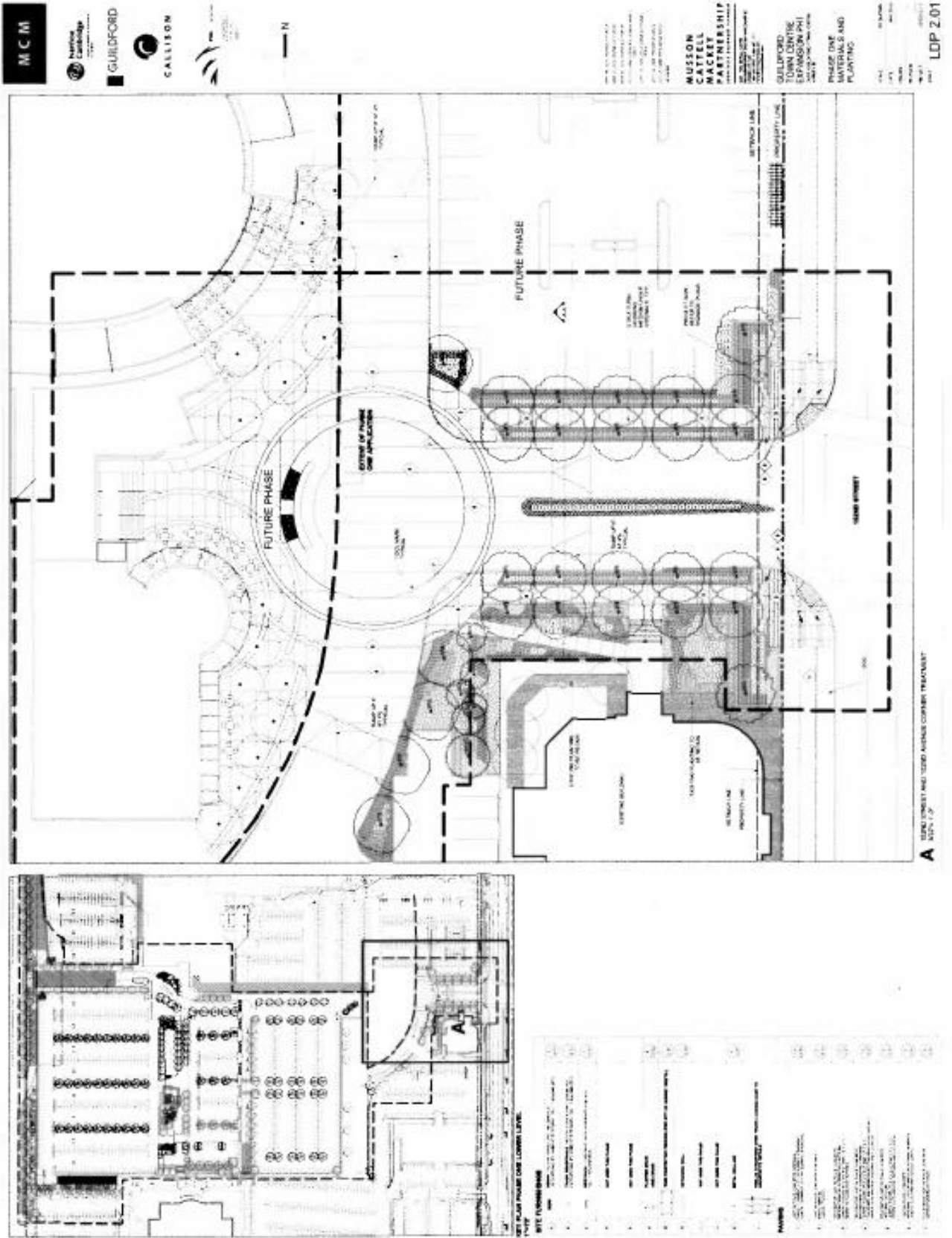
1. ALL TREES TO BE PLANTED BY THE CONTRACTOR AND MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
2. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
3. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
4. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
5. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
6. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
7. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
8. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
9. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.
10. ALL TREES TO BE PLANTED IN ACCORDANCE WITH THE CITY OF GUILDFORD TREE ORDINANCE.

Tree List - Phase I and II

Tree ID	Species	Size	Location	Notes
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Tree List - Phase I

Tree ID	Species	Size	Location	Notes
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DATE: 11/11/11
 SCALE: AS SHOWN
 DRAWN BY: J. HARRIS
 CHECKED BY: J. HARRIS
 PROJECT NO.: 11-001

MUSSON
 CATTELL
 MACKAY
 PARTNERSHIP

GUILD FORD
 LANDSCAPE
 ARCHITECTURE
 1110 11TH AVENUE N.E.
 SUITE 100
 SEASIDE, WA 98148

PHASE ONE
 MATERIALS AND
 PLANTING

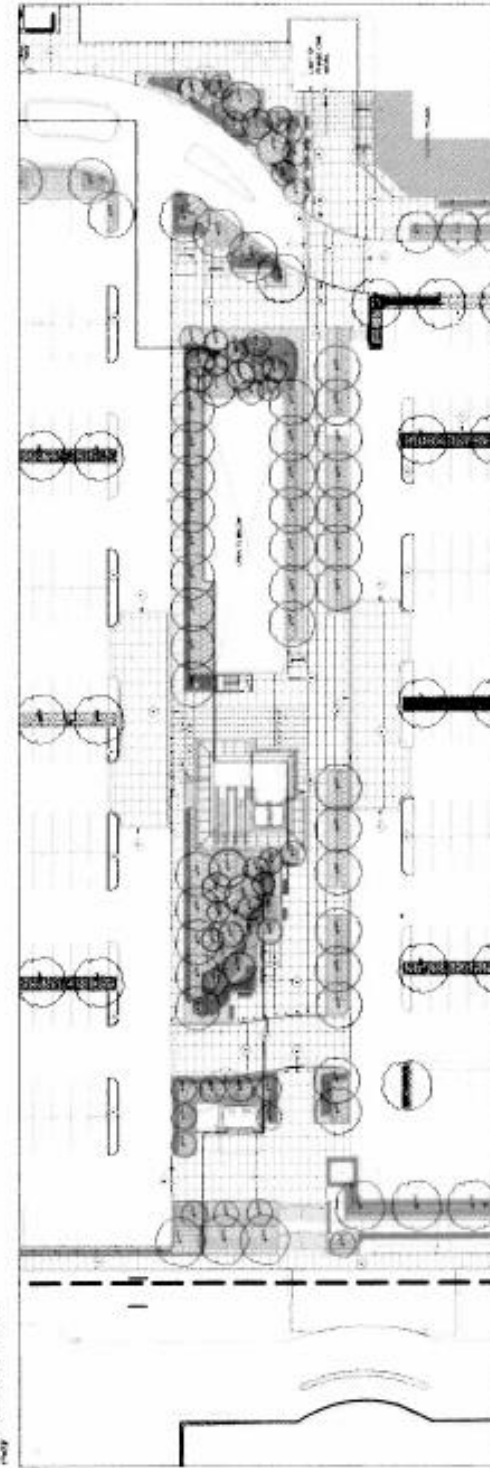
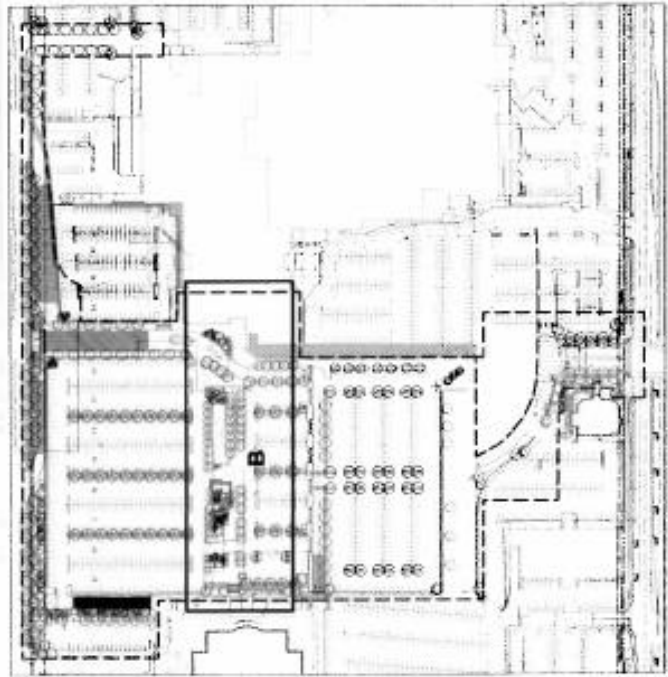
DATE: 11/11/11
 SCALE: AS SHOWN
 DRAWN BY: J. HARRIS
 CHECKED BY: J. HARRIS
 PROJECT NO.: 11-001
 SHEET NO.: 101
 LDP 2.01

A SEASIDE TOWNSHIP AND 133RD AVENUE CORNER TREATMENT PLANT



NO.	DESCRIPTION	UNIT	QTY	UNIT PRICE	TOTAL PRICE
1	CONCRETE	M ³	100	150	15000
2	STEEL	KG	500	30	15000
3	CEMENT	M ³	200	75	15000
4	AGGREGATE	M ³	300	50	15000
5	BRICKS	NO.	10000	10	10000
6	ROOFING	M ²	500	30	15000
7	PAINT	L	100	150	15000
8	LABOUR	HR	1000	15	15000
9	TRANSPORT	HR	100	150	15000
10	WATER	L	10000	10	10000
11	ELECTRICITY	KWH	1000	10	10000
12	TELEPHONE	HR	100	150	15000
13	INSULATION	M ²	500	30	15000
14	GLASS	M ²	100	150	15000
15	DOORS	NO.	10	1500	15000
16	WINDOWS	NO.	20	750	15000
17	FLOORING	M ²	1000	15	15000
18	CEILING	M ²	1000	15	15000
19	MECHANICAL	HR	100	150	15000
20	ELECTRICAL	HR	100	150	15000

NO.	DESCRIPTION	UNIT	QTY	UNIT PRICE	TOTAL PRICE
21	CONCRETE	M ³	100	150	15000
22	STEEL	KG	500	30	15000
23	CEMENT	M ³	200	75	15000
24	AGGREGATE	M ³	300	50	15000
25	BRICKS	NO.	10000	10	10000
26	ROOFING	M ²	500	30	15000
27	PAINT	L	100	150	15000
28	LABOUR	HR	1000	15	15000
29	TRANSPORT	HR	100	150	15000
30	WATER	L	10000	10	10000
31	ELECTRICITY	KWH	1000	10	10000
32	TELEPHONE	HR	100	150	15000
33	INSULATION	M ²	500	30	15000
34	GLASS	M ²	100	150	15000
35	DOORS	NO.	10	1500	15000
36	WINDOWS	NO.	20	750	15000
37	FLOORING	M ²	1000	15	15000
38	CEILING	M ²	1000	15	15000
39	MECHANICAL	HR	100	150	15000
40	ELECTRICAL	HR	100	150	15000



MUSSEON
CATTELL
MACKAY
PARTNERSHIP
CIVIL ENGINEERS
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B MATERIALS AND PLANTING
1:100

MCM

Guilford

GUILDFORD

CALLISON

MISSION

VISION

MAKLEY

PARTNERSHIP

GUILDFORD TOWN CENTRE EXPANSION PH1

LANDSCAPE PLAN

MATERIALS & PLANTING

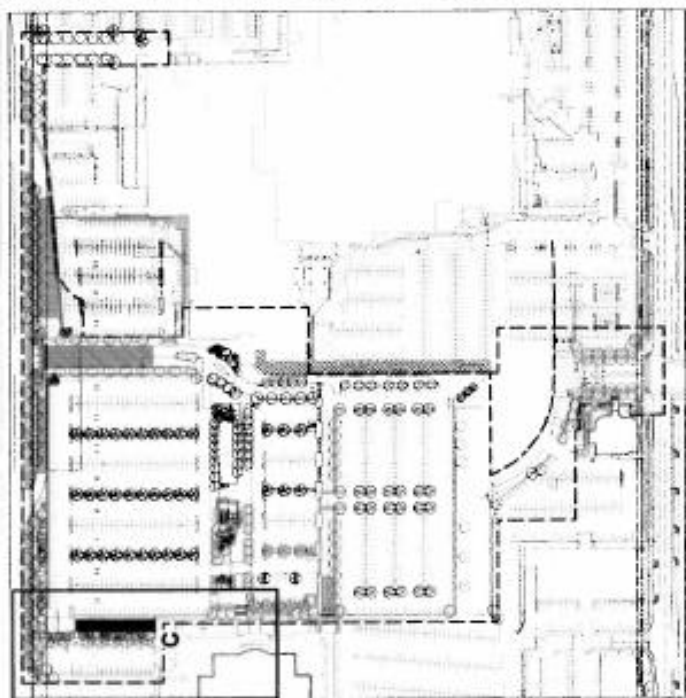
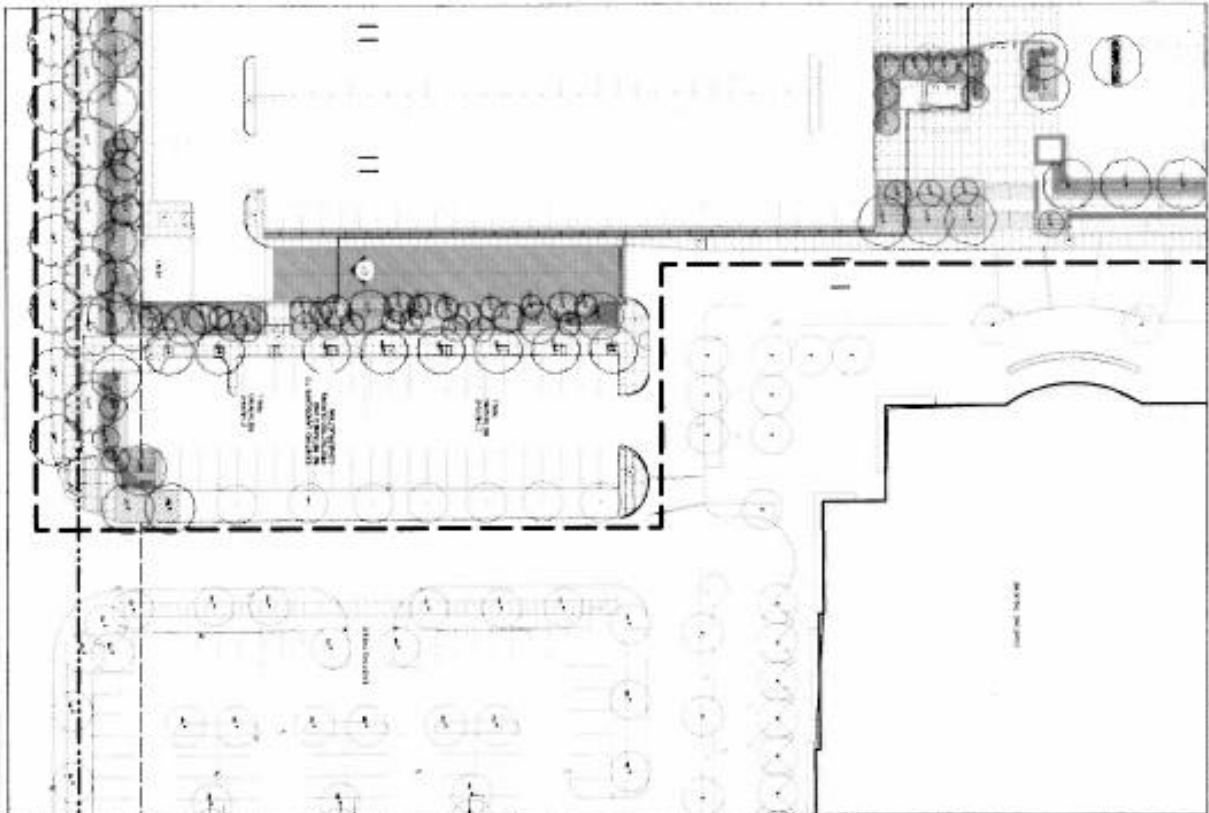
DATE

SCALE

PROJECT

NO.

LDP 2.03



OFF PLAN HOUSE ONE UPPER LEVEL

NOTES

1.	SEE PLAN FOR TREE SIZES AND PLANTING SCHEDULE.
2.	ALL PLANTING TO BE MAINTAINED TO COMPLETION OF CONSTRUCTION.
3.	ALL PLANTING TO BE REMOVED AT THE END OF THE PROJECT.
4.	ALL PLANTING TO BE REPLACED AT THE END OF THE PROJECT.
5.	ALL PLANTING TO BE MAINTAINED TO COMPLETION OF CONSTRUCTION.
6.	ALL PLANTING TO BE REMOVED AT THE END OF THE PROJECT.

KEY

1.	EXISTING PLANTING TO BE MAINTAINED TO COMPLETION OF CONSTRUCTION
2.	EXISTING PLANTING TO BE REMOVED
3.	NEW PLANTING
4.	EXISTING PLANTING TO BE MAINTAINED TO COMPLETION OF CONSTRUCTION
5.	EXISTING PLANTING TO BE REMOVED
6.	NEW PLANTING

C PLANTING AND PLANTING



100% COMPLETE
 DATE OF PRELIMINARY
 11/11/2014
 11/11/2014
 11/11/2014

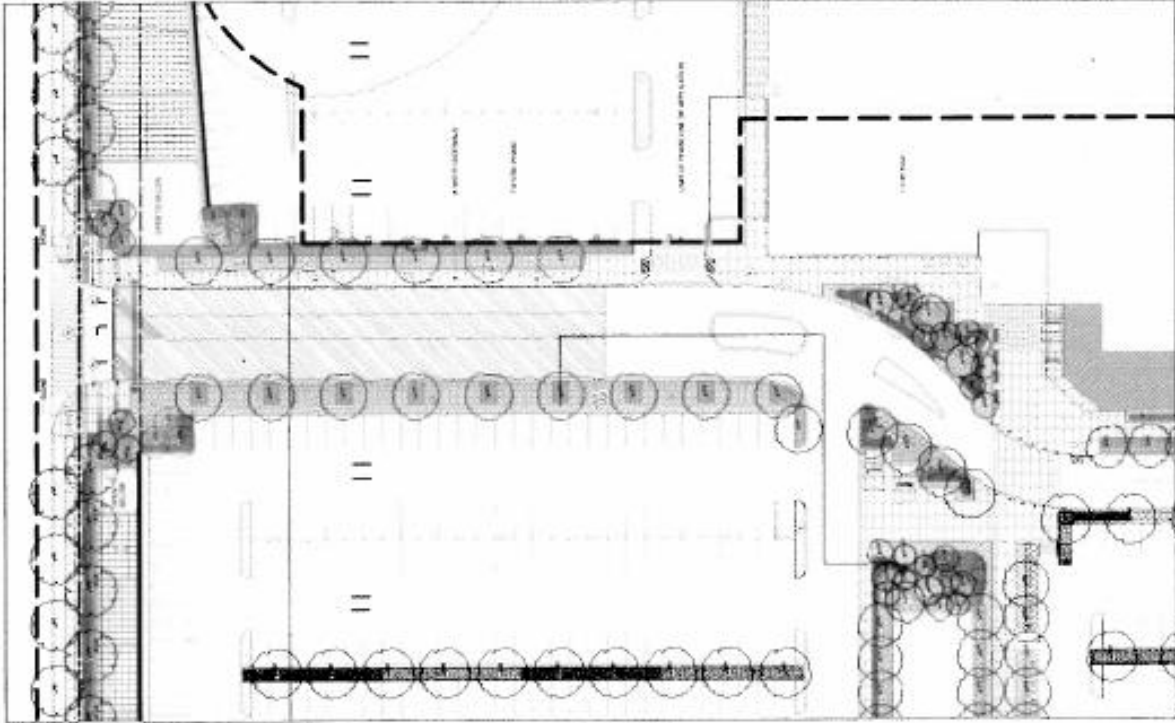
**MUSLIM
 CAPITAL
 PARTNERSHIP**

GUILDFORD
 TOWN CENTRE
 EXPANSION PH1
 2014

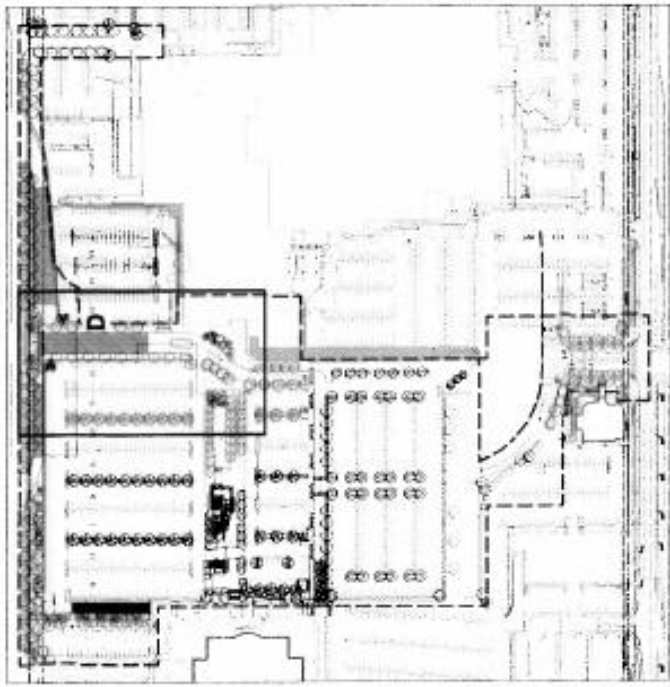
**MATERIALS AND
 PLANTING**

DATE: 11/11/2014
 DRAWN: [Name]
 CHECKED: [Name]
 2014

LOP 2.04



D PAVED PLANTING



D PAVED PLANTING

NO.	DESCRIPTION	QTY	UNIT
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NO.	DESCRIPTION	QTY	UNIT
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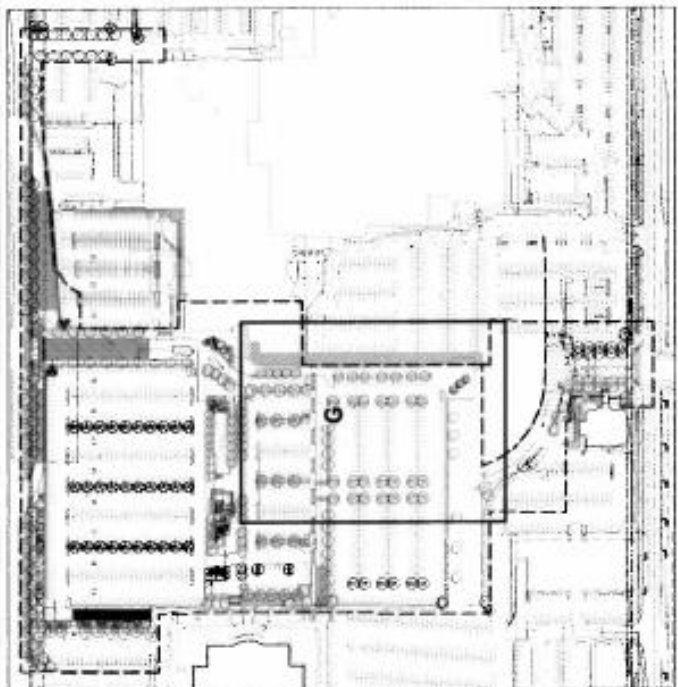
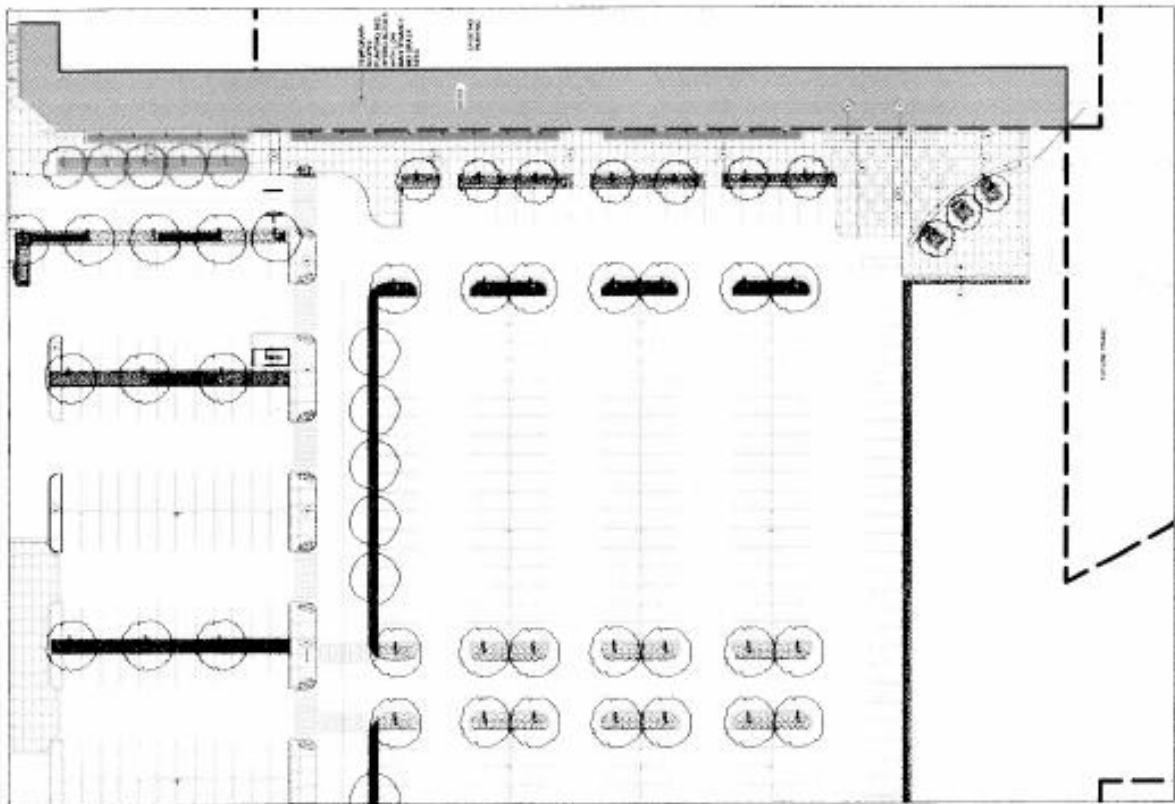


MUSSON
CATTRELL
WACKETT
ARCHITECTS
PARTNERSHIP

GUILDFORD
TOWN CENTRE
EXPANSION PH1

MATERIALS AND
PLANNING

DATE: 10/01/17
DRAWN: JH/17
CHECKED: JH/17
SCALE: 1:100
SHEET: 207
PROJECT: LDP 2.07



1:2500 PLAN FRAME ONE UPPER LEVEL

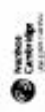
NOTES

1. SEE 207/10/17/001/001
2. SEE 207/10/17/001/002
3. SEE 207/10/17/001/003
4. SEE 207/10/17/001/004
5. SEE 207/10/17/001/005
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7. SEE 207/10/17/001/007
8. SEE 207/10/17/001/008
9. SEE 207/10/17/001/009
10. SEE 207/10/17/001/010

KEY PLAN FRAME ONE UPPER LEVEL

1. SEE 207/10/17/001/001
2. SEE 207/10/17/001/002
3. SEE 207/10/17/001/003
4. SEE 207/10/17/001/004
5. SEE 207/10/17/001/005
6. SEE 207/10/17/001/006
7. SEE 207/10/17/001/007
8. SEE 207/10/17/001/008
9. SEE 207/10/17/001/009
10. SEE 207/10/17/001/010

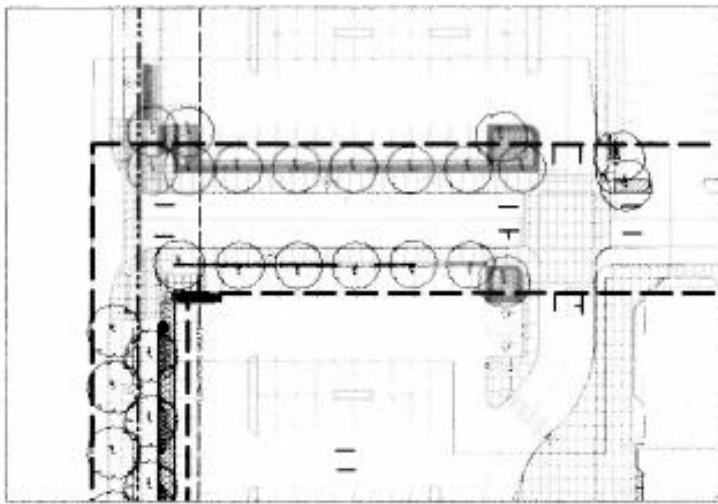
G UPPER AND MIDDLE LEVELS UPPER LEVEL FRAME ONE



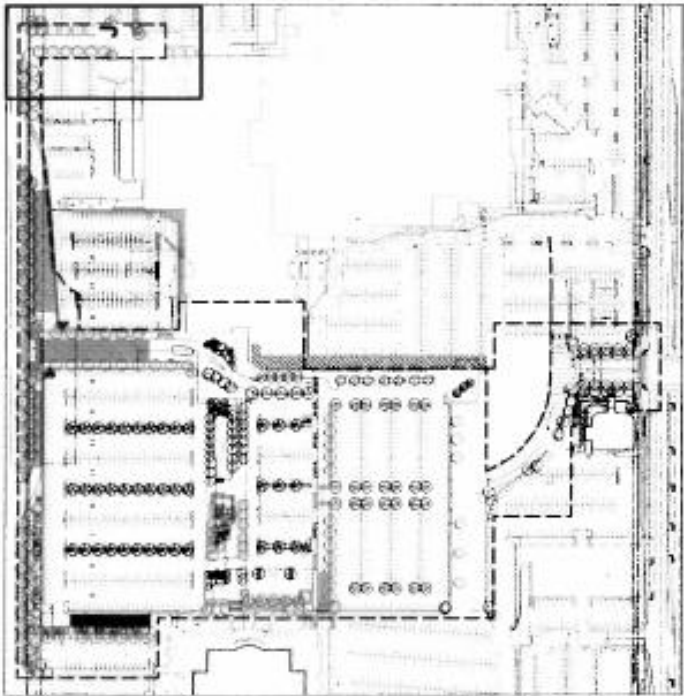
GUILDFORD



CALLISON



J UPPER LEVEL LEVEL IN USE ONE



K SET BACK PARKING AND UPPER LEVEL

PLANTING

1	PLANTING	10'
2	PLANTING	10'
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4	PLANTING	10'
5	PLANTING	10'
6	PLANTING	10'
7	PLANTING	10'
8	PLANTING	10'
9	PLANTING	10'
10	PLANTING	10'

NOTE PLANTING

1. PLANTING TO BE INSTALLED AS SHOWN.
2. PLANTING TO BE INSTALLED AS SHOWN.
3. PLANTING TO BE INSTALLED AS SHOWN.
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10. PLANTING TO BE INSTALLED AS SHOWN.

DATE: 11/11/2011
 TIME: 10:00 AM
 PROJECT: GULDFORD TOWN CENTRE EXPANSION (PHASE 1)
 SHEET: 25 OF 25
 DRAWN BY: [Name]
 CHECKED BY: [Name]

**MUSSON
 HALL
 PARTNERSHIP**

GULDFORD
 TOWN CENTRE
 EXPANSION (PHASE 1)

**MATERIALS AND
 PLANTING**

DATE: 11/11/2011
 TIME: 10:00 AM
 PROJECT: GULDFORD TOWN CENTRE EXPANSION (PHASE 1)
 SHEET: 25 OF 25
 DRAWN BY: [Name]
 CHECKED BY: [Name]

LDP 2.09

M C M



GUILDFORD
TOWN CENTRE



CALLISON



DATE: 15/03/2022

PROJECT: WESTMINSTER TOWN CENTRE

CLIENT: MCM

DESIGNER: MCM

SCALE: 1:1000

DATE: 15/03/2022

PROJECT: WESTMINSTER TOWN CENTRE

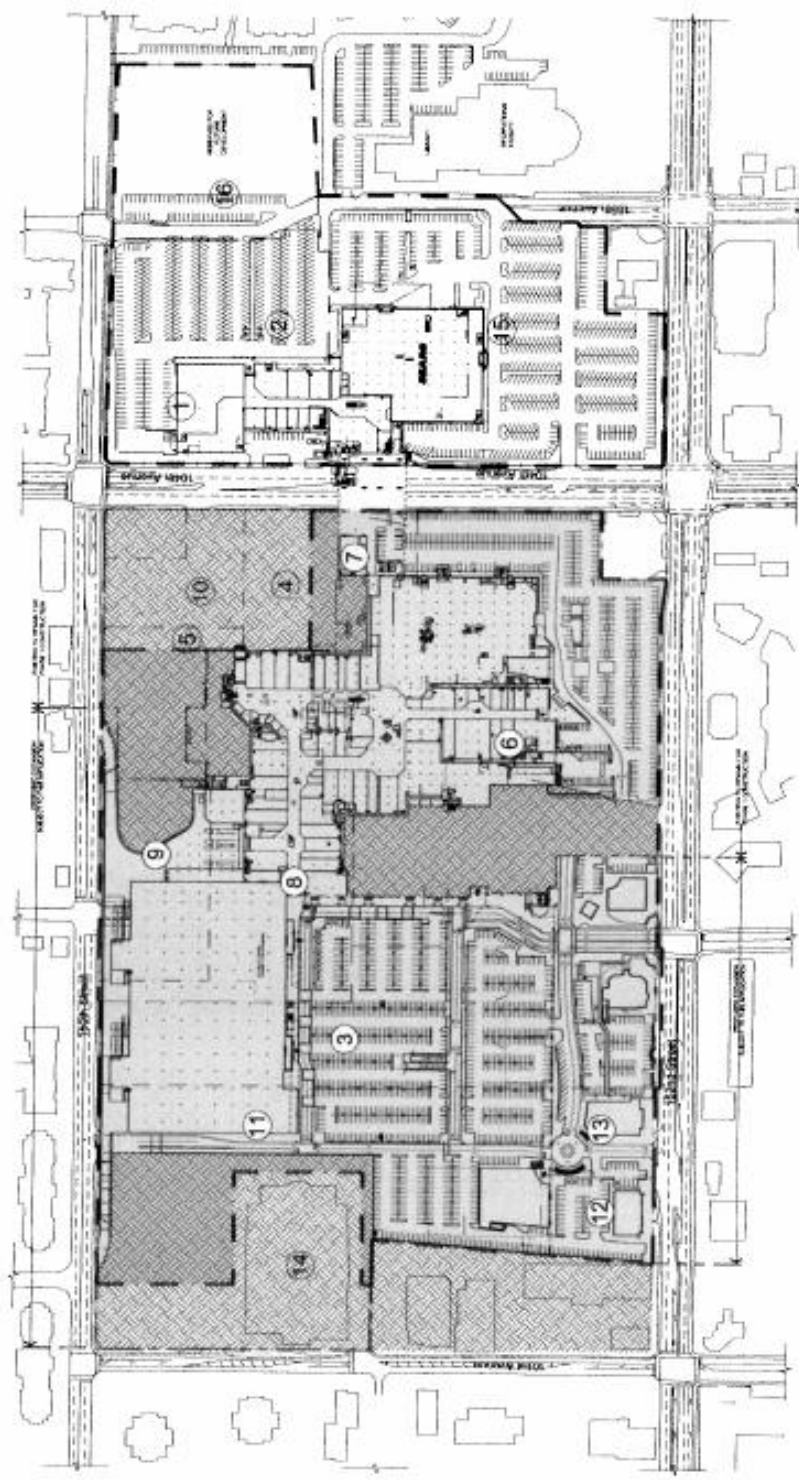
CLIENT: MCM

DESIGNER: MCM

SCALE: 1:1000

DATE: 15/03/2022

PROJECT: WESTMINSTER TOWN CENTRE



- LOTS TO BE CONSOLIDATED:**
- 1 LOT 67 EXCEPT; FIRSTLY: PART SUBDIVIDED BY PLAN 28202 SECONDLY: PARCEL A REFERENCE PLAN 28190 THIRDLY: PART SUBDIVIDED BY PLAN 28237 FOURTHLY: PART DEDICATED ROAD ON PLAN LMP2844, SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 2 LOT 74 EXCEPT; FIRSTLY: PART SUBDIVIDED BY PLAN 28244 SECONDLY: PART SUBDIVIDED BY PLAN 28274 THIRDLY: PART DEDICATED ROAD ON PLAN LMP2704 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28244
 - 3 LOT 74 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28244
 - 4 LOT 108 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 5 LOT 109 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 6 LOT 110 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN LMP2824
 - 7 LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 8 LOT 108 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 9 LOT 109 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 10 LOT 110 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN LMP2824
 - 11 LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 12 LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202
 - 13 LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202

LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202

LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202

LOT 111 SECTION 28 BLOCK 5 NORTH RANGE 1 WEST NEW WESTMINSTER DISTRICT PLAN 28202



INTER-OFFICE MEMO

TO: **Manager, Area Planning & Development
- North Surrey Division
Planning and Development Department**

DATE: **November 6, 2009**
(Supersedes Dec 14/06)
FILE: **7806-0413-00**

FROM: **Development Services Manager, Engineering Department**

RE: **Engineering Requirements (Commercial)
Location: 1250 Guildford Town Centre**

REZONE/SUBDIVISION

Property and Right-of-Way Requirements

- Dedicate 3.955 metres (north side) and 3.955 to 2.500 metres (south side) on 104 Ave;
- Dedicate 3.955 metres on 152 Street;
- Dedicate 1.308 metres on 150 Street;
- Dedicate 5-metre x 5-metre corner cuts at the intersections of 104 Avenue/152 Street and 104 Avenue/150 Street;
- Dedicate by-law roads as roads; and
- Register statutory right-of-way for westerly extension of 105 Avenue.

Works and Services

- Revise driveway accesses;
- Confirm existing watermain on 150 Street has fire protection for the proposed use;
- Confirm existing sanitary sewer system has adequate capacity for the proposed development; and
- Construct new sidewalk on 150 Street and 152 Street along with new urban forest boulevard.

A Servicing Agreement is required prior to Rezone/Subdivision.

DEVELOPMENT PERMIT

The following issues are to be addressed as a condition of issuance of the Development Permit:

- Remove bollards from 150 Street road allowance within proposed new sidewalk;
- Revise service ramps off 150 Street for curb letdown; and
- Revise proposed road boulevard trees on 150 Street and 152 Street to species indicated in the Engineering Review document.

RWB

Sam Lau, P.Eng.
Development Services Manager



LAND DEVELOPMENT ENGINEERING REVIEW

(Commercial)

File: 7806-0413-00

Location: 1250 Guildford Town Centre

Applicant: Jacques Beaudreault
Address: 555 Burrard St Suite 1600
Fax: 604-687-1771
Owner: 4239431 Canada Inc

- | | | |
|--|---|---|
| <input type="checkbox"/> OCP Amendment | <input type="checkbox"/> NCP Amendment | <input type="checkbox"/> ALR Exclusion |
| <input checked="" type="checkbox"/> Rezone | <input checked="" type="checkbox"/> LUC Partial Discharge | <input checked="" type="checkbox"/> Subdivision |
| Existing Land Use: LUC #365 & C-8 | | Existing Lots: 15 |
| Proposed Land Use: C-8 | | Proposed Lots: 8 |
| <input checked="" type="checkbox"/> DP | <input type="checkbox"/> DVP | |

Land Development Engineering Contacts:
 Richard Bull , Project Manager
 604-591-4144, RWBull@surrey.ca
 Sam Lau, P.Eng. - Development Services Manager
 604-591-4356, SLau@surrey.ca

Attachments:
 Project Layout
 Road Right-of-Way Requirements Sketch

Distribution:
 Applicant
 Transportation Manager
 Sewer Engineer
 Water Engineer
 Drainage Planning Manager
 Project Manager, Development Services

No.	Date	Revision
3	November 6, 2009	Layout revised
2	January 23, 2007	
1	December 14, 2006	Original

LAND DEVELOPMENT ENGINEERING REVIEW

File 7806-0413-00, Map #023

Background

The applicant is proposing the following:

- Partial discharge of Land Use Contract No. 365 to allow for the expansion of Wal-Mart under the C-8 Zone;
- Apply for a Development Permit to facilitate the construction of the new Wal-Mart with a floor area of approximately 165,000 sq. ft. and associated parking; and
- Consolidate eight of the fifteen existing lots into one lot on the south side of 104 Avenue.

Phase 1 involves the construction of the new Wal-Mart, proposed parking facilities and land consolidation.

This Review represents the key issues that the Engineering Department is aware of at this time. The issues listed may not be fully comprehensive and exhaustive and the applicant is required, as part of the planning and design process (including Public Hearing) to identify and resolve all items relating to the proposed land development.

Property and Right-of-Way Requirements

The applicant is required to dedicate as road:

- By-law road (Parcels 'One', 'A' and '2'; Plan with By-law filed B7958) on a road dedication or subdivision plan; and
- By-law road (Parcel 'A', Reference Plan 29780) on a road dedication or subdivision plan.

The following road right-of-way dedications, as illustrated on the Road Right-of-Way Requirements sketch attached, are required on existing roads fronting the site:

Between 105A Avenue and 104 Avenue

- 3.955 metres for a 39-metre wide road right-of-way on 104 Avenue;
- 3.955 metres for a 39-metre wide road right-of-way on 152 Street;
- 1.308 metres for a 27-metre wide road right-of-way on 150 Street to the north property line of the site;
- 5-metre x 5-metre corner cuts at the intersections of 104 Avenue/150 Street and 104 Avenue/152 Street; and
- The applicant is indicating the use of City land (10550 – 150 Street) for a drive aisle in conjunction with the proposed expansion of the parking lot (separate DP Application to be made to the Planning Department). The Engineering Department supports this request subject to the applicant entering into an appropriate licence agreement with the Realty Division (604-598-5700), the provision of a 22-metre wide statutory right-of-way for the westerly extension of 105 Avenue, and the appropriate road dedication on 150 Street and 152 Street for the ultimate divided arterial roads.

Between 104 Avenue and 101 Avenue

- 3.955 metres and 2.500 metres for a 39-metre wide road right-of-way on 104 Avenue;
- 7.308/3.955 metres for a 39-metre wide road right-of-way on 152 Street;
- 1.308 metres for a 27-metre wide road right-of-way on 150 Street;
- 5-metre x 5-metre corner cut at the intersection of 104 Avenue/150 Street; and
- 0.5-metre wide statutory right-of-way along the east side of 150 Street for sidewalk.

The applicant should address all road dedication and rights-of-way issues prior to finalizing layout or commencing legal survey or detailed design.

Servicing Requirements

These Works are required as a condition of this Rezone and Subdivision.

Transportation/Traffic Management

A Traffic Impact Study must be completed to the satisfaction of Surrey Transportation Engineering.

Driveway Accesses and Locations

104 Avenue - South Side

At this time, the applicant is not proposing any changes to the driveway accesses on 104 Avenue. Although the access to the parking lot (approximately 56 metres east of 150 Street) is acceptable to the City, any opportunity to relocate the driveway further eastwards would be supported and encouraged by the City.

As the gas station at the intersection of 104 Avenue and 150 Street has been removed and is shown on the plans presented by the applicant as an ultimate parking area, the existing driveway, approximately 12 metres east of 150 Street, is required to be removed as part of this application.

150 Street

The proposed relocation of the existing access, approximately 140 metres south of the 104 Avenue, to the new location approximately 90 metres south of 104 Avenue, provides a lesser standard of service to the site. The relocation closer to the 104 Avenue intersection will also introduce a weaving of traffic from the proposed driveway into the left turn lane at 104 Avenue.

Since there is no opportunity to relocate the proposed driveway further south on 150 Street (due to the proposed North Service Access Up Ramp), the driveway is restricted to right-in/right-out only. The applicant is required to construct a raised median on 150 Street between 104 Avenue and 102A Avenue.

As the gas station at the intersection of 150 Street and 104 Avenue has been removed and is shown on the plans presented by the applicant as an ultimate parking area, the existing driveway, approximately 20 metres south of 104 Avenue, is required to be removed as part of this application.

There are two Service Access Ramps proposed, a one-way up ramp and a one-way down ramp:

- North access is the up ramp to 150 Street. The applicant is to submit the detailed design of the level landing area for the City's review and acceptance to ensure it will fully accommodate the design vehicle (WB-20). Egress is restricted to right-out only by the construction of a raised median on 150 Street. Driveway is to be a curb letdown style and the ramp grade is to be as flat as possible to reduce noise impact on the neighbourhood; and
- South access is the down ramp from 150 Street. A landing area is to be provided to allow the design vehicle to efficiently exit off 150 Street. Access is restricted to right-in only and is to be designed to prevent left-in movements. Driveway is to be a curb letdown style.

The existing driveway access approximately 95 metres north of 101 Avenue (theater parking lot access) will be able to operate as a full movement access for the short term. When 150 Street is widened in the future to the ultimate divided arterial road cross section, the access will be restricted to right-in/right-out only. The applicant will register a restrictive covenant for future right-in /right-out only. As the theater lot is not being consolidated with this application, the applicant is required to provide a reciprocal parking/access easement agreement between the newly consolidated lot and the theater lot for the use of the parking area south of the abovenoted driveway.

The adequacy of the existing 102A Avenue intersection/mall access is to be reviewed in light of the restrictions on movements at neighbouring driveways and recommendations on laning, signalized intersection control and road widening improvements, at the applicant's cost, is to be provided.

Details of appropriate laning, road widening and location of medians on 150 Street is to be provided to reflect the changes in accesses proposed and to support access management requirements identified by the City.

101 Avenue

Access arrangements are unchanged. The applicant is to confirm right-in/right-out only for the access west of the theatre.

152 Street

The southern access, existing in the 10100 block, will be restricted to right-in/right-out/left-in only. Currently, this driveway has a grade change. The driveway grade is required to have an appropriate landing area of a minimum 6.0 metres in length at a maximum grade of 5% and followed by a maximum grade of 10%.

Due to the proposed changes to the existing 102A Avenue intersection/driveway access, the adequacy of the existing intersection is to be reviewed and recommendations on laning and road widening improvements, at the applicant's cost, are to be provided to the City as part of the site design. In particular, the proposed method of internal intersection control to ensure there is no impact on the safe and efficient operation of the 152 Street traffic signals.

General Access/Driveway Comments

- Restrictive covenants are to be registered for all driveways that have restricted movements as indicated;
- Driveways should be designed to support access restrictions with use of delta islands/curb adjustments;
- Sight distances: Typically, a 3-metre x 3-metre sight triangle is to be provided for all accesses. Minimum stopping sight distance of 85 metres (for 60 km/h limit) is required;
- All driveways (except for 150 Street service ramps) should be curb return style with typical radius of 7 - 9 metres;
- All onsite driveways should provide a pedestrian sidewalk with minimum 1.5 metre width. A 2.0 metre width is preferred at higher pedestrian activity locations (e.g. routes to and from transit);
- All onsite sidewalks must be fully wheelchair accessible; and
- Driveway medians are to be constructed to separate movements and to provide pedestrian refuge locations.

On-Site Parking and Circulation Comments

- Fire and garbage access: Minimum 12-metre radius curve is required on 7.3-metre drive aisles. The applicant is required to confirm with the Fire Department the adequacy of proposed circulation/clearances within the underground parking areas;
- Standard parking stall width is 2.7 metres on 7.3-metre drive aisles. Accessible parking of 1 stall per 100 stalls has been identified; these stalls are to be conveniently located and fully accessible;
- Cycle parking: Proposed numbers and locations of safe and secure cycle parking are to be confirmed. 135 spaces are required for the entire mall. Currently, there is only a commitment to providing appropriate cycle parking for the expansion;
- The applicant is required to address any impacts to the existing parking area on the east side of the building resulting from the required road dedication along the north side of 104 Avenue;
- The existing parking/drive aisle between the west and east parking lots is to shift south to be outside of the required road dedication along the south side of 104 Avenue. The applicant is to address possible impact to the existing loading area resulting from the parking/drive aisle shift;
- The existing parking lot on the west side of 152 Street is to shift west to be outside of the required road dedication;
- The applicant has indicated that they will not be consolidating the four lots on the south side of 104 Avenue east of 150 Street. As these four lots will be used as parking for the overall consolidated mall site, a reciprocal parking/access easement agreement is required over these lots in favour of the newly consolidated lot; and

- The applicant's ultimate site plan has not addressed the ultimate road dedication requirements along 150 Street or the south side of 104 Avenue fronting the lots which are not to be consolidated with this application. As the applicant is proposing to relocate the sidewalk fronting these lots, they will also be required to register a statutory right-of-way matching the future road dedication requirements in order to construct the new sidewalks proposed within their plan in the ultimate locations.

New Construction

The following road works are required on existing roads fronting the site:

150 Street south of 104 Avenue

- Construct the east side of 150 Street to the Divided Arterial Road standard from 104 Avenue to 101 Avenue. The applicant will be responsible for construction of the pavement widening, new concrete curb and gutter, new 2.5-metre wide concrete multi-use sidewalk (0.5 metres to be within a statutory right-of-way on the property), new driveway approximately 90 metres south of 104 Avenue, relocated street lights and new boulevard street trees;
- Plant Acer x freemanii 'Armstrong' within the road boulevard. This variety is more upright and won't be affected by truck traffic. The urban forest strip is to be prepared in accordance with the Parks median planting specification SSD-PK 5079;
- Construct a raised median between 104 Avenue and 102A Avenue to provide for a new left turn lane at the primary site entrance opposite 102A Avenue;
- Construct fully operational traffic signal at the 150 Street/102A Avenue intersection in conjunction with the new left turn lane into the site;
- The City notes that the applicant is proposing to use colourized concrete and decorative banding, within the road right-of-way sidewalk, at some of the driveway entrances into the site. The applicant is advised that the Engineering Department has no objections to the use of these methods of construction. The details for these unique features are to be approved by the City Planning & Development Department and are to be part of the engineering drawings submitted for construction for this project; and
- The City notes that the applicant is proposing to use black metal bollards within the road right-of-way at some of the driveway entrances. The applicant is advised that the use of bollards within the City road right-of-way is not permitted and to be removed from the landscape plans submitted with the Development Permit application.

152 Street south of 104 Avenue

- The applicant's site plans show the relocation and construction of a new sidewalk on the west side of 152 Street; therefore, construct from 104 Avenue to the southern limit of the site, a new 2.0-metre wide concrete sidewalk adjacent to the new property line complete with new boulevard street trees all as indicated in the applicant's Development Permit application;

- The trees to be used within the road boulevard are to be *Acer rubrum* 'Bowhall'. This variety is more upright and won't be affected by truck traffic. Since the 152 Street boulevard will be larger than normal, until such time as 152 Street is widened, the first two metres adjacent to the new sidewalk is to have topsoil 2.0 metres wide by 600mm deep (see Parks median planting specification SSD-PK 5079) with a 600mm root barrier at the future curb, 150mm behind back of future curb (see Parks specification SSD-PK 5120). The remaining boulevard is to have the usual topsoil and sod treatment;
- Coast Mountain Bus Company - Adam Wisniowski (604-953-3052) - must be consulted to confirm impact/requirements relative to the existing 152 Street bus stops fronting the site;
- The City notes that the applicant is proposing to use colourized concrete and decorative banding, within the road right-of-way sidewalk, at some of the driveway entrances into the site. The applicant is advised that the Engineering Department has no objections to the use of these methods of construction. The details for these unique features are to be approved by the City Planning & Development Department and are to be part of the engineering drawings submitted for construction for this project; and
- The City notes that the applicant is proposing to provide extensive ground cover planting, within the City road right-of-way, near the primary driveway entrance to the site opposite 102A Avenue. Due to the close proximity of the bus stop as well as the fact that the 152 Street curb is going to be moved in the future, we recommend that the funds proposed for this beautification would be better spent to enhance the existing 152 Street median landscaping north of this driveway.

The City is seeking comments from TransLink on the required transit exchange for the Guildford Town Centre. TransLink have identified the need for a facility that accommodates 9 bus bays and 8 spaces for layover. It is anticipated that these will be provided at a combined on-street and on-site facility located on 104 Avenue and/or site north of 104 Avenue.

Drainage/Environmental

The following City storm drainage facilities are located in the vicinity to the site:

- On 150 Street, there is a storm sewer system, which varies in size from 200mm storm sewer near 104 Avenue to a 900mm storm sewer near 101 Avenue. A storm service connection exists approximately 67 metres north of 101 Avenue;
- On 150 Street north of 104 Avenue, there is a 1050mm trunk storm sewer, which passes through the site, within a right-of-way, to 152 Street along the 105 Avenue alignment. A 750mm storm sewer exists just west of 152 Street within a City right-of-way;
- On 101 Avenue, a 525mm storm sewer exists, with an existing storm service connection approximately 64 metres east of 150 Street;
- On 152 Street, there is an existing 250mm storm sewer on the east side of the road for part of the frontage of the site. There are three existing service connections south of 102A Avenue;
- On 152 Street north of 104 Avenue, a 250mm storm sewer exists on the west side of the road and a 450mm storm sewer is on the east side of the road; and
- There are no storm sewers on 104 Avenue.

The applicant should note that a drainage catchment boundary is indicated in the middle of the property south of 104 Avenue. If the applicant is planning to change the catchment boundary, as part of the site redevelopment, then a meeting with the City Development Services staff and Drainage Planning staff is required before the Rezone is approved.

The site is located within the Guildford Master Drainage Plan (MDP). Reduction of the impervious area within the parking lot is required.

A stormwater management plan must be completed to the satisfaction of Surrey Drainage Engineering to assess the five year post development flows (minor system) and 100 year post development flows (major system) within the catchment.

Water quality/sediment control inlet chambers (oil/water separator) are to be installed as a component of the on-site drainage system before discharge at the inspection chambers. A restrictive covenant is required to be registered on the title of the land to require the owner/operator to maintain and keep functional the water quality/sediment control inlet chambers.

An adequately-sized service connection, complete with inspection chamber, must be provided to each lot.

The applicant will be required to obtain two (2) Erosion & Sediment Control (ESC) Permits, under By-law, 2006, No. 16138, from the Engineering Department, the first permit as part of the works and services for this site prior to issuance of the Servicing Agreement AND the second permit prior to issuance of the Building Permit. The process requires submission and approval of an ESC Plan that minimizes sediment and sediment-laden water from entering the City drainage system, during site servicing and building construction.

Water

The following City water facilities are located in the vicinity to the site:

- On 150 Street south of 104 Avenue, a 200mm watermain exists on the west side of the street north of 102A Avenue and on the east side of the street south of 102A Avenue;
- On 150 Street north of 104 Avenue, there is an existing 300 ductile iron watermain on the west side of the road and a 200mm ductile iron watermain on the east side of the road;
- On 101 Avenue, a 200mm ductile iron watermain exists on the south side of the road. There may be an existing service connection to the site approximately 97 metres east of 150 Street; however, there is no record of the size of the connection;
- On 152 Street, the site fronts a 350mm ductile iron watermain on the west side of the road. Two service connections exist south of 102A Avenue;
- On 152 Street north of 104 Avenue, there is a 250mm ductile iron watermain on the east side of the road;
- On 104 Avenue, a 300mm cast iron watermain exists on the north side of the road. The property on either side of 104 Avenue has its primary water connection to the main from this main. Based on the information provided by Steven Iker of

Quadra, Mechanical Engineer of the redevelopment, the existing water system within the site is a combined domestic and fire system. Cross connection control requirements as prescribed in the Plumbing Code and City's Water By-law require backflow prevention assemblies to be installed on the existing fire system so that proper cross connection control is available to the domestic system, or the applicant may be required to install separate domestic line and therefore new service connections into the redevelopment. As discussed with Quadra, further investigation by Quadra and discussion with the City will be required before the Rezone is approved.

The applicant is to confirm that the existing watermain on 150 Street south of 104 Avenue has adequate fire flows for the proposed use prior to the Rezone is approved.

If a new connection is requested for any of the renovations, then the connection will be a metered service connection.

Through normal processing by the Planning and Development Department - Building Division, the on-site fire protection requirements will be reviewed at building permit application stage and may require additional improvements to the building to meet the B.C. Building Code.

Sanitary Sewer

The following City sanitary sewer facilities are located in the vicinity to the site:

- On 150 Street north of 104 Avenue, there is a 300mm sanitary sewer on the west side of the road with a service connection to the site approximately 86 metres north of 104 Avenue. There is also an existing 750mm sanitary sewer near the middle of the road;
- On 150 Street, between 104 Avenue and 102A Avenue, a 750mm sanitary sewer exists near the middle of the road. Between 102A Avenue and 101A Avenue, a 600mm sanitary sewer is located near the middle of the road;
- On 101 Avenue, there is an existing 250mm sanitary sewer on the north side of the centre line of the road;
- On 152 Street south of 104 Avenue, a 250mm sanitary sewer exists on the west side of the road. There are three service connections to the site from this main; and
- On 152 Street north of 104 Avenue, there is an existing 250mm sanitary sewer on the west side of the road with a service connection.

The applicant is to provide detailed sanitary calculations for the proposed Phase 1 renovations. Upon review of the detailed calculations, the City will advise the applicant of any required improvements to the system as a result of the redevelopment of the site.

The applicant is advised that the existing sanitary sewer on 150 Street has more capacity than the existing system on 152 Street.

If an additional service connection is requested, then the minimum size will be 150mm complete with inspection chamber at the applicant's cost.

Discharge from commercial sites must comply with Sewer By-law No. 16611. Also, a permit from the GVRD for non-domestic liquid waste may be required.

Commercial Utilities

The development must be serviced with hydro, gas, telecommunication and cablevision in accordance with utility company requirements and City standards.

Project Management

A Servicing Agreement must be executed before the proposed Rezone/Subdivision/BP can be completed.

The following legal documents are known at this time to be required for this project:

- Right-of-way for public right of passage with or without vehicles adjacent to 150 Street;
- Right-of-way for public right of passage with or without vehicles adjacent to 104 Avenue;
- Right-of-way for public right of passage with or without vehicles for 105 Avenue;
- Restrictive Covenant for water quality/sediment control; and
- Reciprocal Parking/Access Easement Agreements.

All Engineering legal documents required for this project must be executed prior to issuance of the Servicing Agreement.



Financial

A processing fee of \$43,394.84 (GST included) is required for the Servicing Agreement.

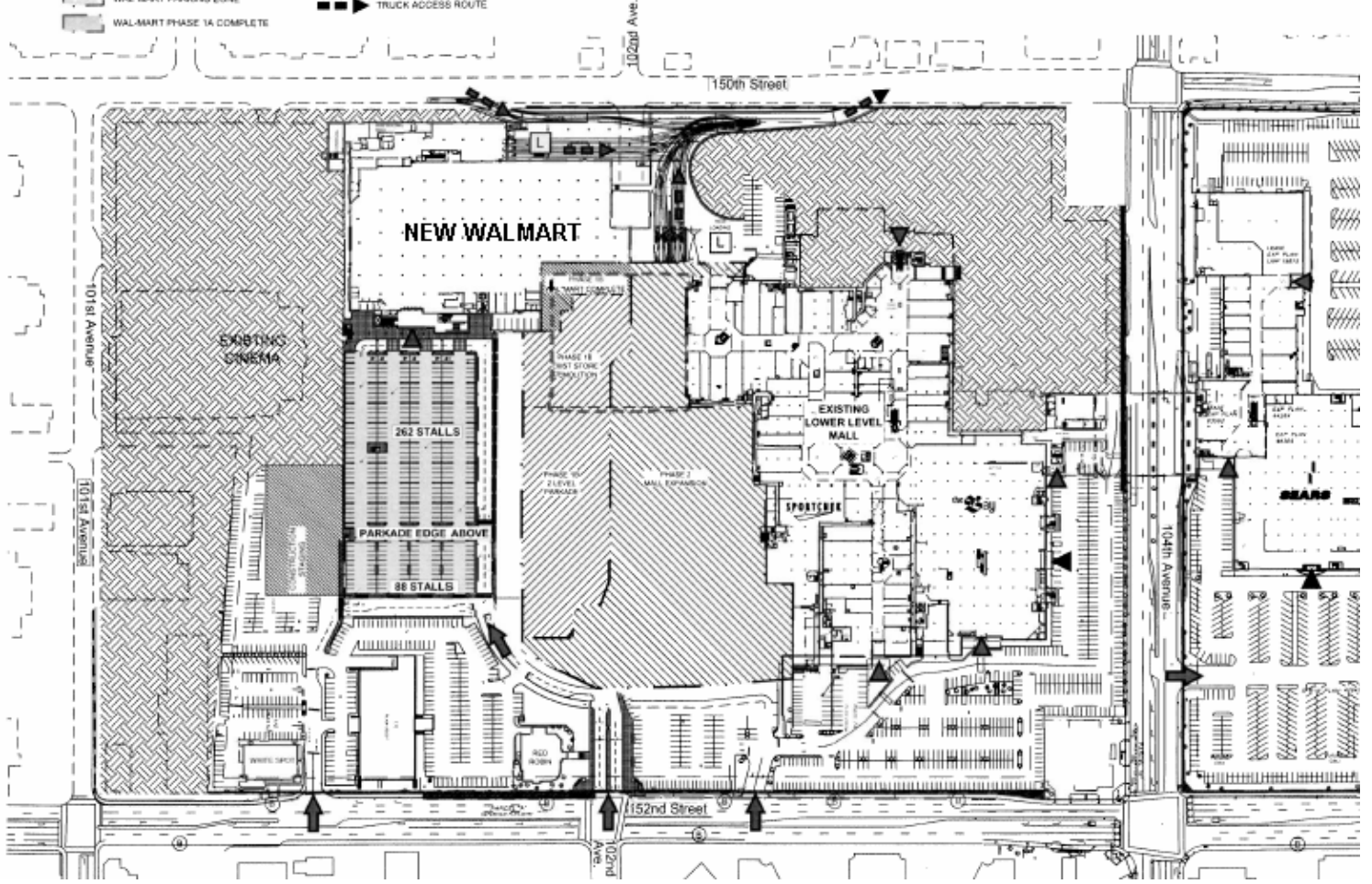
An application fee of \$420.00 (GST included) is required for administration of each of the ESC Permit process.

Project Layout

LOWER LEVEL









-  PHASE 1B - WAL-MART
 -  DECOMMISSIONING OF EXISTING WM
PHASE 1B - WAL-MART & PARKADE
 -  PHASE 2 - MALL EXPANSION
 -  WAL-MART PARKING ZONE
 -  WAL-MART PHASE 1A COMPLETE
-  L LOADING-LOWER LEVEL
 -  → SITE ACCESS POINT
 -  ▲ STORE / MALL ENTRANCE
 -  - - - TRUCK ACCESS ROUTE

WAL-MART PARKING COUNT								
PHASES	LEVEL		GLA	TOTAL STALLS PROVIDED	PARKING RATIO PROVIDED	PARKING RATIO REQD	TOTAL STALLS REQUIRED	
	LOWER	UPPER						
E	EXISTING STORE	0	600	123,317 SF	600	4.8 / 1,000 SF	4.5 / 1,000 SF	566
1A	AFTER PHASE 1A OPEN	350	995	126,094 SF	1045	7.8 / 1,000 SF	4.5 / 1,000 SF	621

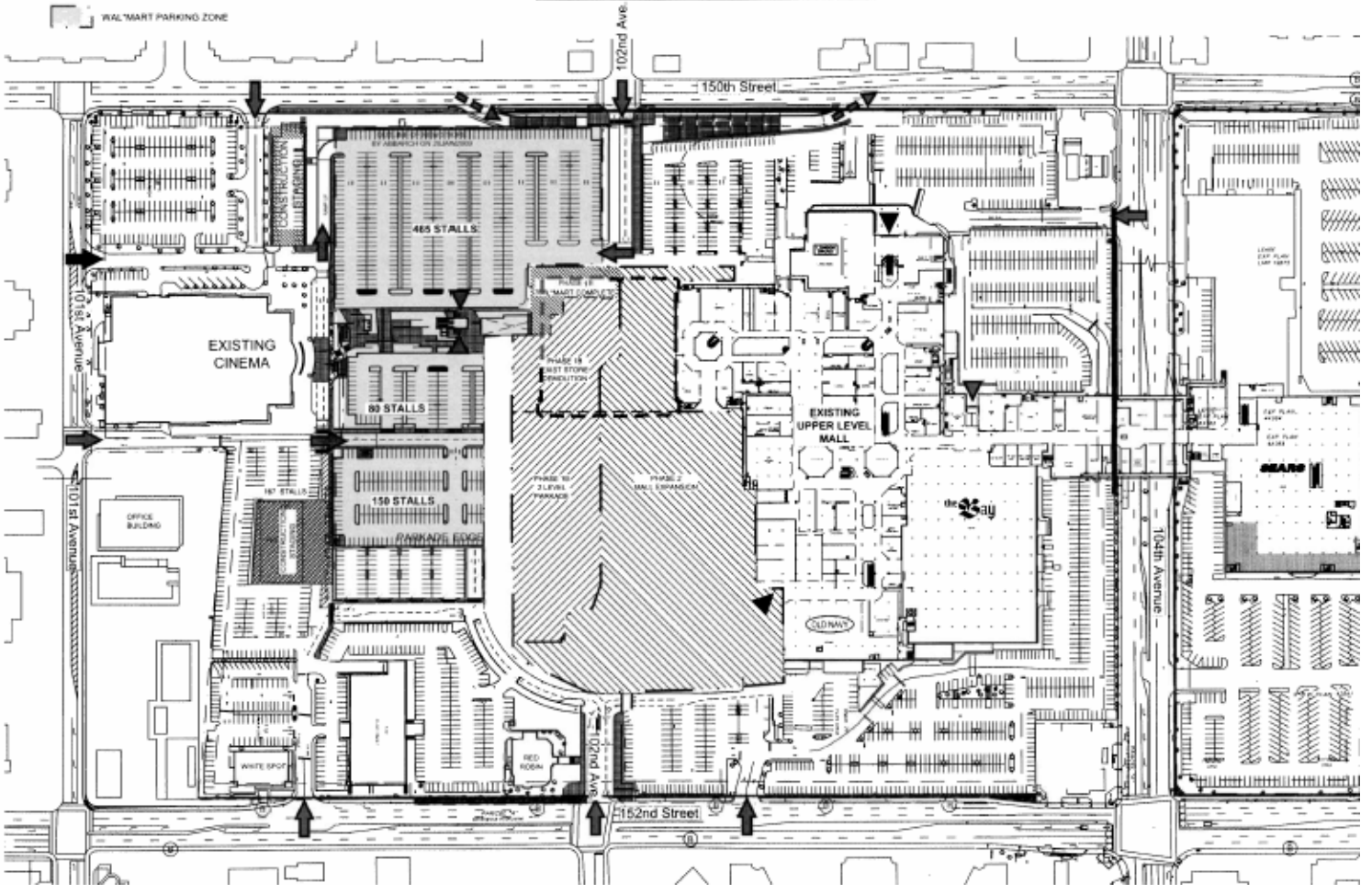


Project Layout

UPPER LEVEL

-  DECOMMISSIONING OF EXISTING RM
DURATION: +/- 1 MONTH
 -  PHASE 1B - WAL-MART & PARKADE
DURATION: +/- 8 MONTHS
 -  PHASE 2 - MALL EXPANSION
 -  PHASE 1B - WAL-MART
DURATION: +/- 8 MONTHS
 -  WAL-MART PARKING ZONE
-  SITE ACCESS POINT
 -  TRUCK ACCESS ROUTE
 -  STORE / MALL ENTRANCE

WAL-MART PARKING COUNT								
PHASES	LEVEL		OLA	TOTAL STALLS PROVIDED	PARKING RATIO PROVIDED	PARKING RATIO REQ'D	TOTAL STALLS REQUIRED	
	LOWER	UPPER						
E	EXISTING STORE	0	600	123,317 SF	600	4.8 / 1,000 SF	4.5 / 1,000 SF	555
1A	AFTER PHASE 1A OPEN	350	695	138,084 SF	1045	7.6 / 1,000 SF	4.5 / 1,000 SF	621



Road Right-of-Way Requirements Sketch

